



HILLINGDON
LONDON



Hillingdon Planning Committee

Councillors on the Committee

Councillor Henry Higgins (Chair)
Councillor Adam Bennett (Vice-Chair)
Councillor Keith Burrows
Councillor Roy Chamdal
Councillor Elizabeth Garelick
Councillor Gursharan Mand
Councillor Jagjit Singh

Date: WEDNESDAY 2 OCTOBER
2024

Time: 7.00 PM

Venue: COMMITTEE ROOM 5 -
CIVIC CENTRE

**Meeting
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to attend and observe the meeting.

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Agenda

Chairman's Announcements

- 1 Apologies for absence
- 2 Declarations of Interest in matters coming before this meeting
- 3 To receive the minutes of the previous meeting 1 - 10
- 4 Matters that have been notified in advance or urgent
- 5 To confirm that the items of business marked Part I will be considered in Public and the items marked Part II will be considered in Private

Planning Committee Report Part 1_Standard Information

11 – 18

Applications with a Petition

6	19 Beacon Close 17969/APP/2024/845	Uxbridge	Demolition of existing bungalow and erection of two pairs of semi-detached dwellings to provide 2 x 3-bedroom dwellings and 2 x 2-bedroom dwellings plus associated hard and soft landscaping. Recommendations: Approval	19 – 62 250 – 257
7	39 Parkfield Road 24825/APP/2023/81	Ickenham & South Harefield	Erection of a replacement dwelling. Recommendations: Approval	63 – 100 258 – 263
8	152-154 Uxbridge Road 4482/APP/2022/213	Yeading	Erection of three storey mixed use retail and 9 residential apartments with ancillary parking, amendments to dropped kerbs, refuse and bicycle storage, following the demolition of existing buildings. Recommendations: Approve + Sec 106	101 – 146 264 – 277

Applications without a Petition

9	Yeading Infant School 17997/APP/2024/1610	Yeading	Installation of solar panels on the school roofs. Recommendations: Approval	147 – 172 278 – 292
10	Pinkwell Primary 11242/APP/2024/1302	Pinkwell	Replacement of the existing timber fence with 3.0m high V mesh security fencing. Recommendations: Approval	173 – 190 293 – 298
11	LBH Central Depot 4501/APP/2024/1618	Colham & Cowley	Installation of an acoustic wall around service yard. Recommendations: Approval	191 – 208 299 – 304

Planning Committee Report Part 3_Policy Appendices

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Plans for the Hillingdon Planning Committee

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Agenda Item 3

Minutes

HILLINGDON PLANNING COMMITTEE

5 September 2024

Meeting held at Committee Room 5 - Civic Centre,
High Street, Uxbridge, UB8 1UW



HILLINGDON
LONDON

	<p>Committee Members Present: Councillors Henry Higgins (Chair), Adam Bennett (Vice-Chair), Roy Chamdal, Keith Burrows, Elizabeth Garelick, Gursharan Mand, and Jagjit Singh</p> <p>Officers Present: Katie Crosbie (Area Planning Service Manager – North), Ed Laughton (Area Planning Service Manager – Central and South), Chris Brady (Planning Team Leader), Eoin Concanon (Planning Team Leader), Alan Corcoran (Deputy Team Leader), Dr Alan Tilly (Transport & Aviation Team Manager), Jimmy Walsh (Legal Advisor), Natalie Fairclough (Legal Advisor), and Ryan Dell (Democratic Services Officer)</p>
22.	<p>APOLOGIES FOR ABSENCE (<i>Agenda Item 1</i>)</p> <p>There were no apologies.</p>
23.	<p>DECLARATIONS OF INTEREST IN MATTERS COMING BEFORE THIS MEETING (<i>Agenda Item 2</i>)</p> <p>None.</p>
24.	<p>TO RECEIVE THE MINUTES OF THE PREVIOUS MEETING (<i>Agenda Item 3</i>)</p> <p>RESOLVED: That the minutes from the meeting on 16 July 2024 be approved.</p>
25.	<p>MATTERS THAT HAVE BEEN NOTIFIED IN ADVANCE OR URGENT (<i>Agenda Item 4</i>)</p> <p>None.</p>
26.	<p>TO CONFIRM THAT THE ITEMS OF BUSINESS MARKED PART I WILL BE CONSIDERED IN PUBLIC AND THE ITEMS MARKED PART II WILL BE CONSIDERED IN PRIVATE (<i>Agenda Item 5</i>)</p> <p>It was confirmed that all items would be heard in Part I.</p>

It was confirmed that item 7 had been withdrawn from the agenda.

27. **36 MOOR PARK ROAD, NORTHWOOD - 77170/APP/2024/1240** (*Agenda Item 6*)

Officers introduced the application and noted the addendum, which referred to a submission made by a Ward Councillor.

Officers added a verbal amendment to Condition 4, which related to restricted permitted development rights. There was a reference in the condition to the third floor of the building being restricted but it was clarified that the restriction would cover the entirety of the building.

The lead petitioner addressed the Committee and made the following points:

- The petitioner thanked the Committee for giving them the opportunity to explain their position
- The petition reflected the concerns of a large number of petitioners, many of whom had been living in the neighbourhood for decades
- The applicant had attempted to airbrush the use as comparative to a family dwelling, but this was not true
- The proposal would have a significantly detrimental impact on noise both inside and outside of the property; parking; congestion; trip generation; CO2 emissions; and disturbance to neighbours due to comings and goings during the day and also during evenings and weekends
- The report stated that the property would cater for up to four children with emotional and behavioural difficulties with a staff ratio of two adults to one child. This implied up to eight carers plus managerial staff
- The report assumed there would be only three car users. This overlooked the 2:1 ratio
- There could be 14-16 people in the property at any one time
- Further footfall from social workers, support workers, parents and friends of the children had not been accounted for
- All of this would add to the noise, parking, traffic and CO2 emissions
- The report's conclusions, that were based on three carers rather than eight, were hence flawed
- The application stated that there would be three parking spaces in front of the property and two additional spaces which were essentially a garage. However, once two cars were parked in the garage it would be difficult, if not impossible, to open a car door to get out of a vehicle. This was impractical
- One of the bays was blocked by another bay
- The report stated that the site can potentially accommodate in excess of half-a-dozen vehicles arranged in an informal fashion. The safety impact of jamming cars into the driveway had not been considered. There was no consideration for emergency vehicles to access the building. Displaced on street parking was therefore inevitable
- On noise, the application stated that the children would have behavioural and emotional difficulties, and acknowledged that despite meticulous planning and care, the children's behaviour may occasionally fall below acceptable standards. Staff may need to use restraint techniques. This would cause noise and disturbance
- The noise control plan was merely words. It gave an email address to register a complaint which would aim to be resolved within three working days. The

Council did not investigate domestic noises

- In addition to the noise, there would be disturbance from the comings and goings to the property by four children; up to 8 carers; social workers; health workers and four sets of friends and family.
- Petitioners disagreed with paragraph 7.32 of the report. Footfall and vehicular traffic would lead to the property having a feel of a commercial enterprise rather than a family dwelling. This would cause significant disturbance to the locality in increased carbon emissions
- The property had a PTAL (Public Transport Accessibility Level) score of 0 as per the transport assessment report. Public transport was not a viable or convenient alternative. Care home staff and visitors would have to rely on their own private vehicles, thereby increasing the vehicle trips
- The nearest shops were 15 minutes away by foot, or a bus or car ride away
- Moreover, there were no state schools for children aged 11 plus in Northwood. The nearest schools were in Northwood Hills, Pinner, or Watford. Not only would this give rise to more vehicles, but was this location even an appropriate location for a care home for 12 and 16 year olds?
- There was a profound and extensive opposition and concern from residents to the proposal which did not comply with the Local Plan
- Petitioners requested the Committee put residents' interests first, as per the Hillingdon motto, and not the interests of a for-profit company.
- This proposal would alter the fabric of the neighbourhood

The applicant's representatives addressed the Committee and made the following points:

- The representatives thanked the Committee for giving them the opportunity to share their views
- The directors of the children's home had extensive experience working with children and families
- Collectively they had over 35 years of experience working in various roles within social care
- This experience demonstrated their commitment to promoting the welfare and safeguarding of children
- They also had insights and the ability to understand what contributed to positive outcomes for children looked after
- They understood the journey of the child and often the trauma they had experienced before coming into care
- Children looked after came from diverse cultures and backgrounds and for various reasons were unable to live with their birth families. Therefore, the applicants had made it their responsibility to ensure they had the opportunity to thrive, find happiness and lead fulfilling lives
- The goal was to provide children looked after with practical and emotional support to help them map out their clear route to a prosperous, independent future
- New Chapters Homes were driven to ensure that the children in their care feel valued and safe, and they aimed to give them a childhood where they were no longer experiencing significant harm
- They had reviewed the information shared within the petition
- They empathised with residents and recognised that the unknown can be daunting
- The petition referenced children looked after being linked to increases in antisocial behaviour and personal risk. In the professional capacity of the

applicant, children looked after were often vulnerable and did not present a greater risk to adults or children around them

- With the right support and environment, they can have the same outcomes as children who were not in care
- The applicant wished to work with residents and not against them
- The representatives highlighted the lived experience of a young person currently living in a children's home, whose name had been anonymised:
 - *My name is Lily and I'm 12 years old*
 - *I feel scared at home sometimes and don't always know how the adults in my life are going to treat me*
 - *I'm worried about being at home, but I don't want anyone to know*
 - *Every night I would lie awake, dreading the sound of my bedroom door creaking open*
 - *They told me to keep quiet, blaming me for everything, so I stayed silent at school*
 - *At school I watched other kids laugh, feeling like there was a wall between us*
 - *I pretended everything was OK, even though I was falling apart inside*
 - *One day, a teacher noticed how I flinched when someone touched me*
 - *She kept asking me questions, showing kindness I hadn't felt in a long time*
 - *Eventually I broke down and told her everything. I was terrified she would blame me, but she didn't*
 - *She held my hand and told me it wasn't my fault, that I was brave for speaking up, and that's when I met my social worker*
 - *I moved in with a foster family, but they didn't understand me, and I felt uncomfortable in someone else's home*
 - *I hurt myself and ended up in hospital*
 - *My social worker told me after that it would be difficult for me to be given a placement with a foster family again*
 - *I felt like it was my fault and that I didn't deserve anything good*
 - *I was moved to a children's home and was really scared when I saw the house. I'd never been in a house this big and quiet*
 - *I didn't feel like I belonged in this home or the area, and I wanted to run away when I first got there, but I knew I couldn't go home to my family*
 - *I was given a key worker who helped me feel a little bit more at home and I was able to have my own room and choose what colour the room would be and now things are still hard but I'm not alone anymore*
 - *I'm learning that my past doesn't define me and that I deserve to laugh and be happy too*
 - *I now have adults around me that I can trust at the home, But I miss my home and hope to go back one day*

Members asked about the staff numbers and ratios presented in the application. Clarification was sought on the expected number of staff during the busiest days. The representatives explained that while the capacity was for up to four children, it was unlikely to have four children at once due to the need for matching and considering the children's needs. It was not expected for there to be more than four staff members in the day, including senior management. Members replied that one of the petitioner's concerns was the number of car journeys, especially given the PTAL rating of 0. Members asked for further clarification on numbers of staff. The representatives noted that due to the complexity of the children it was unlikely that there would be four children at one time. It would be important to ensure that from the time the first young

person was received in the home, any additional young people had to be matched. If a young person had a high level of need and needed a staff ratio of 2:1 or 3:1, it was very unlikely that the home would be able to take on another child.

Members asked for more clarity on the number of staff if there were four children. The representatives noted that if there were four children, it would have to be four children with low level needs where they could have a 1:1 worker or no worker. Children came with different complexities. For example, a child who struggled with sleeping at night but was able to function in the day may not require a 1:1 worker. If there were children of this nature then there could be up to four children and it would only require two to three members of staff in the day. A child with more complexities may require 1:1 or 2:1 support. The work with children would involve things such as art therapy, music therapy, talking therapy and this required space.

Members asked about the parking provision, noting concerns about accessing certain bays when other bays were occupied, and the impact on local traffic. The representatives explained that any on-site parking issues could be resolved as all staff working there would know each other, similar to at a family home.

Members asked how the number of staff would translate into parking and vehicle movements. The Highways officer noted that they would expect, and asked for a condition for, the parking layout to be revised to ensure five usable spaces. Officers referred to the development plan to determine how many car parking spaces proposals can provide. The London Plan was silent on care homes, in which case officers had referred to the Local Plan. The Local Plan would require two car parking spaces plus one space per warden. It was taken that there would be three wardens. A minimum of two spaces plus three for wardens gave five spaces. Therefore, officers took the view that five was the maximum amount of car parking spaces that the policy would allow. If the demand for parking exceeded supply, consideration would have to be given to parking displacement. It was noted that this would not raise highways concerns.

The Chair noted that there were some parking restrictions on the road and it was congested during school times.

The Chair noted the comments from Councillor Lewis as Ward Councillor which had been noted within the addendum.

There was a need nationally and locally to look after vulnerable children. There was a need for this type of accommodation and the challenge was to ensure that it was the right development for the area and that residents' concerns were listened to.

Members asked about the possibility of conditioning the ratio of parking to the occupancy of the home and level of need of the children. Officers noted that it would be unreasonable to condition the car parking on the basis of staff numbers. There was a condition on a maximum of four children. A parking management plan could be considered.

Members asked about potential noise and disturbance from the facility, particularly during staff shift changes. Officers noted that conditions related to noise and disturbance were typically covered by other legislation and may not be enforceable through planning conditions.

Members further asked if conditions could be imposed on timing of staff shift changes.

	<p>Conditions would need to be enforceable and precise. A condition on this would be difficult to enforce.</p> <p>It was highlighted that the parking scheme was nearby, not in the road in question. These restrictions had been provided for road safety purposes such as double yellow lines on the corners, the keep clear markings outside the school and the 20mph speed limit.</p> <p>Members noted some confusion around a number of aspects including the number of staff and the parking provision. The Chair suggested that a site visit may help to ascertain the parking situation. Members queried how a site visit would clarify the numbers of staff. It was noted that if the item was deferred in that there was not full information in front of Members, an additional benefit would be a site visit.</p> <p>The petitioner had noted that the Council did not investigate domestic noise complaints and Members asked if this would constitute domestic noise. It was noted that these types of noise issues were covered by separate legislation and this would be outside of planning legislation.</p> <p>It was summarised that Members wanted additional information on how many staff were going to be on site, and shift times.</p> <p>The proposal to defer for further information and to undertake a site visit was moved, seconded and when put to a vote, agreed.</p> <p>RESOLVED: That the item be deferred</p>
28.	<p>39 PARKFIELD ROAD, ICKENHAM - 24825/APP/2023/81 (<i>Agenda Item 7</i>)</p> <p>This item was withdrawn from the agenda.</p>
29.	<p>37 EDWARDS AVENUE, RUISLIP - 65680/APP/2023/2256 (<i>Agenda Item 8</i>)</p> <p>Officers introduced the application. It was important to note that the assessment of the proposal placed before Members for determination was restricted to the proposed amendments and not matters which had already been benefited from planning consent.</p> <p>The lead petitioner addressed the Committee and made the following points:</p> <ul style="list-style-type: none"> • The petitioner noted the 2011 application (65680/APP/2011/36) • The current application reference was 65680/APP/2023/2256 • It was disappointing that only the two conditions were being considered and not the full application • Residents had a lot of interest in this application given the history of the site • The site was derelict and untouched • One of the key points was that the parking management scheme should not be negatively affected <p>Councillor Steve Tuckwell addressed the Committee as Ward Councillor and made the following points:</p> <ul style="list-style-type: none"> • It was fully understood that the current application was on the basis of determining the variation of Condition 5 and the removal of Condition 18 from the parent planning permission that was granted in April 2011 • The Committee can only determine what was presented to it

- However, points needed to be raised for public record and for clarification
- Residents believed that their opportunity to raise legitimate concerns using the petitions process about the overall application has been denied. For this reason, planning officers were asked to outline the following points:
 - What was the underpinning logic of granting the Certificate of Lawful Development on the 17 July 2024?
 - What weight was applied to the planning application document contained within today's application that expressly referenced that the development of the site did not commence until the 01 May 2014, one month after the 2011 planning consent expired?
 - Given that this petition was submitted by residents in September 2023 expressing concern over the overall development, why was the Certificate of Lawful Development granted before the matter was brought before this Committee this evening?
- Residents believed that the original planning application had expired and that the new application with the full adherence to updated planning policies, should have been submitted for full determination
- Whilst the Committee can only determine the variation and removal of conditions, not having the ability to determine the full application render this application devoid of the updated and refreshed planning policies, such as the provision of family homes and electric charging points, air quality and the excessive size of the dropped curb

Officers advised that they accepted an application for a certificate of lawfulness before determining the current application because the more robust way to address the extent of the original application was for the applicant to formalise and submit an application for officers to consider all the information. This was done and granted in July 2024.

In terms of the weight given to this current application documents, the application form did state that the dwelling had been demolished in May 2014. This was presumed to be an error and it was corrected to the date of 01 April 2014 and that was submitted on 01 July 2023. The applicant had submitted the certificate of lawfulness.

Regarding the logic for granting the certificate of lawfulness, the planning permission was granted in April 2011 and that was for the demolition of the bungalow and construction of four back-to-back dwellings. All pre-commencing conditions were discharged with the exception of Condition 18, which was being considered this evening. Condition One on that planning permission required that the development would have to begin within three years of that date, which brought it up to 04 April 2014. In terms of determining whether the development with the planning permission had commenced, officers needed to evaluate, on the balance of probability, when the existing bungalow had been demolished and had it been demolished prior to 01 April 2014. This was the evidence that the applicant submitted for the certificate of lawfulness.

To clarify what was assessed when looking at certificates of lawfulness, paragraph six of the MPG stated that in the cases of applications for existing use, if a local planning authority had no evidence itself, nor any from others, to contradict or otherwise make the applicant's version of events less than probable, there was no good reason to refuse the application provided the applicant's evidence alone was sufficiently precise and unambiguous to justify the grant of a certificate on the balance of probability. As part of the evidence that the applicant had submitted, they submitted a statutory declaration from a local estate agent which stated that the demolition commenced on

17 March 2014. The application form also said this. A second statutory declaration from the applicant also confirmed when the site was purchased and that the sale of the site included a demolition method statement which confirmed that the demolition of the dwelling was programmed for the week commencing 17 March 2014. The demolition method statement itself was also submitted. In respect to the Council's own records and whether there was anything that was contrary to this, there were building control records from an e-mail received on 14 April 2014, which advised that the demolition had already commenced. While that date is slightly over, it did not contradict that the demolition had happened during the week of 17 March 2014. It was noted that it was not standard practise for the Council to also consult residents, but officers did post letters to a significant number of residents within the street and the adjoining street to see if anyone had any other evidence that would be contrary to the applicant's version of events. There was nothing that did contradict it. On the balance of probability, it was determined that the bungalow was demolished before 14 April 2014, and therefore the planning permission was valid. The application being considered this evening was a variation, and it was not possible to revisit things that were already considered in the determination of the original application, only the differences.

Members referred to the appendix, which referenced a time limit deleted condition. Members asked for clarification of this. The time limit referred to the demolition having to have commenced within three years of the permission. As it had already commenced, the condition was deleted.

Members asked about Condition 18, noting other recent applications where it had been specified that future occupants could not have access to parking permits if there was a parking management scheme in the street. Members asked why this was not the case for the current application. Officers noted that the parking that would be provided as part of the application was four parking spaces for the four x two bed units, so one parking space for each dwelling. This was a minor over-provision and so was considered, on balance, acceptable. In terms of the mechanism, it would not meet the test of the condition which would ordinarily be secured through legal agreement, which had not been done at the time.

Members referred to the history of planning, noting an application approved in September 2014. Members asked what the difference was between this and the application approved in 2011. Officers clarified that the 2014 application related to the discharge of conditions of the application that was approved in 2011. In order to demonstrate that the development had been commenced, the applicant would have to have discharged the pre-commencement conditions and this was the application which came in in 2014 and was approved.

Officers' recommendations were moved, seconded and when put to a vote, agreed.

RESOLVED: That the application be approved

30. **ATLAS LODGE - 585/APP/2024/1558** (*Agenda Item 9*)

Officers introduced the application.

Members highlighted the need for affordable housing.

Members asked about the legal definition of primary occupants, and whether the development would be restricted to over 55s only or just be primarily for over 55s. It

	<p>was noted that the previous scheme had a similar arrangement whereby the primary occupier was restricted to being a person needing assisted living. The current arrangement would work in a similar way.</p> <p>Officers' recommendations were moved, seconded and when put to a vote, unanimously agreed.</p> <p>RESOLVED: That the application be approved</p>
31.	<p>3 ST MARGARETS AVENUE - 59652/APP/2024/1459 (<i>Agenda Item 10</i>)</p> <p>Officers introduced the application.</p> <p>Officers' recommendations were moved, seconded and when put to a vote, unanimously agreed.</p> <p>RESOLVED: That the application be approved</p>
	<p>The meeting, which commenced at 7.00 pm, closed at 8.35 pm.</p>

These are the minutes of the above meeting. For more information on any of the resolutions please contact Ryan Dell at democratic@hillington.gov.uk. Circulation of these minutes is to Councillors, Officers, the Press and Members of the Public.

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Planning Committee Report Part 1:

Standard Information for
Members - Applicable to All
Applications on the Agenda



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Part 1: Statutory Planning and Human Rights Considerations

1.1 Development Plan

1.1.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 and section 70(2) of the Town and Country Planning Act 1990, require that applications for planning permission be determined in accordance with the development plan, unless material considerations indicate otherwise.

1.1.2 The development plan for the London Borough of Hillingdon consists of the following documents:

- [Hillingdon Local Plan Part 1: Strategic Policies \(2012\)](#)
- [Hillingdon Local Plan Part 2: Development Management Policies \(2020\)](#)
- [Hillingdon Local Plan Part 2: Site Allocations and Designations \(2020\)](#)
- [The West London Waste Plan \(2015\)](#)
- [The London Plan \(2021\)](#)

1.2 Equality Act

1.2.1 Section 149 of the Equalities Act 2010, requires the Council, in considering planning applications to have due regard to the need to eliminate discrimination, advance equality of opportunities and foster good relations between people who have different protected characteristics. The protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

1.2.2 The requirement to have due regard to the above goals means that Members should consider whether persons with protected characteristics would be affected by a proposal when compared to persons who do not share that protected characteristic. Where equalities issues arise, Members should weigh up the equalities impact of the proposals against the other material considerations relating to the planning application. Equalities impacts are not necessarily decisive, but the objective of advancing equalities must be considered in weighing up the merits of an application. The weight to be given to any equalities issues is a matter for the decision maker to determine in all the circumstances.

1.3 Human Rights

1.3.1 The Human Rights Act 1998 incorporates the key articles of the European Convention on Human Rights into domestic law. These include:

- Article 8: Right to respect for private and family life.

Everyone has the right to respect for his private and family life, his home and his correspondence. This right embodies the right to a name, the right to change one's civil status and to acquire a new identity, and protection against telephone tapping, collection of private information by a State's security services and publications infringing privacy. This right also enables Members of a national minority to have a traditional lifestyle.

- Article 1 of the First Protocol: Protection of property.

Every natural or legal person is entitled to the peaceful enjoyment of his possessions. No one shall be deprived of his possessions except in the public interest and subject to the conditions provided for by law and by the general principles of international law.

- Article 14: Prohibition of discrimination.

The enjoyment of the rights and freedoms set forth in this Convention shall be secured without discrimination on any ground such as sex, race, colour, language, religion, political or other opinion, national or social origin, association with a national minority, property, birth, or other status.

1.3.2 Members must be aware of the rights contained in the Convention (particularly those set out above) when making any planning decisions. However, most Convention rights are not absolute and set out circumstances when an interference with a person's rights is permitted. Any interference with any of the rights contained in the Convention must be sanctioned by law and be aimed at pursuing a legitimate aim and must go no further than is necessary and be proportionate. Members must, therefore, carefully consider the balance to be struck between individual rights and the wider public interest.

1.4 Development in Conservation Areas

1.4.1 Section 72(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990 requires the local planning authority, in determining applications affecting conservation areas, to pay special attention to the desirability of preserving or enhancing the character or appearance of that area. This statutory duty needs to be considered alongside relevant heritage policies contained in the National Planning Policy Framework and local plan.

1.5 Development Affecting Listed Buildings

- 1.5.1 Section 66(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990 requires the local planning authority, in determining applications affecting a listed building or its setting, to “have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses”. This statutory duty needs to be considered alongside relevant heritage policies contained in the National Planning Policy Framework and local plan.

Part 1: Other Relevant Information for Members

2.1 Five Year Housing Land Supply

- 2.1.1 Land supply is a key part of planning and links plan policies and sites with actual delivery. The need to demonstrate a 5yr rolling supply of sites, known as 5yr housing land supply (5YHLS), is an embedded part of the planning system.
- 2.1.2 When councils are unable to demonstrate a 5YHLS the National Planning Policy Framework 2023 (NPPF) presumption in favour of sustainable development - the so-called ‘tilted balance’ - is engaged. NPPF paragraph 11 (d) ii states that in these circumstances the development plan policies most important for determining the application are to be treated as out-of-date. Therefore, where the presumption applies, planning permission should be granted unless:
1. The application of policies in the NPPF that protect areas or assets of particular importance provides a clear reason for refusing the development proposed; or
 2. Any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole.
- 2.1.3 Hillingdon Council is currently able to demonstrate a 5yr supply of deliverable housing sites. Therefore, the ‘tilted balance’ is not engaged.

2.2 Planning Appeals / Risk of Costs Award Against the Council

- 2.2.1 Members should be aware that in the event of an appeal, local planning authorities are at risk of an award of costs if they behave unreasonably with respect to the substance of the matter under appeal. For example, by

unreasonably refusing or failing to determine planning applications, or by unreasonably defending appeals.

- 2.2.2 A further example includes imposing a condition that is not necessary, relevant to planning and to the development, enforceable, nor precise or reasonable in all other respects (and thus does not comply with NPPF guidance on planning conditions and obligations). It should be noted that planning conditions can be appealed.
- 2.23 Another example includes failing to substantiate each reason for refusal on appeal. Therefore, should members determine to refuse an application (contrary to officer recommendation for approval) planning reasons for refusal should be provided.

2.3 Use of Planning Conditions

- 2.3.1 Members may decide to grant planning consent subject to conditions. Planning consent should not be refused where planning conditions can overcome a reason for refusal.
- 2.3.2 Planning conditions should only be imposed where members are satisfied that imposing the conditions are necessary, relevant to planning, relevant to the development to be permitted, enforceable, precise and reasonable in all other respects. Where conditions are imposed, the Council is required to provide full reasons for imposing those conditions.

2.4 Planning Obligations

- 2.4.1 Policy DMCI 7 of the Hillingdon Local Plan: Part 2 (2020) states that whilst infrastructure requirements will be predominantly addressed through the Council's Community Infrastructure Levy (CIL), planning obligations will be sought on a scheme-by-scheme basis. Applications that fail to secure an appropriate Planning Obligation to make the proposal acceptable will be refused.
- 2.4.2 The Community Infrastructure Levy Regulation 2010 (Regulations issued Pursuant to the 2008 Act) and the NPPF have put three tests on the use of planning obligations into law. It is unlawful to request planning obligations that do not meet the following tests:
- i. necessary to make the development acceptable in planning terms,
 - ii. directly related to the development, and
 - iii. fairly and reasonable related in scale and kind to the development.
- 2.4.3 The effect of the Regulations is that the Council must apply the tests much more strictly and is only to ask for planning obligations that are genuinely

necessary and directly related to a development. Should planning obligations be requested that do not meet the policy tests, the Council would have acted unlawfully and could be subject to a High Court challenge.

- 2.4.4 Members must be satisfied that any planning obligations to be secured by way of an agreement or undertaking pursuant to Section 106 of the Town and Country Planning Act 1990 are necessary to make the development acceptable in planning terms. The obligations must be directly related to the development and fairly and reasonably related to the scale and kind to the development (Regulation 122 of Community Infrastructure Levy 2010).

2.5 Community Infrastructure Levy (CIL)

- 2.5.1 The Community Infrastructure Levy (CIL) allows local authorities to raise funds from developers undertaking new building projects in their area. The Community Infrastructure Levy (CIL) is a charge collected from new developments.
- 2.5.2 The CIL applies to all proposals that add 100 square metres of new floorspace or an extra dwelling. This includes bringing a vacant building back into use. The amount to pay is the increase in floorspace (m²) multiplied by the rate in the CIL charging schedule plus indexation.
- 2.5.3 The money raised from the Community Infrastructure Levy pays for the infrastructure required to support development. This includes transport schemes, flood defences, schools, health and social care facilities, parks, open spaces and leisure centres.
- 2.5.4 The London Borough of Hillingdon adopted its [CIL Charging Schedule](#) on 10 July 2014 and it is applied to new developments in the borough since 1 August 2014. The use types that are charged borough CIL is large format retail development (greater than 1,000sqm) outside of designated town centres; offices; hotels; residential dwellinghouses; and industrial storage and distribution.
- 2.5.5 The Mayor's CIL (MCIL)
- The Mayor's CIL applies to all qualifying developments approved on or after 1 April 2012. Hillingdon Council is a CIL collecting authority for the Mayor of London.
- 2.5.6 The Mayoral CIL 1 (MCIL 1) rate was £35 per sqm plus indexation and is used by the Mayor of London to fund the delivery of Crossrail.
- 2.5.7 For planning permissions granted from 1 April 2019, the Mayoral CIL 2 (MCIL 2) rate of £60 per square metre plus indexation applies. This rate may also apply to some phased planning permissions granted before then.

2.6 Environmental Impact Assessment

- 2.6.1 The Town and Country Planning (Environmental Impact Assessment) Regulations 2017 (as amended) requires that an Environmental Impact Assessment (EIA) is undertaken, and an Environmental Statement (ES) produced for certain developments.
- 2.6.2 EIA is a procedure which serves to provide information about the likely significant effects of a proposed project to inform the decision-making process and whether the project should be allowed to proceed, and if so on what terms.
- 2.6.3 An overview of the EIA process is provided as part of government's [Planning Practice Guidance](#). An EIA is normally only necessary for a small proportion of projects.
- 2.6.4 An EIA Screening Opinion can be obtained from the council to determine whether a proposed development needs an EIA. Once it has been determined that an EIA is required, an EIA Scoping Opinion can be obtained from the Local Planning Authority to provide advice on the scope and content of the Environmental Statement (ES).

Report of the Head of Development Management and Building Control Committee Report Part 2 – Application Report

Case Officer: Rhian Thomas	17969/APP/2024/845
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Date Application Valid:	02/04/2024	Statutory / Agreed Determination Deadline:	04/10/24
Application Type:	Full	Ward:	Uxbridge

Applicant: **Mr Roda**

Site Address: **19 Beacon Close, Uxbridge**

Proposal: **Demolition of existing bungalow and erection of two pairs of semi-detached dwellings to provide 2 x 3-bedroom dwellings and 2 x 2-bedroom dwellings plus associated hard and soft landscaping.**

Summary of Recommendation: **GRANT planning permission subject to section 106 legal agreement and conditions**

Reason Reported to Committee: **Required under Part 3 of the Planning Scheme of Delegation (Petition received)**



Summary of Recommendation:

GRANT planning permission subject to the conditions set out in Appendix 1.

1 Executive Summary

- 1.1 Planning permission is sought for the demolition of existing bungalows and the erection of two pairs of semi-detached dwellings to provide 2 x 3-bedroom dwellings and 2 x 2-bedroom dwellings, plus associated hard and soft landscaping.
- 1.2 Planning permission was previously refused by the Borough Planning Committee in February 2024 (application ref: 17969/APP/2023/1014) for the erection of four terraced dwellings incorporating landscaping, parking provision, waste and cycle stores following demolition of the existing dwelling. The application was refused for 4 reasons which are set out in more detail within Section 4 of this committee report. In summary concerns were raised regarding the design and its departure from the existing pattern of development and character of the area, the excessive provision of hardstanding and the lack of a legal agreement to prohibit future occupiers from joining the local parking management scheme.
- 1.3 The current revised scheme seeks to overcome the previous reasons for refusal by providing 4 new dwellings in the form of two pairs of semi-detached dwellings (2x3 and 2x2 bed) with revised landscaping and parking arrangements. Notably, the scale of the dwellings has been reduced and they have been set back so the front building line does not protrude beyond the front elevation of No.24 Beacon Close. During the assessment of the application the applicant submitted a landscaping drawing which demonstrates that the frontages would no longer be heavily dominated by hard surfacing. Finally, the applicant has submitted a Unilateral Undertaking (UU) which demonstrates that future occupiers of the development will not be eligible to join the local parking management scheme. It is officer's opinion that the changes set out above and described in further detail within this report have satisfactorily addressed the concerns which previously formed the reasons for refusal.
- 1.4 During the process of the application, a petition in objection to the development was received.
- 1.5 The main issues which shall be addressed within this Committee Report relate to the intensification of the site and the impact on the character and appearance of the street scene.
- 1.6 The Committee Report seeks to provide a comprehensive assessment of the full application and supporting documentation. All material planning considerations have been considered.

Hillingdon Planning Committee – 5th September 2024

PART 1 – Members, Public & Press

2 The Site and Locality

- 2.1 The application site is approximately rectangular in shape and is located at the southern end of Beacon Close, which is a cul-de-sac. It comprises a detached, single storey dwelling with a hipped roof and a detached garage with a flat roof located to the north-east of the dwelling. There is a vehicle crossover to the front of the detached garage which emerges onto Beacon Close. There is a second vehicle crossover along the north-western site frontage. Parking on Beacon Close is restricted by a single yellow line and is controlled by Car Parking Zone U8 which restricts parking to allow only permit holders to park within the street parking bays Mondays - Fridays between the hours of 9am to 5pm.
- 2.2 The street scene on Beacon Close is characterised by detached dwellings set-back from the highway. To the west of the site is number 24 Beacon Close, an adjacent detached bungalow that has been extended by single storey side and rear extensions and a rear dormer. To the east of the site, and sited on substantially higher grounds, are the backland developments at numbers 213B and 213C Harefield Road.
- 2.3 According to the Council's GIS, the site is designated within the Colne Valley Archaeological Priority Zone, Northolt RAF- 3km Buffer Zone and an Air Quality Management Area. A section of the highway to the front of the site falls within a Surface Water Flood Zone, although the application site itself does not fall within this designation. The site is in Flood Zone 1 and has a Public Transport Accessibility Level (PTAL) of 1b (poor).

Figure 1: Location Plan (application site edged red)

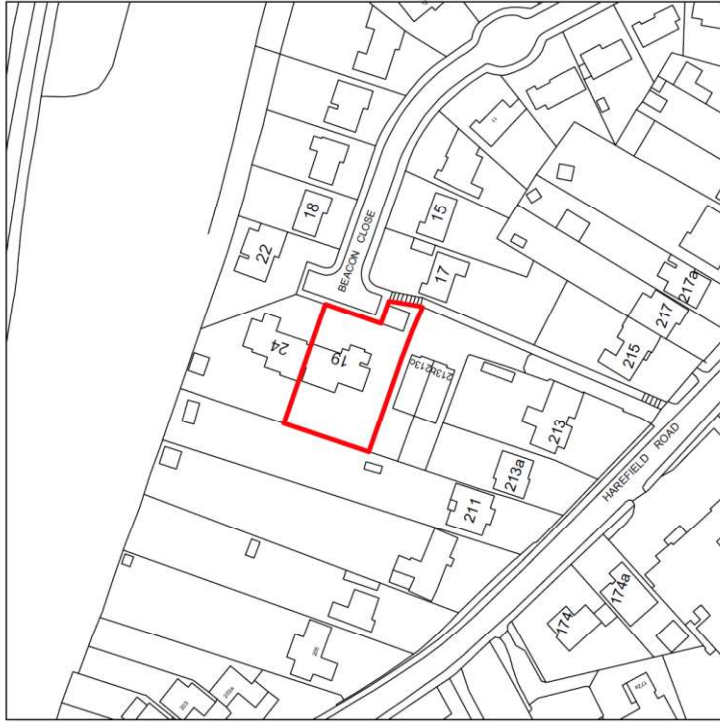


Figure 2: Street View Image of the Application Property



3 Proposal

- 3.1 The application proposes the demolition of the existing bungalow and erection of two pairs of semi-detached dwellings to provide 2 x 3-bedroom dwellings and 2 x 2-bedroom dwellings plus associated hard and soft landscaping. The existing bungalow and its associated detached garage would be demolished.
- 3.2 All 4 dwellings would be market sale properties.
- 3.3 During the process of the application revised drawings were sought and received to amend the front landscaping and parking arrangements and the applicant has provided a Biodiversity Net Gain Assessment.

Figure 3: Proposed Plans (please note – larger version of plan can be found in the Committee Plan Pack)

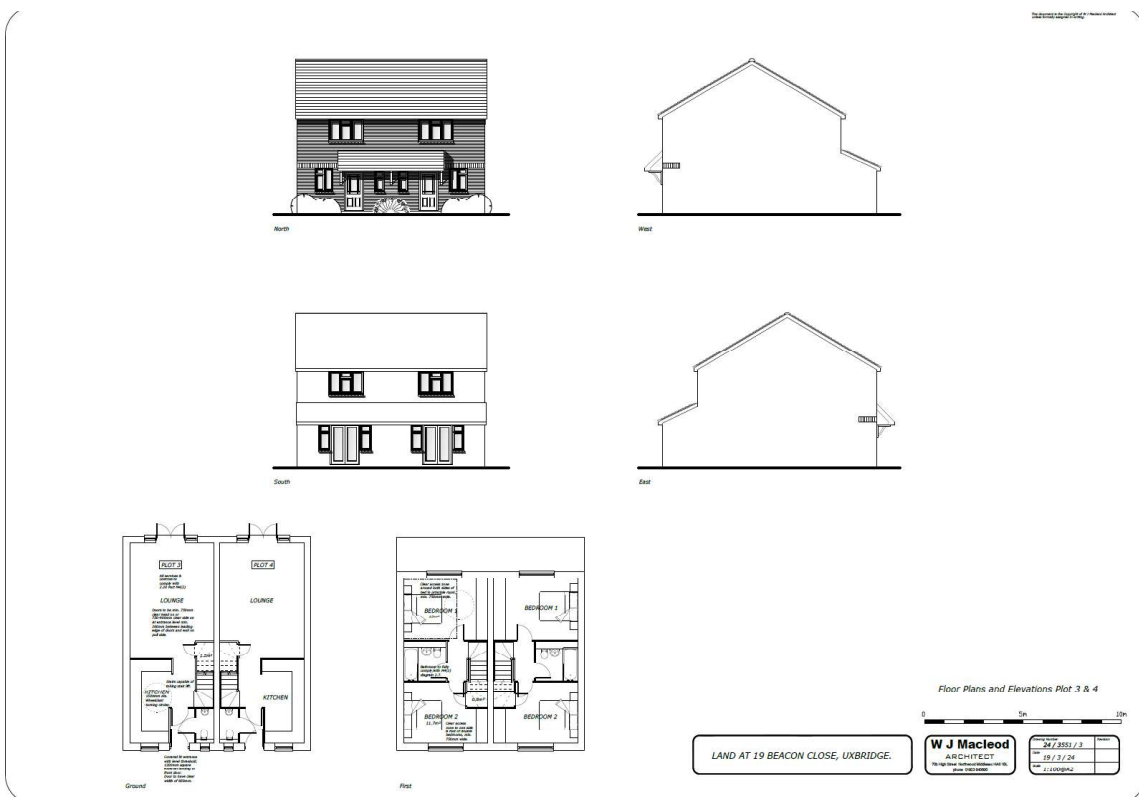
Proposed Site Plan



Proposed Plans Plot 1 and Plot 2



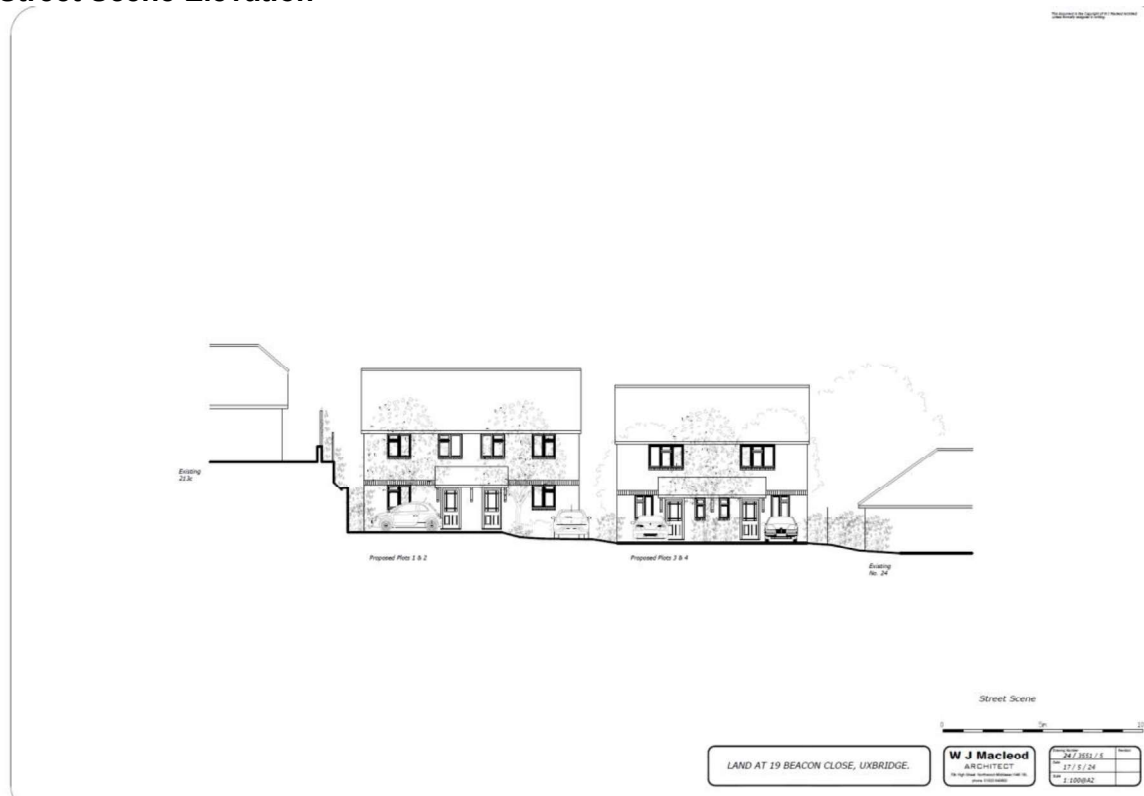
Proposed Plans Plot 3 and Plot 4



Hillingdon Planning Committee – 5th September 2024

PART 1 – Members, Public & Press

Street Scene Elevation



4 Relevant Planning History

- 4.1 A list of the relevant planning history related to the property can be found in Appendix 2.
- 4.2 The most recent application at the site was refused permission under planning ref: 17969/APP/2023/1014 for the erection of four terraced dwellings incorporating landscaping, parking provision, waste and cycle stores following demolition of existing dwelling.
- 4.3 The application was refused at the Borough Planning Committee in February 2024 for the following reasons:
 - 4.4 1. The proposed development, by reason of the number of dwellings being proposed, their terraced form, layout and siting, would result in an incongruous form of overdevelopment of the site that would fail to harmonise with the existing local context and prevailing pattern of development on Beacon Close. The principle of intensifying the residential use of the site as proposed, would have a detrimental impact on the street scene and character and appearance of the area as a whole. The proposal is therefore detrimental to the visual amenity and character of the surrounding area and contrary to Policy BE1 of the Hillingdon Local Plan: Part One - Strategic Policies (2012), Policies DMH 6, DMHB 11 and DMHB 12 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020), Policies D3 and D4 of the London Plan (2021) and Chapter 12 of the National Planning Policy Framework (2023).

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- 4.5 2. The proposed development, by reason of its forward projection beyond the established front building line in this section of Beacon Close, plot width, depth, massing, bulk, scale and design, would fail to harmonise with the character and architectural composition of surrounding properties, appearing as an awkward, incongruous and cramped form of development which would be detrimental to the visual amenity of the street scene and harmful to the character and appearance of the surrounding area. The proposal would therefore be contrary to Policy BE1 of the Hillingdon Local Plan: Part One - Strategic Policies (2012), Policies DMH 6, DMHB 11 and DMHB 12 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020), Policies D3 and D4 of the London Plan (2021) and Chapter 12 of the National Planning Policy Framework (2023).
- 4.6 3. The proposed development, by virtue of the excessive area of hard standing for the on-site parking provision, negligible soft landscaping and tree planting, removal of the existing front boundary treatment and number and site coverage of associated requirements for cycle stores and refuse bins in the front gardens, would erode and be out of keeping with the existing pleasant, spacious and suburban character of the site and the surrounding street scene on Beacon Close. The proposal would introduce an urbanising form of development that would cause harm to the open and verdant character and appearance of the area. The proposal would therefore have a harmful impact upon the character, appearance and visual amenities of the area, contrary to Policy BE1 of the Hillingdon Local Plan: Part One - Strategic Policies (2012), Policies DMH 6, DMHB 11, DMHB 12 and DMHB 14 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020), Policies D3, D4 and G7 of the London Plan (2021) and Chapter 12 of the National Planning Policy Framework (2023).
- 4.7 4. In the absence of a legal agreement to cover the required highways work to enable the proposed vehicle crossovers and on-site car parking provision and preventing the issuing of parking permits to future occupiers of the proposed dwellings which are located within a permit-controlled area, the proposed development is likely to lead to an increase in pressure for on-street parking and have a consequent adverse effect on highway safety, through inconsiderate and potential hazardous parking and a risk to road users. The proposal would therefore be contrary to Policy DMT 1, DMT 2 and DMT 6 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), Policies T4, T6 and T6.1 of the London Plan (2021) and paragraph 115 of the National Planning Policy Framework (2023).
- 4.8 The main issue for the current application is whether these reasons for refusal have been overcome.
- 4.9 This current application is a re-submission of the refused scheme. The application seeks to overcome the above reasons for refusal. The main change within this application is the development has been split into two sets of semi-detached properties as opposed to one set of terraced properties with changes to the front landscaping and parking arrangements.

4.10 Overall, as discussed within the relevant sections of this Committee Report, the proposed development is considered to have overcome all the previous reasons for refusal.

5 Planning Policy

5.1 A list of planning policies relevant to the consideration of the application can be found in Appendix 3.

6 Consultations and Representations

6.1 9 neighbouring properties and North Uxbridge Residents Association were consulted on the application by letter dated 16-04-24. The consultation period expired 08-05-24.

Internal and external consultations were also sent out and a summary of the comments received are noted below in Table 2 of this Committee Report.

6.2 Representations received in response to public consultation are summarised in Table 1 (below). Consultee responses received are summarised in Table 2 (below). Full copies of the responses have also separately been made available to Members.

Table 1: Summary of Representations Received

Representations	Summary of Issues Raised	Planning Officer Response
A petition of 22 valid signatures has been received against the application	1. Gross Overdevelopment of the site.	Discussed at paragraph 7.6 of this report.
	2. Design is not consistent with the street scene.	Discussed at paragraph 7.13 of this report.
7 individual letters of objection have been received	I. Disturbances due to building work.	Noise and disturbance of building work are governed under separate legislation and is not a material planning consideration. As such this matter has not been discussed further.
	II. Loss of privacy and views.	Discussed in paragraphs 7.26 to 7.30 of this report.

	III. Decrease in property values.	This matter is not a material planning consideration. As such this matter has not been discussed further.
	IV. Increase in flooding and blocked drains.	Discussed at paragraphs 7.67 and 7.68 of this report.
	V. Loss of suburban garden view that is out of character with the surrounding area.	Discussed at paragraph 7.16 of this report.
	VI. Traffic and Parking Concerns.	Discussed at paragraph 7.38 of this report.
	VII. Overdevelopment of the site.	Discussed at paragraph 7.6 of this report.
	III. Impact wildlife, trees and biodiversity	Discussed at paragraph 7.57 and 7.65 of this report.
	IX. Semi-detached properties are out of character with the detached properties in the area.	Discussed at paragraph 7.16 of this report.
	X. There are no social, community or aesthetic benefits.	The proposed development would provide two new family sized units (3 bed properties) and two additional two bed properties which would make a contribution towards the Borough's needs for family sized housing.
	XI. Similar schemes have been refused by the council previously on the plot.	The proposed development differs from previously refused applications in terms of its design, site layout and a

		full assessment has been made within this report.
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Table 2: Summary of Consultee Responses

Consultee and Summary of Comments	Planning Officer Response
Statutory Consultation	
Ministry of Defence:	No objection
Greater London Archaeological Advisory Service (GLAAS):	GLAAS were consulted on the application as the previous applications had them listed as a consultee. However, they did not consider it necessary to be consulted and left no further comment.
Access Officer: This latest proposal for the erection of four terraced houses with landscaping and parking has been reviewed with reference to London Plan Policy D7. No accessibility concerns are raised subject to conditions pertaining to the submission of step free access details, and compliance with the relevant M4 standards.	The comments from the access officer are noted and the relevant conditions will be added to the decision notice.
Highway Officer: The application site has a PTAL ranking of 1b indicating access to public transport is poor compared to London as a whole, suggesting that few opportunities for trips to be made to and from the application site by modes other than the private car would be available. However, it should be noted that the site is located within 10 minutes walking distance the Uxbridge Metropolitan Town Centre which benefits from a PTAL rating of 6. The Town Centre accommodates a train station benefiting from access to two different train lines and a bus station which accommodates multiple bus links as well as accommodating town centre infrastructure such as retail, community space and public open space.	

<p>Access The application site has 2no. vehicle crossovers, with 1no. single crossover serving the existing dwelling and 1no. double crossover serving the existing garage located on the eastern side of the turning head adjacent to the stepped adopted footpath that links Beacon Close to Harefield Road.</p> <p>Drawing 24/3551/1 titled Land At 19 Beacon Close, Uxbridge shows the proposed site layout which provides a single vehicle crossover to Plot 4, extends the existing vehicle crossover to serve Plot 2 and Plot 3 and provides vehicular access to parking spaces for Plot 1 over the existing vehicle crossover to the garage which would be removed.</p> <p>The shared vehicle crossover to Plot 2 and Plot 3 should concur with DVFC 4.2.2 Shared Crossover/Shared Access.</p> <p>Removal of the garage to provide parking for Plot 1 may undermine the structure supporting the steps to the adjacent adopted footpath which would not be acceptable. Further details would be required, and The Structural Engineer should be consulted.</p> <p>Pedestrian visibility splays of 2.4m x 2.4m in which there is no obstruction over 0.6m are required.</p> <p>Car Parking Drawing 24/3551/1 titled Land At 19 Beacon Close, Uxbridge which is the site plan illustrates a total of 6no. car parking spaces for the development which would be in accordance with the London Plan maximum standard and would be acceptable. The applicant should be advised that the Highway Authority would require that the applicant enter a legal agreement that prohibits future occupants of the proposed development from applying for a permit to join any parking management scheme in the vicinity of the application site.</p> <p>Electric Vehicle Charging Points (EVCP's) Drawing 24/3551/1 titled Land At 19 Beacon Close, Uxbridge shows 1no. EVCP for each dwelling which would be acceptable but should amended to show 1no. active 7Kw EVCP for each dwelling which should be conditioned.</p>	<p>Noted and a condition pertaining to the submission of a structural survey to be attached to the decision notice.</p> <p>Noted and conditioned.</p> <p>Noted and a Unilateral Undertaking has been submitted to the Council for review. The UU demonstrates that the applicant agrees to prohibit future occupiers from applying to join the local PMS.</p> <p>Noted and conditioned</p>
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<p>Cycle Parking Drawing 24/3551/1 titled Land At 19 Beacon Close, Uxbridge, indicates that each of the dwellings would benefit from 2no. cycle parking spaces however, the locations are remote from the dwellings and are accessed through narrow gated pathways of approximately 0.8m wide and bound to both sides which would not be acceptable.</p> <p>Conclusion The Highway Authority are satisfied that the proposal would not discernibly exacerbate congestion or parking stress and would not raise any measurable highway safety concerns and would therefore offer no objection to the application subject to conditions pertaining to the delivery of EVCP provision, further cycle storage details and the submission of a legal agreement to prohibit future occupiers from joining the parking management scheme.</p>	<p>Noted by Officers and a response is set out in paragraph 7.42 of this committee report.</p>
<p>Environmental Specialist: It is considered that a biodiversity net gain solution is capable of being secured for the development and therefore reverting to the statutory pre-commencement condition would be acceptable. However, the solution presented remains unacceptable due to the unreasonable demands placed on a future occupier to retain a relatively high-grade habitat type within the useable private curtilage of the property.</p> <p>A future BNG solution will need to clearly set out how habitat will be managed to retain its distinctiveness. The higher the level of distinctiveness, the more management will be required.</p> <p>It is therefore advisable that any landscaping plans are not part of any subsequent conditional approval as these may change with a future approach to BNG.</p>	<p>The comments made by the Ecology Specialist are noted and the relevant conditions have been added to the decision notice.</p>

7 Planning Assessment

Principle of Development

New Housing and Garden Land Development

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PART 1 – Members, Public & Press

- 7.1 The proposed development would be built upon garden land following the demolition of the existing dwelling. As such, Policy DMH 6 of the Hillingdon Local Plan (2020) would be relevant which states ‘there is a presumption against the loss of gardens due to the need to maintain local character, amenity space and biodiversity’.
- 7.2 Although located on garden land, the principle for residential use has been established through the existing dwelling. Therefore, the intensification of the site and housing mix must be assessed in accordance with Policy DMH 6. The loss of garden land for development will be strongly resisted unless applicants can demonstrate that the development proposal does not present a departure from the existing pattern of development, is designed to compliment the character and appearance of the area, would not result in significant harm to neighbour amenity or the local highway network.
- 7.3 The previous application included the provision of four dwellings in terraced form. This current application has amended the design to include 2 sets of semi-detached properties creating spaces between the properties to maintain the openness of the area. As set out within the character and appearance section of this committee report, the design of the proposal is considered to have an acceptable impact in terms of its impact on local character and would have an appropriate area of amenity space for future occupiers. The impact on existing ecology, trees and biodiversity has been discussed in more ecology and biodiversity sections of this report.
- 7.4 Policy H10 on the London Plan (2021) and Policy DMH2 of the Hillingdon Local Plan (2020) relate to housing mix and the need for family sized housing. The proposed development would provide four new dwellings (2 x 2 bed and 2 x 3 bed) with a net gain of three dwellings. The development would provide 2 family sized dwellings (a net increase of 1) which would make a contribution towards the Boroughs needs for family sized housing. Accordingly, it is considered that the proposal would consist of appropriate mix.
- 7.5 Overall, the principle of the proposed development is accepted and complies with the relevant planning policies. These policies can be read in full in the Committee Report Part 3 - Policy Appendix.

Density

- 7.6 Numerical densities are more appropriate to larger sites and what is considered of greater significance to the determination of this application is the local contextual factors. The key consideration is therefore whether the development would acceptably integrate with the character and appearance of the area, and would respect residential amenity considerations, rather than the consideration of the numerical density of the proposal.

Design / Impact on the Character and Appearance of the Area

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- 7.7 Policies D1, D3 and D4 of the London Plan (2021) require development proposals to be of high quality and to enhance the local context by delivering buildings and spaces that positively respond to local distinctiveness
- 7.8 Policies BE1 of the Hillingdon Local Plan Part 1 – Strategic Policies (2012), DMHB 11, DMHB 12 and DMHD 1 of the Hillingdon Local Plan Part 2 – Development Management Policies (2020) in summary seek to secure a high quality of design that enhances and contributes to the area in terms of form, scale and materials, is appropriate to the identity and context of the townscape and would improve the quality of the public realm and respect local character. These aims are also supported by Chapter 12 of the NPPF (2023).
- 7.9 Beacon Close is comprised of wide detached properties set-back from the highway by front drives and front gardens. In terms of architectural style, the properties along Beacon Close are characterised with gable end roofs, externally finished mostly with brickwork on ground level and cladding at first floor level. The site frontages of the existing properties are typically characterised with low level boundary treatment with a mixture of hard and soft landscaping with space for off-street car parking.
- 7.10 At the turning head of Beacon Close, are two bungalows at numbers 19 Beacon Close (the application site) and 24 Beacon Close. Directly east of the application site lies numbers 213B and 213C Harefield Road, a pair of semi-detached properties approved under application reference 59140/APP/2011/1113.
- 7.11 At present, the application site is characterised by a bungalow property with a low-level brick wall and soft landscaped front garden. There is an area of hard surfacing along the western boundary of the application site for a single-width driveway served by an existing crossover. In the northeastern corner of the plot lies an existing garage structure served by a vehicle crossover.
- 7.12 Beacon Close is a cul-de-sac which features a reasonably uniformed front building line despite the curved nature of the road and comprises of mainly two storey detached dwellings. Whilst the front buildings lines are reasonably aligned, the rear buildings line have become distorted which is mainly due to the construction of residential extensions. Whilst not specifically within Beacon Close, there is an examples of back land development constructed on a site immediately adjacent to the application site which benefitted from permission to construct semi-detached dwellings accessed off Harefield Road. Furthermore No.2F Beacon Close was subject to a planning application for the construction of an attached 3-bedroom house thus forming a semi-detached pair of dwellings. As such it is considered that developments consisting of semi-detached dwellings do contribute to the character and appearance of the area.
- 7.13 This application has been submitted in response to the previous refusal of application reference 17969/APP/2023/1014 and involves the demolition of the existing bungalow and the erection of two sets of semi-detached properties comprising 2 x 2bed and 2 x 3 bed dwellings. In forming reasons for refusal 1 and 2 which relate to the design of the development, concerns were raised with

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regards to the provision of a block of 4 dwellings which formed a terrace. As terraces are uncommon within the immediate context it was considered that the development would result in a departure from the character and appearance of the area and that the design was led by a desire to over intensify the use of the property rather than a development which complimented and enhanced the local character.

- 7.14 In seeking to respond to the concerns raised regarding the proposal of a terraced block, the revised proposal seeks consent for two pairs of semis. The dwellings have been reduced in scale, bulk and mass to create additional space around each pair and would be set apart by a separation distance of 2 metres.
- 7.15 In determining the previous application Officers raised concern with the location of the proposed block as it stepped forward of the front building line at No.21 Beacon Close. The application site is located at the end of the cul-de-sac meaning that the established building line is formed by the two adjacent properties only which are No.24 Beacon Close and No.213c and 213b Harefield Road which a set of semis constructed on a backland development site. The existing dwelling is similar in scale bulk and mass to No.24 Beacon Close and features a small front projection towards the western elevation. The proposed site plan illustrates that the dwellings would be set back further so that they do not protrude beyond the front elevation of No.21 Beacon Close and would be set behind the front elevation of No's 213b and 213c Harefield Road. As such Officers consider that the revised proposal would not result in a departure from the existing pattern of development as shown in figure 3. Furthermore it should be noted that the front building line of No.22 sits well behind the front building line of No.20 Beacon Close meaning that the established building lines within this part of the cul-de-sac are restricted to the to adjacent sites only and in the event that the set back of the front elevation of plots 1 and 2 behind 213c and 213b Harefield Road is no different to the relationship between No.22 and No.20 Beacon Close. As such Officers consider that the revised design responds to the concerns raised when refusing the previous applications.
- 7.16 This proposal is revised in terms of the bulk, scale, mass and general appearance of the proposed dwellings. Although the properties along Beacon Close are detached in nature, they appear very wide as many have benefitted from large extensions and have the appearance of being semi-detached. The proposed dwellings would appear similar in size and design to the surrounding properties and as such would integrate with the character and appearance of the street scene. Furthermore, the elevations indicated that design features from the surrounding properties have been adopted when designing the proposed dwellings. Notably most of the dwellings which form the street scene are two storeys in height characterised by gable ended and tiled roofs, a mixture of brick and tile hung front facades and generous sized front and rear gardens. As referred to earlier in this report, most properties also benefit from various side and rear additions as well as porches some of which are reasonably large in scale. The proposed elevations illustrated the dwellings are of a similar scale, bulk and mass to the surrounding properties, they have been designed with gable ended, tiled roofs, would feature centralise canopies over the front entrance doors and

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additional soft landscaping is proposed to the front which is a similar design feature to the other properties within the street scene. Although there would be an uplift of 3 new residential units on the site, the plot is large and reduction in the scale bulk and mass would result in a development which fits comfortably within its context. The unit sizes (number of occupiers) have decreased from the previous application, the design has been improved and the landscaping has been altered. As such, it is not considered that the proposed development would be an overdevelopment of the site

- 7.17 The proposed development would maintain adequate separation distances to the surrounding properties. The dwellings would be set in from the side boundaries by 1m and the two sets of semi-detached properties would have a separation gap of approx. 2m. Given the site levels, the building ridge heights would be staggered, forming a bridge between number 213C Harefield Road and 24 Beacon Close allowing views beyond the dwellings to the south. The building lines of the development would not break the building line of Beacon Close. It is considered that the proposed development would fit comfortably within the plot.
- 7.18 During the process of the application, revised drawings were sought to reduce the width of the front canopy features. It is now considered that the proposed design of the properties would fit in with the character and appearance of the street scene. A condition has been added to the decision notice requiring the sample of materials.
- 7.19 In terms of landscaping, the previous application included a refusal reason due to the 'excessive area of hard standing, negligible soft landscaping and tree planting, removal of the existing front boundary treatment and number and site coverage of cycle stores and refuge stores.' The proposed development has revised the front landscaping with the relocation of the cycle stores to the rear gardens to maintain the local character. The level of tree planting, hedges and planting beds has been increased with the introduction of front lawns to break up the parking areas. It is considered that the proposed front landscaping would fit in with the character and appearance of the street scene given the surrounding properties have a mixture of hard and soft landscaping. The application is supported by an Arboricultural Implications Report which confirms that no trees of high landscape or biodiversity value are to be removed as part of the development. The proposed removal of individuals and groups of trees will represent no alteration to the main arboricultural features of the site, only a minor alteration to the overall character of the site and will not have an adverse impact on the arboricultural character and appearance of the local landscape.
- 7.20 Overall, the proposed design of the development would fit in with the character and appearance of the street scene. The proposed massing and layout is comparable to the majority of dwellings along Beacon Close and retains space and views around the development. The proposed front landscaping has been improved; however, the provision of a detailed landscape plan has been conditioned. It is considered that the proposed development has sufficiently overcome refusal reasons 1, 2 and 3 of the previous refused application reference 17969/APP/2023/1014.

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- 7.21 It is considered that the proposed development would comply with the overarching aims of Policy BE1 of the Hillingdon Local Plan: Part One - Strategic Policies (2012), Policies DMHB 11 and DMHB 12 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), Policies D3 and D4 of the London Plan (2021) and the NPPF (2023).

Heritage

- 7.22 The site lies in the Colne Valley Archaeological Priority Area which contains archaeological remains dating from the prehistoric to post-medieval periods. The Greater London Archaeological Advisory Service were consulted on the application and acknowledged receipt of the consultation request. No further response has been received and the consultation period has now expired.
- 7.23 On this basis, it is considered that the proposed development would not cause harm to heritage assets of archaeological interest. The proposal would therefore accord with Policy DMHB 7 of the Hillingdon Local Plan (2020), Policy HC1 of the London Plan (2021) and the NPPF (2023), in this respect.

Impact on the Green Belt

- 7.24 The application site does not lie within the Green Belt.

Residential Amenity

- 7.25 The previously refused scheme was not considered to have a significant harmful impact on the neighbouring properties through a loss of light and outlook (reference 17969/APP/2023/1014) This remains the case with this current scheme. An assessment of the potential impact on neighbouring residential amenity is discussed as follows.
- 7.26 Number 24 Beacon Close is a detached bungalow located to the west of the application site. Plot 4 would be the closest of the proposed properties to this neighbour. It would be sited approx. 1m from the side boundary and be two stories in nature. The dwelling would contain no ground or first floor flank windows which would face this neighbour and as such there would be no harmful level of overlooking or loss of privacy. The proposed ground floor would extend marginally beyond the single storey rear extension at No.24; however, it is not considered that this would cause any harmful loss of light or overshadowing.
- 7.27 It is acknowledged that the proposed first floor rear windows would provide oblique views into the rear gardens of No.24 Beacon Close and Nos. 213B and 213C Harefield Road and long views towards the end of the rear gardens of the properties fronting Harefield Road. However, a mutual degree of overlooking between first floor windows and rear gardens of neighbouring properties already exists, which is not uncommon in suburban locations such as this. It is therefore considered that the proposed first floor rear windows would not result in such a material loss of privacy as to warrant a reasonable ground for refusal on this basis.

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- 7.28 Numbers 213B and 213C Harefield Road are two-storey properties located to the east of the application site. These neighbouring properties sit on substantially higher ground than the application site. The rear building line of the proposed dwelling on Plot 1 would not breach the 45-degree line of sight taken from the nearest first floor rear habitable window at No. 213C Harefield Road. There would be ground and first floor flank windows within the dwelling at plot 1, however due to the high ground position of no.213C, there would be no level of overlooking or loss of privacy. Taking these factors into account, and the south facing aspect of Nos. 213B and 213C, it is considered that the proposal would not result in an unreasonable loss of light, outlook or sense of enclosure for these neighbouring occupiers and their associated private amenity space.
- 7.29 Plot two would include the provision of two ground floor flank windows and one upper floor flank window facing Plot 3. However, as Plot 3 does not contain any flank windows there would be no level of overlooking or loss of privacy.
- 7.30 Having regard to the above, it is considered that the proposed development would not unduly impact on the living conditions of neighbouring occupiers. It would therefore comply with Policy DMHB 11 part B) of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

Quality of Residential Accommodation (Internal and External)

- 7.31 Regarding internal accommodation, Policy D6 of the London Plan (2021) sets out the requirements for the gross internal floor area of new dwellings at a defined level of occupancy. Table 3.1 of the London Plan (2021) set out the same gross internal area space standards set out in the technical housing standards - nationally described space standard (2015). Policy DMHB 16 of the Local Plan (2020) Aligns with this policy.
- 7.32 The proposed development comprises 4 new dwellings (2x2 bed and 2x3 bed) The proposed 3bed semi-detached properties would have a GIA of approx. 102.5sqm and would house up to 5 people.
- 7.33 The proposed 2bed semi-detached properties would have a GIA of approx. 79sqm and would house up to 4 people. All properties would meet the internal space standards of the London Plan (2021). It is considered that all habitable rooms would have an adequate source of light and outlook. As such the proposed development complies with Policy D6 of the London Plan (2021) and Policy DMHD 16 of the Local Plan (2020).
- 7.34 With regard to external amenity space, Policy DMHB 18 of Local Plan (2020) states that all new residential development and conversions will be required to provide good quality and usable private outdoor amenity space. Amenity space should be provided in accordance with the standards set out in Table 5.3, which requires houses with two and three bedrooms to provide a minimum of 60 square metres of private genuinely usable amenity space.

- 7.35 Plots 1 and 2 would have approx. 70sqm of rear garden space each and plots 3 and 4 would have approx. 64sqm each complying with Policy DMHB 18. The proposal would provide the future occupiers of the proposed dwellings with external private amenity space provision that is of a sufficient size, usability and functionality, in accordance with Policy DMHB 18 of the Hillingdon Local Plan (2020).

Highways and Parking

- 7.36 The application site is located at the end of the turning head on Beacon Close, a residential cul-de-ac with 30mph speed limit which is subject to a single yellow line parking restrictions Monday-Saturday between 8am and 6:30pm. An adopted stepped footpath runs from the northeastern corner of the turning head to Harefield Road.

Access

- 7.37 The application site has 2no. existing vehicle crossovers with one serving the existing dwelling and one serving the existing detached garage located on the eastern side of the turning head adjacent to the stepped footpath. Drawing no 24/3551/1 Rev A shows the proposed site layout which shows an additional crossover to serve Plots 2 and 3. Upon review of the proposal the Local Highway Authority requested that the crossover to the front of x be located at least 1 metre away from the existing lamppost column. In response to this drawing reference 24/3551/1 Rev A illustrates that the crossover is located 1 metre away from the lamp column which complies with The London Borough of Hillingdon Domestic Vehicle Footway Crossover policy (DVFC) 2022 4.9 Street Furniture and Traffic Calming document. Subject to the submission of a plan indicating pedestrian visibility splays of 2.4m x 2.4m in which no obstruction over 0.6m can be implemented which will be secured by way of condition, no objections are raised to the provision of new crossovers to serve the proposed development.

Parking

- 7.38 The London plan table 10.3-Maximum Residential Parking Standards allows dwellings in outer London with a PTAL of 1b to have a maximum of 1.5 spaces per dwelling. The proposed development would provide 2no car parking spaces each for the 3-bedroom dwellings and 1no car parking spaces each for the 2-bedroom dwellings which would be in accordance with the London plan maximum standards. Plot 2 would benefit from a tandem parking space, whilst this is not the ideal parking situation, the property would be for one single family, and it is considered that the need for 2 parking spaces for the 3-bed dwelling in a PTAL ranking area of 1b outweighs the negatives of the tandem parking space. On balance the layout of car parking spaces is considered acceptable.
- 7.39 As per the Local Highway Authority comments, due to the limited provision of off-street parking and prevention of parking stress in the local area it is necessary to restrict future occupiers of the dwellings entering parking permit schemes. A Unilateral Undertaking has been drafted and will be reviewed by the Councils

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Legal Team. The Unilateral Undertaking will need to be submitted and reviewed before the decision of the application is released. Although yet to be signed, the provision of the agreement in draft form is adequate demonstration that the applicant will agree to a restriction on future occupants joining the parking management schemes in the local area.

Electric Vehicle Charging Points

- 7.40 The published London Plan (2021) required that 20% of car parking spaces have active Electric Vehicle Charging Points (EVCP) and 80% of spaces have passive EVCPs. Drawing 24/3551/1 Rev A shows 1no. EVCP for each dwelling which would be acceptable, however this should be amended to show 1no active 7Kw EVCP. This is to be secured by condition.

Cycle Parking

- 7.41 The Published London Plan (2021) Table 10.2 Maximum Cycle Parking Standards requires dwellings with two or more bedrooms to have a minimum of 2no. cycle parking spaces which are shown on Drawing 24/3551/1 Rev A. The cycle storage is located within the rear gardens accessed by individual paths. It is considered that the number of cycle storage is acceptable for the development.
- 7.42 It is noted that the Highway Authority has concerns regarding the location of the cycle storage to the rear of the properties due to the 0.9m wide footpaths. However, from reviewing the revised block plan showing revised landscaping and parking arrangements it is considered that the cycle storage located in the rear gardens of the dwellings form a better design compared to a front garden position. If the cycle storage were to be located to the front of the dwellings it would require additional hard surfacing which was raised as a design concern when refusing the previous application and cycle storage in front gardens is not a common feature within the street scene. In fact, the location of cycle parking within rear gardens is a common arrangement for residential properties. Therefore, it is considered on balance an acceptable arrangement. A condition has been added to the decision requiring the details of the cycle storage including their dimensions, materials and location.

Other Matters

- 7.43 The application is bounded by a public right of way which adjoins the western boundary. As there is a land level change which slopes away as you travel east, steps are located towards the bottom of the footpath. In assessing the proposals, the Local Highway Authority have suggested that a structural survey is undertaken to ensure the structural integrity of the wall is not compromised during the construction of the development. A condition pertaining to the submission of a structural survey is to be attached to the decision notice.

Conclusion

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- 7.44 The Highway Authority have been consulted on the application and are satisfied that the proposal would not discernibly exacerbate congestion or parking stress and would not raise any measurable highway safety concerns and officer no objection to the application subject to certain conditions.

Noise

- 7.45 Policy D14 of the London Plan (2021) requires that proposals minimise noise pollution and Policy EM8 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012) promotes the maximum possible reduction in noise levels and seeks to ensure that noise impacts can be adequately controlled and mitigated.
- 7.46 The site would be used in an exclusively residential capacity. As such, in terms of the operational phase of the proposed development, no significant issues are considered to be raised by the proposal, in respect to noise. A condition has been added requiring the submission of a Construction Management Plan to minimise noise and other emissions caused during the construction phase as far as practicable.

Air Quality

- 7.47 Local Plan Policy DMEI 14 states:
Policies SI 1 of the London Plan (2021), EM8 of the Hillingdon Local Plan: Part 1 (2012) and DMEI 14 of the Hillingdon Local Plan: Part 2 (2020) are all directly relevant to the proposal. These policies can be read in full in the Committee Report Part 3 - Policy Appendix, and in summary, seek to safeguard and improve air quality to protect existing and new sensitive receptors. These aims are also supported by the NPPF (2023) at Chapter 15.
- 7.48 The development site is located within an Air Quality Management Area. The proposal would result in 3 additional homes at the site, their occupiers would have access to 6 car parking spaces with 4 being served with active vehicle charging points (covered by way of a condition). Whilst the development would result in a net increase in trip generation, the number of trips would be de minimis and therefore it would not be justifiable to require mitigation be secured by way of condition. Notwithstanding this point given the constraints of the site and its located within a cul-de-sac; the applicant will need to submit a Construction Management Plan to minimise air and other emissions caused during the construction phase. This condition is recommended to be added to the decision notice.

Accessibility

- 7.49 Policy D5 of the London Plan (2021) seeks to ensure development proposals achieve the highest standards of accessible and inclusive design. Policy D7 of the London Plan (2021) requires at least ten percent of dwellings to meet Building Regulation requirement M4(3) 'wheelchair user dwellings', with all other dwellings meeting Category M4(2) 'accessible and adaptable dwellings'.

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7.50 The Councils Access Officer has been consulted on the application and has no objection to the proposed development in terms of accessibility subject to conditions pertaining to step free access which have been added to the decision notice.

Trees and Landscaping

7.51 An Arboricultural Impact Assessment has been submitted with the proposed development. The submitted Arboricultural Report indicates that five trees would be felled, all being 'Category C' specimens. In principle, the loss of these low value trees is acceptable. However, consideration also needs to be given to the proposed landscape scheme, and replacement tree planting.

7.52 The proposed development would include landscaping works to the front of the site to include new areas of hardstanding for vehicle parking and soft landscaping. The previous refused application states that 'the proposal would still introduce an urbanising form of development that would not integrate well with its surroundings. The proposed replacement trees and landscaping would not mitigate the harm caused to the character and appearance of the area.'

7.53 The current application seeks to overcome the previous refusal reason by a revised front landscaping scheme. As discussed within the Character and Appearance section of the report, the proposed design of the development would not cause harm to the character and appearance of the street scene and would integrate within the surrounding properties. The level of hard surfacing has been reduced with the introduction of front lawn areas and trees to integrate with the surrounding properties. Subject to a comprehensive landscape plan which has been conditioned it is considered that the proposed development would comply with Policy DMHB 14 of the Hillingdon Local Plan (2020) and has sufficiently overcome refusal reason 3 of planning reference 17969/APP/2023/1014.

Ecology

7.54 The site is in proximity to The Alder glade Nature Reserve and Frays Farm, which is a designated Nature Conservation Site of Local Importance. It is therefore considered that the site is located within habitat that matches that where bat roosts have previously been found in the Borough.

7.55 A Preliminary Ecology Appraisal and Preliminary Roost Assessment has been submitted with this application. The report methodology includes a desk top survey and walkover survey to determine the potential presence of protected and notable species. This concluded that the site was not suitable for, or no evidence was documented of, a number of notable species.

7.56 In respect to bats, due to the small size of the site, small number of trees and better-quality habitats at Alder glade Nature Reserve and Frays Farm, the site was determined to be of limited value to foraging and commuting bats. In respect to opportunities for bat roosts, the main dwelling was concluded to be of low potential with limited entry points into the loft (for example lifted roof tiles) and no

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evidence of bat activity found in the loft space (for example droppings, feeding remains or urine staining). The garage and shed on site were considered to have negligible potential for roosting bats due to the roofing felt on these structures being in good condition and tightly sealed.

- 7.57 The proposal satisfactorily demonstrates that there would be no harm to protected species and their habitats. The paragraphs below discuss issues relating to biodiversity.

Biodiversity Net Gain

- 7.58 In England, Biodiversity Net Gain (BNG) is mandatory under Schedule 7A of the Town and Country Planning Act 1990 (as inserted by Schedule 14 of the Environment Act 2021). It became mandatory for major developments on 12 February 2024 and small sites on 2 April 2024. Developers must deliver a BNG of at least 10%. This means a development will result in more or better-quality natural habitat than there was before development. The landowner is legally responsible for creating or enhancing the habitat and managing that habitat for at least 30 years to achieve the target condition.
- 7.59 Policy 15 of the National Planning Policy Framework (2023) states that planning decisions should contribute to and enhance the natural and local environment by minimising impacts on and providing net gains for biodiversity.
- 7.60 Paragraph 8.6.6 of Policy G6 of The London Plan (2021) states that biodiversity net gain is an approach to development that leaves biodiversity in a better state than before. Losses should be avoided, and biodiversity offsetting is the option of last resort.
- 7.61 Policy EM7 of Hillingdon Council's Local Plan Part One Strategic Policies (2012) states that Hillingdon's biodiversity and geological conservation will be preserved and enhanced, with particular attention given to improving biodiversity from all development.
- 7.62 Paragraph 6.28 of Policy DMEI 7 (Biodiversity Protection and Enhancement) of Hillingdon Council's Local Plan Part Two Development Management Policies (2020) states it is important that planning decisions are appropriately informed by the right level of survey and information on ecology features. The Council will apply Natural England's standing advice at the validation stage. Applications will only be validated if they have the appropriate information. Where initial assessments recommend further surveys, these will be expected to be provided as part of a planning submission. All ecological reports or information submitted should adhere to nationally accepted best practice survey standards and be consistent with the British Standard BS 42020: 2013 Biodiversity - Code of Practice for Planning and Development or an updated variation. Where appropriate, the Council will require the use of the approved DEFRA biodiversity impact calculator (as updated) to inform decisions on no net loss and net gain.

- 7.63 During the process of the application a Biodiversity Net Gain Assessment was requested and provided. The Council's Environmental Specialist has reviewed the documents and concluded that the habitat proposed to be provided is not appropriate for an urban setting and consequently has artificially raised the biodiversity score. The introduction of 'other neutral grassland' will be located adjacent to parking spaces and around the access tracks and will not therefore operate in a manner that is captured by this habitat requirement. It is not appropriate to select a higher scoring habitat type to artificially raise the BNG assessment levels.
- 7.64 Notwithstanding the above paragraph, the Environmental Specialist believes that a biodiversity net gain solution is capable of being secured for the development and therefore reverting to the statutory pre-commencement condition would be acceptable. However, the solution presented remains unacceptable due to the unreasonable demands placed on a future occupier to retain a relatively high-grade habitat type within the useable private curtilage of the property. A future BNG solution will need to clearly set out how habitat will be managed to retain its distinctiveness. The higher the level of distinctiveness, the more management will be required.
- 7.65 As such, the application would be able to provide a 10% increase in biodiversity, therefore the statutory condition has been added to the decision notice. However, a comprehensive landscape plan would need to be provided to ascertain how the 10% increase is met. No landscaping plans will be approved within this application.

Flooding and Drainage

- 7.66 Policy SI12 and SI13 of the London Plan (2021) require, in summary, that flood risk is minimised and mitigated, and that surface water runoff is managed close to source.
- 7.67 The site lies within Flood Zone 1 of the Environment Agency's Flood Risk Map. This means the site is classified as being at low risk and defined as having a less than 1 in 1,000 probability of fluvial and tidal flooding. As such, there are no restrictions on development, including more vulnerable uses such as Use Class C3 (dwellinghouses), in this location, in terms of fluvial and tidal flood risk.
- 7.68 A section of the highway to the front of the application site is designated within a Surface Water Flooding Zone. A condition requiring the submission of a sustainable water management scheme, that incorporates sustainable urban drainage systems (SuDs) has been added to the decision notice.

Waste Management

- 7.69 Policy DMHB 11 part (d) of the Hillingdon Local Plan (2020) states that development proposals should make sufficient provision for well-designed internal and external storage space for general, recycling and organic waste,

with suitable access for collection. External bins should be located and screened to avoid nuisance and adverse visual impacts to occupiers and neighbours.

- 7.70 The proposed landscape plan does not show any provision for refuse and recycling storage. To conform with the Council's 'waste-collection' distance parameter of 10 metres, refuse, recycling and food waste would need to be deposited kerbside on collection day. It is assumed that the proposed dwellings could store waste to the rear of the properties in individual gardens due to their semi-detached design. A suitable condition has been added to the decision notice requiring the provision and details of refuse storage to be provided.

Sustainability

- 7.71 Policy DMEI 2 of the Hillingdon Local Plan (2020) requires all developments to make the fullest contribution to minimising carbon dioxide emissions in accordance with the London Plan targets.
- 7.72 The proposed development is of a minor scale therefore whilst the principle of SI 2 (carbon reduction) is applicable, the London Plan Policy applies more specifically to major scale applications. The applicant is therefore not required to submit an energy statement with the application or demonstrate a policy level of on-site savings. Notwithstanding this point, the modern construction of the development would be considered as providing sufficient energy savings itself and therefore the development would comply with the principles of the carbon saving development plan policies.
- 7.73 A condition will be secured requiring the proposed development to achieve as a minimum, a water efficiency standard of no more than 110 litres per person per day maximum water consumption (to include a fixed factor of water for outdoor use of 5 litres per person per day in accordance with the option requirement defined within Approved Document G of the Building Regulations).
- 7.74 The proposal would therefore be compliant with Policy SI 2 of the London Plan (2021) and Policy DMEI 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

Airport Safeguarding

- 7.75 Policy DMAV 1 of the Hillingdon Local Plan (2020) states that the Council will ensure that uses such as housing, education and hospitals are not located in areas significantly affected by aircraft noise without acceptable mitigation measures.
- 7.76 The application site is within 3km of the RAF Northolt Zone. However, as the site is within an established residential area within this zone, it is considered that visibility and audibility of aircraft operations associated with RAF Northolt would not be of significant harm to the living conditions of future occupiers. It is therefore considered that it would be unreasonable to refuse the application on the ground of harm to the residential amenity of the future occupiers, in respect

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to aircraft noise associated with RAF Northolt. The Ministry of Defence were also consulted on the application and have no objection to the proposed development.

Land Contamination

- 7.77 Policy DME1 12 of the Local Plan (2020) states that for sites which are identified as being at potential risk of land contamination a contaminated land report detailing the history of contamination on site, relevant survey work and findings should be submitted in support of the application.
- 7.78 The application site is not located on contaminated land therefore a survey is not required nor are conditions pertaining to the submission of further information.

Fire Safety

- 7.79 Policy D12 of the London Plan states that all developments must achieve the highest standards of fire safety.
- 7.80 The application is not supported by a fire safety strategy. A condition pertaining to the submission of a fire safety strategy which is to be compiled by a suitably qualified individual in accordance with the criteria set out in Policy D12 is to be attached to the decision notice.

8 Other Matters

Human Rights

- 8.1 The development has been assessed against the provisions of the Human Rights Act, and in particular Article 1 of the First Protocol and Article 8 of the Act itself. This Act gives further effect to the rights included in the European Convention on Human Rights. In arriving at this recommendation, due regard has been given to the applicant's reasonable development rights and expectations which have been balanced and weighed against the wider community interests, as expressed through third party interests / the Development Plan and Central Government Guidance.

Equality

- 8.2 Due consideration has been given to Section 149 of the Equality Act with regard to the Public Sector Equality Duty in the assessment of this planning application. No adverse equality impacts are considered to arise from the proposal.

Local Finance Considerations and CIL

- 8.3 The Council adopted its own Community Infrastructure Levy (CIL) on 1st August 2014. The Hillingdon CIL charge for residential developments is £95 per square metre of additional floor space. This is in addition to the Mayoral CIL charge of £60 per square metre. CIL rates are index linked. The proposal involves the

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erection of new dwellings and is therefore CIL liable if planning permission is granted.

9 Conclusion / Planning Balance

- 9.1** On balance, the proposed development would have a satisfactory impact on the character and appearance of the area and would not give rise to any undue harm to neighbouring amenities, or the local highway network. Additionally, adequate living accommodation would be provided for future residents. The proposal would contribute additional family sized dwellings to the borough's housing stock. The proposal is considered to overcome all of the previous applications reasons for refusal.
- 9.2 The proposal is considered to comply with the Development Plan and no material considerations indicate that a contrary decision should be taken. Consequently, the application is recommended for approval subject to the conditions set out in Appendix 1.

10 Background Papers

- 10.1 Relevant published policies and documents taken into account in respect of this application are set out in the report. Documents associated with the application (except exempt or confidential information) are available on the [Council's website here](#), by entering the planning application number at the top of this report and using the search facility. Planning applications are also available to inspect electronically at the Civic Centre, High Street, Uxbridge, UB8 1UW upon appointment, by contacting Planning Services at planning@hillingdon.gov.uk.

APPENDICES

Planning Application

17969/APP/2024/845

Appendix 1: Recommended Conditions and Informatives

Conditions

1. RES3 Time Limit

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

REASON

To comply with Section 91 of the Town and Country Planning Act 1990

2. RES4 Accordance with Approved Plans

The development hereby permitted shall not be carried out except in complete accordance with the details shown on submitted plans numbers:

The Location Plan
24-3551-1 Rev A
24-3551-2 Rev A
24-3551-3
24-3551-5

Thereafter be retained/maintained for as long as the development remains in existence.

REASON

To ensure the development complies with the provisions of the Hillingdon Local Plan Part 1 (2012), Part 2 (2020) and the London Plan (2021).

3. RES9 Landscaping (car parking & refuse/cycle storage)

Prior to commencement of the hereby approved development (excluding demolition and site clearance), a landscape scheme shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall include: -

1. Details of Soft Landscaping
 - 1.a Planting plans (at not less than a scale of 1:100),
 - 1.b Written specification of planting and cultivation works to be undertaken,
 - 1.c Schedule of plants giving species, plant sizes, and proposed numbers/densities where appropriate

2. Details of Hard Landscaping
 - 2.a Refuse Storage (including location, dimensions and materials)
 - 2.b Cycle Storage (including location, dimensions and materials)
 - 2.c Means of enclosure/boundary treatments
 - 2.d Car Parking Layouts (including the provision of one 7Kw active Electric Vehicle charging

Points for each dwelling)

2.e Hard Surfacing Materials

2.f External Lighting

2.g Other structures (such as play equipment and furniture)

3. Details of Landscape Maintenance

3.a Landscape Maintenance Schedule for a minimum period of 5 years.

3.b Proposals for the replacement of any tree, shrub, or area of surfing/seeding within the landscaping scheme which dies or in the opinion of the Local Planning Authority becomes seriously damaged or diseased.

4. Schedule for Implementation

Thereafter the development shall be carried out and maintained in full accordance with the approved details.

REASON

To ensure that the proposed development will preserve and enhance the visual amenities of the locality and provide adequate facilities in compliance with policies DMHB5, DMHB 11, DMHB 12, DMHB 14, DMEI 1 and DMT 2 of the Hillingdon Local Plan Part 2 (2020) and Policy G5 of the London Plan (2021).

4. RES7 Materials (Submission)

Save for demolition and site clearance works, no above ground works shall take place until details of all materials and external surfaces have been submitted to and approved in writing by the Local Planning Authority. Thereafter the development shall be constructed in accordance with the approved details and be retained as such.

Details should include information relating to make, product/type, colour and photographs/images.

REASON

To ensure that the development presents a satisfactory appearance in accordance with Policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

5. RES6 Levels

Prior to any works on site above damp proof course level, details of step free access via all points of entry and exit shall be submitted to, and approved in writing, by the Local Planning Authority. Such provision shall remain in place for the life of the building.

REASON

To ensure housing of an inclusive design is achieved and maintained in accordance with Policies D5 and D7 of the London Plan (2021).

6. RES12 No additional windows or doors

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development)(England) Order 2015 (or any order revoking and re-enacting that Order with or without modification), no additional windows, doors or other openings shall be constructed in the walls or roof slopes of the development hereby approved.

REASON

To prevent overlooking to adjoining properties in accordance with Policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

7. NONSC Block Plan

Notwithstanding the landscape arrangements shown on drawing number 24-3551-1A. The approved drawing is to be read as a block plan only. The landscaping works would be subject to review post permission.

REASON:

To ensure that the proposed development will preserve and enhance the visual amenities of the locality and provide adequate facilities in compliance with Policies DMHB5, DMHB 11, DMHB 12, DMHB 14, DMEI 1 and DMT 2 of the Hillingdon Local Plan Part 2 (2020) and Policy G5 of the London Plan (2021). and to ensure the development delivers a Biodiversity Net Gain within the borough and secures the protection and effective management of the remaining habitat on site in accordance with Policy 15 of the National Planning Policy Framework, Policy G6 of The London Plan, and Policy DMEI 7 (Biodiversity Protection and Enhancement) of Hillingdon Council's Local Plan Part 2 Development Management Policies.

8. NONSC Sustainable Urban Drainage (SUDs)

No development approved by this permission shall be commenced until a scheme for the provision of sustainable water management has been submitted to and approved in writing by the Local Planning Authority. The scheme shall clearly demonstrate that sustainable drainage systems (SUDS) have been incorporated into the designs of the development in accordance with the hierarchy set out in accordance with Policy SI5 of the London Plan and will:

- i. provide information about the design storm period and intensity, the method employed to delay and control the surface water discharged from the site and the measures taken to prevent pollution of the receiving groundwater and/or surface waters;
- ii. include a timetable for its implementation; and
- iii. provide a management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption by any public authority or statutory undertaker and any other arrangements to secure the operation of the scheme throughout its lifetime.

The scheme shall also demonstrate the use of methods to minimise the use of potable water

through water collection, reuse and recycling and will:

- iv. provide details of water collection facilities to capture excess rainwater;
- v. provide details of how rain and grey water will be recycled and reused in the development; and
- vi. Provide details of how the dwelling will achieve a water efficiency standard of no more than 110 litres per person per day maximum water consumption (to include a fixed factor of water for outdoor use of 5 litres per person per day in accordance with the optional requirement defined within Approved Document G of the Building Regulations).

Thereafter the development shall be implemented and retained/maintained in accordance with these details for as long as the development remains in existence.

REASON

To ensure the development does not increase the risk of flooding in accordance with Policies DMEI 9 and DMEI 10 of the Hillingdon Local Plan Part 2 (2020) and Policies S12 and 13 of the London Plan (2021).

9. NONSC Ecological Enhancement scheme

No development shall take place until a Biodiversity Net Gain scheme to deliver a 10% increase in biodiversity value relative to the pre-development value of the onsite habitat has been submitted to and approved by the Local Planning Authority. The scheme shall include full details of the measures to enhance natural habitats; this must include a robust landscaping strategy which considers biodiversity value as well as the inclusion of features such as bird boxes across the site.

The approved details shall be implemented prior to development and maintained for the life of the development.

REASON

In order to encourage a wide diversity of wildlife on the existing semi-natural habitat of the site in accordance with Policy DMEI 7 of the Hillingdon Local Plan Part 2 (2020) and Policy G5 of the London Plan (2021).

10. NONSC Habitat Management Plan

No development shall take place on any part of the site until a written 30-year Habitat Management Plan (HMP) for the site has been submitted to and approved in writing by the Local Planning Authority.

The approved HMP shall be strictly adhered to, and development shall commence and operate in accordance with it.

The HMP should, as a minimum, include:

- a) Description and evaluation of the features to be managed.

- b) Aims, objectives and targets for management.
- c) Description of the management operations necessary to achieving aims and objectives.
- d) Prescriptions for management actions.
- e) Preparation of a works schedule, including an annual works schedule.
- f) Details of the monitoring needed to measure the effectiveness of management.
- g) Details of the timetable for each element of the monitoring programme and;
- h) Details of the persons responsible for the implementation and monitoring.
- i) Report to the Council routinely regarding the state of the Biodiversity Net Gain requirements for development in years 1 (post-completion), 3, 5, 10, 20, and 30, with biodiversity reconciliation calculations at each stage.

REASON

To ensure the development delivers a Biodiversity Net Gain within the borough and secures the protection and effective management of the remaining habitat on site in accordance with Policy 15 of the National Planning Policy Framework, Policy G6 of The London Plan, and Policy DMEI 7 (Biodiversity Protection and Enhancement) of Hillingdon Council's Local Plan Part 2 Development Management Policies.

11. NONSC Structural Survey

Prior to commencement, details of the proposed works to the embankment, including any excavation, shall be confirmed in writing to the Local Planning Authority. This shall include a conditions survey of the adjoining footway on Beacon Close, details of how the integrity of this foot way will be maintained, and a commitment to the repair of any damage to this footway as a result of adjacent works during the construction and operation of the development.

REASON

To ensure that the development does not compromise the structural integrity of the embankment adjoining Beacon Close and does not in turn compromise the safety of pedestrians or highway users, in accordance with Policies DMT 2 and DMT 5 of the Hillingdon Local Plan: Part 2 (2020).

12. NONSC Fire Strategy

Prior to development a detailed fire safety strategy compiled by a suitably qualified assessor shall be submitted and approved in writing by the Local Planning Authority.

REASON

To ensure the proposed development complies with fire safety regulations and Policy D12 of the London Plan (2021).

13. RES23 Visibility Splays - Pedestrian

The access for the proposed car parking shall be provided with those parts of 2.4m x 2.4m pedestrian visibility splays which can be accommodated within the site in both directions and

shall be maintained free of all obstacles to the visibility which shall not exceed a height of 0.6m above the level of the adjoining highway.

REASON

In the interests of highway and pedestrian safety in accordance with Policy DMT 2 of the Hillingdon Local Plan Part 2 (2020).

14. RES14 Outbuildings, extensions and roof alterations

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development)(England) Order 2015 (or any order revoking and re-enacting that Order with or without modification); no garage(s), shed(s) or other outbuilding(s), nor extension or roof alteration to any dwellinghouse(s) subject of this permission shall be erected without the grant of further specific permission from the Local Planning Authority.

REASON

To protect the character and appearance of the area and amenity of residential occupiers in accordance with policies DMHB 11 and DMHD 2 of the Hillingdon Local Plan Part 2 (2020)

15. NONSC Construction Logistics Plan

Prior to development commencing, a demolition and construction management plan shall be submitted to and approved in writing by the Local Planning Authority. The plan shall detail:

- (i) The phasing of development works
- (ii) The hours during which development works will occur (please refer to informative I15 for maximum permitted working hours).
- (iii) Measures to prevent mud and dirt tracking onto footways and adjoining roads (including wheel washing facilities).
- (iv) Traffic management and access arrangements (vehicular and pedestrian) and parking provisions for contractors during the development process (including measures to reduce the numbers of construction vehicles accessing the site during peak hours).
- (vi) Measures to reduce the impact of the development on local air quality and dust through minimising emissions throughout the demolition and construction process.
- (vi) The storage of demolition/construction materials on site.

The approved details shall be implemented and maintained throughout the duration of the demolition and construction process.

REASON

To safeguard the amenity of surrounding areas in accordance with Policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

16. NONSC M4(2) Compliance

The dwellings hereby approved shall accord with the requirements of Policy D7 of the London Plan and shall not be occupied until certification of compliance with the technical

specifications for an M4(2) dwelling, as set out in Approved Document M to the Building Regulations (2010) 2015, has been submitted to, and approved in writing, by the Local Planning Authority. All such provisions must remain in place for the life of the building.

REASON

To not only allow the Building Control body to require the development to comply with the optional Building Regulations standards, but to also ensure the appropriate quantity and standard of accessible and adaptable housing is constructed and maintained in accordance with Policy D7 of the London Plan.

Informatives

1.

The effect of paragraph 13 of Schedule 7A to the Town and Country Planning Act 1990 is that planning permission granted for the development of land in England is deemed to have been granted subject to the condition "(the biodiversity gain condition)" that development may not begin unless:

- (a) a Biodiversity Gain Plan has been submitted to the planning authority, and
- (b) the planning authority has approved the plan.

The planning authority, for the purposes of determining whether to approve a Biodiversity Gain Plan if one is required in respect of this permission would be the London Borough of Hillingdon.

There are statutory exemptions and transitional arrangements which mean that the biodiversity gain condition does not always apply. These are listed below.

Based on the information available this permission is considered to be one which will require the approval of a biodiversity gain plan before development is begun because none of the statutory exemptions or transitional arrangements listed below are considered to apply.

Statutory exemptions and transitional arrangements in respect of the biodiversity gain condition.

1. The application for planning permission was made before 12 February 2024.
2. The planning permission relates to development to which section 73A of the Town and Country Planning Act 1990 (planning permission for development already carried out) applies.
3. The planning permission was granted on an application made under section 73 of the Town and Country Planning Act 1990 and
 - (i) the original planning permission to which the section 73 planning permission relates* was granted before 12 February 2024; or
 - (ii) the application for the original planning permission* to which the section 73 planning permission relates was made before 12 February 2024.
4. The permission which has been granted is for development which is exempt being:

4.1 Development which is not 'major development' (within the meaning of article 2(1) of the Town and Country Planning (Development Management Procedure) (England) Order 2015) where:

- i) the application for planning permission was made before 2 April 2024;
- ii) planning permission is granted which has effect before 2 April 2024; or
- iii) planning permission is granted on an application made under section 73 of the Town and Country Planning Act 1990 where the original permission to which the section 73 permission relates* was exempt by virtue of (i) or (ii).

4.2 Development below the de minimis threshold, meaning development which:

- i) does not impact an onsite priority habitat (a habitat specified in a list published under section 41 of the Natural Environment and Rural Communities Act 2006); and
- ii) impacts less than 25 square metres of onsite habitat that has biodiversity value greater than zero and less than 5 metres in length of onsite linear habitat (as defined in the statutory metric).

4.3 Development which is subject of a householder application within the meaning of article 2(1) of the Town and Country Planning (Development Management Procedure) (England) Order 2015. A "householder application" means an application for planning permission for development for an existing dwellinghouse, or development within the curtilage of such a dwellinghouse for any purpose incidental to the enjoyment of the dwellinghouse which is not an application for change of use or an application to change the number of dwellings in a building.

4.4 Development of a biodiversity gain site, meaning development which is undertaken solely or mainly for the purpose of fulfilling, in whole or in part, the Biodiversity Gain Planning condition which applies in relation to another development, (no account is to be taken of any facility for the public to access or to use the site for educational or recreational purposes, if that access or use is permitted without the payment of a fee).

4.5 Self and Custom Build Development, meaning development which:

- i) consists of no more than 9 dwellings;
- ii) is carried out on a site which has an area no larger than 0.5 hectares; and
- iii) consists exclusively of dwellings which are self-built or custom housebuilding (as defined in section 1(A1) of the Self-build and Custom Housebuilding Act 2015).

4.5 Development forming part of, or ancillary to, the high speed railway transport network (High Speed 2) comprising connections between all or any of the places or parts of the transport network specified in section 1(2) of the High Speed Rail (Preparation) Act 2013.

* "original planning permission means the permission to which the section 73 planning permission relates" means a planning permission which is the first in a sequence of two or more planning permissions, where the second and any subsequent planning permissions are section 73 planning permissions.

Irreplaceable habitat

If the onsite habitat includes irreplaceable habitat (within the meaning of the Biodiversity Gain Requirements (Irreplaceable Habitat) Regulations 2024) there are additional requirements for the content and approval of Biodiversity Gain Plans.

The Biodiversity Gain Plan must include, in addition to information about steps taken or to be

taken to minimise any adverse effect of the development on the habitat, information on arrangements for compensation for any impact the development has on the biodiversity of the irreplaceable habitat.

The planning authority can only approve a Biodiversity Gain Plan if satisfied that the adverse effect of the development on the biodiversity of the irreplaceable habitat is minimised and appropriate arrangements have been made for the purpose of compensating for any impact which do not include the use of biodiversity credits.

The effect of section 73D of the Town and Country Planning Act 1990

If planning permission is granted on an application made under section 73 of the Town and Country Planning Act 1990 (application to develop land without compliance with conditions previously attached) and a Biodiversity Gain Plan was approved in relation to the previous planning permission ("the earlier Biodiversity Gain Plan") there are circumstances when the earlier Biodiversity Gain Plan is regarded as approved for the purpose of discharging the biodiversity gain condition subject to which the section 73 planning permission is granted.

Those circumstances are that the conditions subject to which the section 73 permission is granted:

- i) do not affect the post-development value of the onsite habitat as specified in the earlier Biodiversity Gain Plan, and
- ii) in the case of planning permission for a development where all or any part of the onsite habitat is irreplaceable habitat the conditions do not change the effect of the development on the biodiversity of that onsite habitat (including any arrangements made to compensate for any such effect) as specified in the earlier Biodiversity Gain Plan.

2.

The development hereby approved includes the carrying out of alterations to a vehicular access. Prior to undertaking work on the adopted highway you will require a Section 184 licence from the Highway Authority. The works shall be to the specification and constructed to the satisfaction of the Highway Authority. Fees are payable for the approval of the highway details, and inspection of the works. Further information and an application form are available on the London Borough of Hillingdon website <https://www.hillingdon.gov.uk/dropped-kerb-form>

3. I52 Compulsory Informative (1)

The decision to GRANT planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

4. I15 Control of Environmental Nuisance from Construction Work

Nuisance from demolition and construction works is subject to control under The Control of Pollution Act 1974, the Clean Air Acts and other related legislation. In particular, you should ensure that the following are complied with:-

A. Demolition and construction works which are audible at the site boundary shall only be carried out between the hours of 08.00 and 18.00 hours Monday to Friday and between the hours of 08.00 hours and 13.00 hours on Saturday. No works shall be carried out on Sundays, Bank or Public Holidays.

B. All noise generated during such works shall be controlled in compliance with British Standard Code of Practice BS 5228:2009.

C. Dust emissions shall be controlled in compliance with the Mayor of London's Best Practice Guidance 'The Control of dust and emissions from construction and demolition.

D. No bonfires that create dark smoke or nuisance to local residents.

You are advised to consult the Council's Environmental Protection Unit (www.hillingdon.gov.uk/noise Tel. 01895 250155) or to seek prior approval under Section 61 of the Control of Pollution Act if you anticipate any difficulty in carrying out construction other than within the normal working hours set out in (A) above, and by means that would minimise disturbance to adjoining premises.

5. 147 Damage to Verge - For Council Roads:

The Council will recover from the applicant the cost of highway and footway repairs, including damage to grass verges.

Care should be taken during the building works hereby approved to ensure no damage occurs to the verge or footpaths during construction. Vehicles delivering materials to this development shall not override or cause damage to the public footway. Any damage will require to be made good to the satisfaction of the Council and at the applicant's expense.

For further information and advice contact - Highways Maintenance Operations, Central Depot - Block K, Harlington Road Depot, 128 Harlington Road, Hillingdon, Middlesex, UB3 3EU (Tel: 01895 277524).

For Private Roads: Care should be taken during the building works hereby approved to ensure no damage occurs to the verge of footpaths on private roads during construction. Vehicles delivering materials to this development shall not override or cause damage to a private road and where possible alternative routes should be taken to avoid private roads. The applicant may be required to make good any damage caused.

6. 173 Community Infrastructure Levy (CIL) (Granting Consent)

Under the terms of the Planning Act 2008 (as amended) and Community Infrastructure Levy

Regulations 2010 (as amended), this development is liable to pay the London Borough of Hillingdon Community Infrastructure Levy (CIL) and the Mayor of London's Community Infrastructure Levy (CIL). This will be calculated in accordance with the London Borough of Hillingdon CIL Charging Schedule 2014 and the Mayor of London's CIL Charging Schedule 2012. Before commencement of works the development parties must notify the London Borough of Hillingdon of the commencement date for the construction works (by submitting a Commencement Notice) and assume liability to pay CIL (by submitting an Assumption of Liability Notice) to the Council at planning@hillingsdon.gov.uk. The Council will then issue a Demand Notice setting out the date and the amount of CIL that is payable. Failure to submit a valid Assumption of Liability Notice and Commencement Notice prior to commencement of the development may result in surcharges being imposed.

The above forms can be found on the planning portal at:
www.planningportal.gov.uk/planning/applications/howtoapply/whattosubmit/cil

Pre-Commencement Conditions: These conditions are important from a CIL liability perspective as a scheme will not become CIL liable until all of the pre-commencement conditions have been discharged/complied with.

153 **Compulsory Informative (2)**

The decision to GRANT planning permission has been taken having regard to the policies and proposals in the Hillingdon Local Plan Part 1 (2012) and Part 2 (2020) set out below, including Supplementary Planning Guidance, and to all relevant material considerations, including The London Plan and national guidance.

DMCI 7	Planning Obligations and Community Infrastructure Levy
DMEI 10	Water Management, Efficiency and Quality
DMEI 2	Reducing Carbon Emissions
DMEI 7	Biodiversity Protection and Enhancement
DMEI 9	Management of Flood Risk
DMH 2	Housing Mix
DMH 6	Garden and Backland Development
DMHB 11	Design of New Development
DMHB 12	Streets and Public Realm
DMHB 14	Trees and Landscaping
DMHB 16	Housing Standards
DMHB 17	Residential Density
DMHB 18	Private Outdoor Amenity Space
DMT 1	Managing Transport Impacts
DMT 2	Highways Impacts

DMT 5	Pedestrians and Cyclists
DMT 6	Vehicle Parking
LPP D3	(2021) Optimising site capacity through the design-led approach
LPP D4	(2021) Delivering good design
LPP D5	(2021) Inclusive design
LPP D6	(2021) Housing quality and standards
LPP D7	(2021) Accessible housing
LPP GG2	(2021) Making the best use of land
LPP GG4	(2021) Delivering the homes Londoners needs
LPP H1	(2021) Increasing housing supply
LPP H2	(2021) Small sites
LPP SI12	(2021) Flood risk management
LPP SI13	(2021) Sustainable drainage
LPP T5	(2021) Cycling
LPP T6	(2021) Car parking
LPP T6.1	(2021) Residential parking
NPPF- 5	NPPF-5 2018 - Delivering a sufficient supply of homes
NPPF11 -23	NPPF11 23 - Making effective use of land
NPPF12 -23	NPPF12 23 - Achieving well-designed and beautiful places
NPPF2 -23	NPPF2 2023 - Achieving sustainable development
NPPF4 -23	NPPF4 23 - Decision making
NPPF9 -23	NPPF9 23 - Promoting sustainable transport

Appendix 2: Relevant Planning History

17969/APP/2022/3338 19 Beacon Close Uxbridge

Erection of four terraced dwellings incorporating landscaping, parking provision, waste and cycle stores

Decision: 25-01-2023 Refused

17969/APP/2023/1014 19 Beacon Close Uxbridge

Erection of four terraced dwellings incorporating landscaping, parking provision, waste and cycle stores following demolition of existing dwelling.

Decision: 15-02-2024 Refused

17969/PRC/2022/75 19 Beacon Close Uxbridge

Redevelopment of site to incorporate 4 dwellinghouses

Decision: 05-10-2022 Objection

Appendix 3: List of Relevant Planning Policies

The following Local Plan Policies are considered relevant to the application:-

Part 1 Policies:

PT1.BE1 (2012) Built Environment

PT1.H1 (2012) Housing Growth

Part 2 Policies:

DMCI 7 Planning Obligations and Community Infrastructure Levy

DMEI 10 Water Management, Efficiency and Quality

DMEI 2 Reducing Carbon Emissions

DMEI 7 Biodiversity Protection and Enhancement

DMEI 9 Management of Flood Risk

DMH 2 Housing Mix

DMH 6 Garden and Backland Development

DMHB 11 Design of New Development

DMHB 12 Streets and Public Realm

DMHB 14 Trees and Landscaping

DMHB 16 Housing Standards

DMHB 17 Residential Density

DMHB 18 Private Outdoor Amenity Space

DMT 1 Managing Transport Impacts

DMT 2 Highways Impacts

DMT 5 Pedestrians and Cyclists

DMT 6 Vehicle Parking

LPP D3	(2021) Optimising site capacity through the design-led approach
LPP D4	(2021) Delivering good design
LPP D5	(2021) Inclusive design
LPP D6	(2021) Housing quality and standards
LPP D7	(2021) Accessible housing
LPP GG2	(2021) Making the best use of land
LPP GG4	(2021) Delivering the homes Londoners needs
LPP H1	(2021) Increasing housing supply
LPP H2	(2021) Small sites
LPP SI12	(2021) Flood risk management
LPP SI13	(2021) Sustainable drainage
LPP T5	(2021) Cycling
LPP T6	(2021) Car parking
LPP T6.1	(2021) Residential parking
NPPF11 -23	NPPF11 23 - Making effective use of land
NPPF12 -23	NPPF12 23 - Achieving well-designed and beautiful places
NPPF2 -23	NPPF2 2023 - Achieving sustainable development
NPPF- 5	NPPF-5 2018 - Delivering a sufficient supply of homes
NPPF4 -23	NPPF4 23 - Decision making
NPPF9 -23	NPPF9 23 - Promoting sustainable transport

Report of the Head of Development Management and Building Control Committee Report – Application Report

Case Officer: Emilie Bateman	24825/APP/2023/81
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Date Application Valid:	10-01-23	Statutory / Agreed Determination Deadline:	04-10-24
Application Type:	Full	Ward:	Ickenham & South Harefield

Applicant: **Mr L Chira**

Site Address: **39 Parkfield Road, Ickenham**

Proposal: **Erection of a replacement dwelling**

Summary of Recommendation: **GRANT planning permission subject to conditions**

Reason Reported to Committee: **Required under Part 3 of the Planning Scheme of Delegation (Petition received)**



Summary of Recommendation:

GRANT planning permission subject to the conditions set out in Section 2 of the following committee report.

1 Deferred at Planning Committee on 13th March 2024

- 1.1 This application was deferred at the Planning Committee on 13th March 2024 for members to visit the site and for an independent review of the submitted sunlight and daylight report.

Member Site Visit

- 1.2 A member site visit was carried out on 27th April 2023. This included viewing other properties within the street from the footpath, to gain an appreciation of the character and visual amenity of the street scene.
- 1.3 The key matters looked at on the site visit were:
- The relationship between the proposal and adjacent properties and the impact on neighbouring amenity.
 - Impact on the character and appearance of the street scene.

Sunlight & Daylight Report Review

- 1.4 The applicant submitted a report titled 'Analysis of Site Layout for Daylight and Sunlight' (dated February 2024) by Stinton Jones Consulting Engineers, in support of the application. This report concluded that the impact of the proposal would comply with BRE (Building Research Establishment) recommendations and was considered in planning officers' recommendation.
- 1.5 Following the deferral at Planning Committee, this report has been independently reviewed by LSH (Lambert Smith Hampton). LSH have confirmed that the tested windows meet the target values for daylight and pass sunlight testing. LSH have also confirmed that they are satisfied with the methodology undertaken and the report conclusions.
- 1.6 Consequently, the committee report remains unchanged in its recommendation. For transparency the committee report is attached in its original form considered by members on 13th March 2024.

2 Consultation Update

- 2.1 Since the deferral of this application, a new petition has been received with 20 valid signatures. The outcome sought by the petition is refusal or reduction in scale and height with conditions.

Hillingdon Planning Committee – 2nd October 2024

PART 1 – Members, Public & Press

Report of the Head of Development Management and Building Control

Address: 39 PARKFIELD ROAD ICKENHAM

Development: Erection of a replacement dwelling.

LBH Ref Nos: 24825/APP/2023/81

Drawing Nos: 39PR/P100 Rev. B
39PR/P200 Rev. E
39PR/P300 Rev. B
39PR/P400 Rev. A
Arboricultural Survey to BS5837:2012 , Dated 2nd June 2023
Arboricultural Impact Assessment (reference wArbtech AIA 01)
Arboricultural Method Statement to BS5837:2012 dated 3rd July 2023
Tree Protection Plan (Reference 39PR/P200 Rev. B)

Date Plans received: 10-01-2023 **Date(s) of Amendments(s):**

Date Application valid 10-01-2023

1. SUMMARY

The application proposes the erection of a replacement dwelling. This application is being presented at the Borough Planning Committee because a valid petition has been received (refer to Section 6 for further details).

Following negotiations, revised drawings were submitted showing a reduction to the scale and massing of the proposed crown roof profile. Also, the physical siting of the proposed dwelling has been set-back to align with the front building line of the neighbouring property of No. 37 Parkfield Road. Based on these revised drawings, it is considered that the siting, size, scale, bulk, massing and design of the proposed replacement dwelling would not cause harm to the character and appearance of the area. In reaching this position, significant weight has been afforded to the replacement dwelling at No.29 Parkfield Road which is of a similar size to the current proposal, and was allowed at Appeal in 2021 by the Secretary of State (Appeal Decision reference APP/R5510/W/21/3278249). Also, the recently constructed replacement dwellings at Nos. 55, 54 and 54a Parkfield Road has also been taken into account as a material consideration.

The representations received from neighbouring residents have been duly noted. However, given the separation distances involved, Daylight and Sunlight Assessment and noting the siting and dimensions of the proposed replacement dwelling, it is considered that the development would not adversely impact on the residential amenities of neighbouring occupiers. In the event of an approval, a condition would be secured requiring the first floor side window and rear window serving a bathroom and en-suite to be obscure glazed and non-opening up to 1.8 metres of the finished floor level.

Upon Officer's request, an Arboricultural Impact Assessment, Method Statement and Tree Protection Plan was submitted by the applicant. Subject to a condition requiring the construction works to be carried out with the mitigation measures detailed in the report, it is considered that the proposal would not cause harm to health of the trees at the site (which includes a protected Oak tree). The Council's Highways Department is satisfied that the proposal would not present a risk to road safety, hinder the free flow of traffic, or lead to parking stress. It is therefore recommended that planning permission is granted, subject to the imposition of relevant planning conditions.

2. RECOMMENDATION

APPROVAL subject to the following:

1. RES3 **Time Limit**

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

REASON

To comply with Section 91 of the Town and Country Planning Act 1990

2. RES4 **Accordance with Approved Plans**

The development hereby permitted shall not be carried out except in complete accordance with the details shown on the submitted plans, numbers 39PR/P100 Rev. B (dated 24.11.23), 39PR/P200 Rev. E (dated 29.02.24), 39PR/P300 Rev. B (25.03.23) and 39PR/P400 Rev. A (dated 25.05.23) and shall thereafter be retained/maintained for as long as the development remains in existence.

REASON

To ensure the development complies with the provisions of the Hillingdon Local Plan Part 1 (2012), Part 2 (2020) and the London Plan (2021).

3. RES7 **Materials (Submission)**

Prior to the commencement of any work above damp proof course level of the development hereby approved, details of all materials and external surfaces, including details of the roof lights, doors, windows, guttering and fascia, have been submitted to and approved in writing by the Local Planning Authority. Thereafter the development shall be constructed in accordance with the approved details and be retained as such.

Details should include information relating to make, product/type, colour and photographs/images.

REASON

To ensure that the development presents a satisfactory appearance in accordance with Policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

4. COM5 **General compliance with supporting documentation**

The development hereby permitted shall not be occupied until the following has been completed in accordance with the specified supporting plans and/or documents:

Analysis of Site Layout for Daylight and Sunlight dated February 2024
Arboricultural Survey to BS5837:2012 , Dated 2nd June 2023
Arboricultural Impact Assessment (reference wArbtech AIA 01)
Arboricultural Method Statement to BS5837:2012 dated 3rd July 2023
Tree Protection Plan (Reference 39PR/P200 Rev. B)

Thereafter the development shall be retained/maintained in accordance with these details for as long as the development remains in existence

REASON

To ensure that the development complies with the objectives of Policies . Specify Hillingdon Local Plan Part 2 (2020)

5. OM19 Construction Management Plan

Prior to commencement of the development hereby approved, a demolition and construction management plan shall be submitted to and approved in writing by the Local Planning Authority. The plan shall detail:

- (i) The phasing of development works
- (ii) The hours during which development works will occur (please refer to informative I15 for maximum permitted working hours).
- (iii) Measures to prevent mud and dirt tracking onto footways and adjoining roads (including wheel washing facilities).
- (iv) Traffic management and access arrangements (vehicular and pedestrian) and parking provisions for contractors during the development process (including measures to reduce the numbers of construction vehicles accessing the site during peak hours).
- (vi) Measures to reduce the impact of the development on local air quality and dust through minimising emissions throughout the demolition and construction process.
- (vi) The storage of demolition/construction materials on site.

The approved details shall be implemented and maintained throughout the duration of the demolition and construction process.

REASON

To safeguard the amenity of surrounding areas in accordance with Policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

6. NONSC Sustainable Water Management Strategy

Prior to the commencement of the development hereby approved (excluding demolition, ground works and substructure works), a scheme for the provision of sustainable water management and water efficiency shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall:

- i. Provide information about the design storm period and intensity, the method employed to delay and control the surface water discharged from the site and the measures taken to prevent pollution of the receiving groundwater and/or surface waters;
- ii. Include a timetable for its implementation; and
- iii. Provide a management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption by any public authority or statutory undertaker and any other arrangements to secure the operation of the scheme throughout its lifetime.

The scheme shall also demonstrate the use of methods to minimise the use of potable water through water collection, reuse and recycling and will:

- iv. Provide details of water collection facilities to capture excess rainwater;
- v. Provide details of how rain and grey water will be recycled and reused in the development;
- vi. Provide details of how the dwelling will achieve a water efficiency standard of no more than 110 litres per person per day maximum water consumption (to include a fixed factor of water for outdoor use of 5 litres per person per day in accordance with the optional requirement defined within Approved Document G of the Building Regulations).

Thereafter the development shall be implemented and retained/maintained in accordance with these details for as long as the development remains in existence.

REASON

To ensure the development does not increase the risk of flooding in accordance with Policies DMEI 9 and DMEI 10 of the Hillingdon Local Plan Part 2 (2020) and Policies SI2 and SI 13 of the London Plan (2021).

7. NONSC Air Quality Neutral

Prior to any works on site above damp proof course level, details of the heating system installed for the new dwellings shall be submitted for approval. The heating system details shall only be comprised of any of the following

- A heat pump or other zero-emission heat source.
- One or more individual gas boilers with NOx emissions rated at less than 40 mg/kWh.
- The development is connected to an existing heat network.

The development shall be carried out in accordance with the proposed details and maintained for the lifetime of the development.

Reason: In the interests of improving air quality and ensuring an air quality neutral development in accordance with Policy DMEI 14 of the Hillingdon Local Plan Part 2 (2020), Policy SI 1 of the London Plan (2021) and London Plan Guidance: Air Quality Neutral (2023).

8. NONSC Tree Protection Measures

The construction works for the development hereby permitted shall be carried out strictly in accordance with the protection measures detailed in the Arboricultural Impact Assessment (reference Arbtech AIA 01), Arboricultural Method Statement to BS5837:2012 dated 3rd July 2023 and Tree Protection Plan (Reference 39PR/P200 Rev. B).

REASON

To ensure that trees and other vegetation can and will be retained on site and not damaged during construction work and to ensure that the development conforms with policy DMHB 14 of the Hillingdon Local Plan Part 2 (2020).

9. RES10 Tree to be retained

Trees, hedges and shrubs shown to be retained on the approved plan(s) shall not be damaged, uprooted, felled, lopped or topped without the prior written consent of the Local Planning Authority. If any retained tree, hedge or shrub is removed or severely damaged during (or after) construction, or

is found to be seriously diseased or dying, another tree, hedge or shrub shall be planted at the same place or, if planting in the same place would leave the new tree, hedge or shrub susceptible to disease, then the planting should be in a position to be first agreed in writing with the Local Planning Authority and shall be of a size and species to be agreed in writing by the Local Planning Authority and shall be planted in the first planting season following the completion of the development or the occupation of the buildings, whichever is the earlier. Where damage is less severe, a schedule of remedial works necessary to ameliorate the effect of damage by tree surgery, feeding or groundwork shall be agreed in writing with the Local Planning Authority. New planting should comply with BS 3936 (1992) 'Nursery Stock, Part 1, Specification for Trees and Shrubs' Remedial work should be carried out to BS BS 3998:2010 'Tree work - Recommendations' and BS 4428 (1989) 'Code of Practice for General Landscape Operations (Excluding Hard Surfaces)'. The agreed work shall be completed in the first planting season following the completion of the development or the occupation of the buildings, whichever is the earlier.

REASON

To ensure that the trees and other vegetation continue to make a valuable contribution to the amenity of the area in accordance with policy DMHB 14 of the Hillingdon Local Plan Part 2 (2020) and to comply with Section 197 of the Town and Country Planning Act 1990.

10. RES9 Landscaping (car parking & refuse/cycle storage)

Prior to the commencement of any works above damp proof course level of the development approved, a landscape scheme shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall include: -

1. Details of Soft Landscaping

- 1.a Planting plans (at not less than a scale of 1:100) which include a net increase of high quality pollution absorbing trees
- 1.b Written specification of planting and cultivation works to be undertaken,
- 1.c Schedule of plants giving species, plant sizes, and proposed numbers/densities where appropriate

2. Details of Hard Landscaping

- 2.a Refuse Storage (including its location, dimensions, finish and design)
- 2.b Covered Cycle Storage (including its location, dimensions, finish and design)
- 2.c Means of enclosure/boundary treatments
- 2.d Car Parking Layouts (including one active and three passive electric charging points)
- 2.e Permeable Hard Surfacing Materials

3. Details of Landscape Maintenance

- 3.a Landscape Maintenance Schedule for a minimum period of 5 years.
- 3.b Proposals for the replacement of any tree, shrub, or area of surfing/seeding within the landscaping scheme which dies or in the opinion of the Local Planning Authority becomes seriously damaged or diseased.
- 3.c Ecological and biodiversity enhancement plan

4. Schedule for Implementation

Thereafter the development shall be carried out and maintained in full accordance with the approved details.

REASON

To ensure that the proposed development will preserve and enhance the visual amenities of the locality and provide adequate facilities in compliance with policies DMHB 11, DMHB 12, DMHB 14, DMT 5, DMEI 7 of the Hillingdon Local Plan Part 2 (2020) and Policies G5 and T5 of the London Plan (2021).

11. NONSC Step free access

Prior to any works on site above damp proof course level, details of step free access via all points of entry and exit shall be submitted to, and approved in writing, by the Local Planning Authority. Such provision shall remain in place for the life of the building.

REASON

To ensure housing of an inclusive design is achieved and maintained in accordance with Policies D5 and D7 of the London Plan (2021).

12. NONSC Category M4(2)

The dwelling hereby approved shall not be occupied until certification of compliance with the technical specifications for an M4(2) dwelling, as set out in Approved Document M to the Building Regulations (2010) 2015, has been submitted to, and approved in writing, by the Local Planning Authority. All such provisions must remain in place for the life of the building.

REASON

To not only allow the Building Control body to require the development to comply with the optional Building Regulations standards, but to also ensure the appropriate quantity and standard of accessible and adaptable housing is constructed and maintained in accordance with policy D7 of the London Plan.

13. RES13 Obscure Glazing

The first floor windows serving the rooms labelled as 'bathroom' and 'en-suite' on drawing number 39PR/P100 Rev. B (dated 30.05.23) shall be glazed with permanently obscured glass to at least scale 4 on the Pilkington scale and be non-opening below a height of 1.8 metres taken from internal finished floor level for so long as the development remains in existence.

REASON

To prevent overlooking to adjoining properties in accordance with policies DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

14. RES12 No additional windows or doors

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development)(England)Order 2015 (or any order revoking and re-enacting that Order with or without modification), no additional windows, doors or other openings shall be constructed in the walls or roof slopes of the development hereby approved.

REASON

To prevent overlooking to adjoining properties in accordance with policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

15. RES14 Outbuildings, extensions and roof alterations

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development)(England) Order 2015 (or any order revoking and re-enacting that Order with or without modification); no garage(s), shed(s) or other outbuilding(s), nor extension or roof alteration to any dwellinghouse(s) shall be erected without the grant of further specific permission from the Local Planning Authority.

REASON

To protect the character and appearance of the area and amenity of residential occupiers in accordance with policies DMHB 11 and DMHD 2 of the Hillingdon Local Plan Part 2 (2020)

INFORMATIVES

1. I15 **Control of Environmental Nuisance from Construction Work**

Nuisance from demolition and construction works is subject to control under The Control of Pollution Act 1974, the Clean Air Acts and other related legislation. In particular, you should ensure that the following are complied with:-

A. Demolition and construction works which are audible at the site boundary shall only be carried out between the hours of 08.00 and 18.00 hours Monday to Friday and between the hours of 08.00 hours and 13.00 hours on Saturday. No works shall be carried out on Sundays, Bank or Public Holidays.

B. All noise generated during such works shall be controlled in compliance with British Standard Code of Practice BS 5228:2009.

C. Dust emissions shall be controlled in compliance with the Mayor of London's Best Practice Guidance' The Control of dust and emissions from construction and demolition.

D. No bonfires that create dark smoke or nuisance to local residents.

You are advised to consult the Council's Environmental Protection Unit (www.hillingdon.gov.uk/noise Tel. 01895 250155) or to seek prior approval under Section 61 of the Control of Pollution Act if you anticipate any difficulty in carrying out construction other than within the normal working hours set out in (A) above, and by means that would minimise disturbance to adjoining premises.

2. I47 **Damage to Verge - For Council Roads:**

The Council will recover from the applicant the cost of highway and footway repairs, including damage to grass verges.

Care should be taken during the building works hereby approved to ensure no damage occurs to the verge or footpaths during construction. Vehicles delivering materials to this development shall not override or cause damage to the public footway. Any damage will require to be made good to the satisfaction of the Council and at the applicant's expense.

For further information and advice contact - Highways Maintenance Operations, Central Depot - Block K, Harlington Road Depot, 128 Harlington Road, Hillingdon, Middlesex, UB3 3EU (Tel: 01895 277524).

For Private Roads: Care should be taken during the building works hereby approved to ensure no damage occurs to the verge of footpaths on private roads during construction. Vehicles delivering materials to this development shall not override or cause damage to a private road and where possible alternative routes should be taken to avoid private roads. The applicant may be required to make good any damage caused.

3. I52 Compulsory Informative (1)

The decision to GRANT planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

4. I70 LBH worked applicant in a positive & proactive (Granting)

In dealing with the application the Council has implemented the requirement in the National Planning Policy Framework to work with the applicant in a positive and proactive way. We have made available detailed advice in the form of our statutory policies from Local Plan Part 1, Local Plan Part 2, Supplementary Planning Documents, Planning Briefs and other informal written guidance, as well as offering a full pre-application advice service, in order to ensure that the applicant has been given every opportunity to submit an application which is likely to be considered favourably.

5. I73 Community Infrastructure Levy (CIL) (Granting Consent)

Under the terms of the Planning Act 2008 (as amended) and Community Infrastructure Levy Regulations 2010 (as amended), this development is liable to pay the London Borough of Hillingdon Community Infrastructure Levy (CIL) and the Mayor of London's Community Infrastructure Levy (CIL). This will be calculated in accordance with the London Borough of Hillingdon CIL Charging Schedule 2014 and the Mayor of London's CIL Charging Schedule 2012. Before commencement of works the development parties must notify the London Borough of Hillingdon of the commencement date for the construction works (by submitting a Commencement Notice) and assume liability to pay CIL (by submitting an Assumption of Liability Notice) to the Council at planning@hillingdon.gov.uk. The Council will then issue a Demand Notice setting out the date and the amount of CIL that is payable. Failure to submit a valid Assumption of Liability Notice and Commencement Notice prior to commencement of the development may result in surcharges being imposed.

The above forms can be found on the planning portal at:

www.planningportal.gov.uk/planning/applications/howtoapply/whattosubmit/cil

Pre-Commencement Conditions: These conditions are important from a CIL liability perspective as a scheme will not become CIL liable until all of the pre-commencement conditions have been discharged/complied with.

6. IT05 Wildlife and Countryside Act 1981

Wildlife and Countryside Act 1981: Note that it is an offence under this act to disturb roosting bats, nesting birds or any other protected species. Therefore, if applicable, it is advisable to consult your tree surgeon / consultant to agree an acceptable time for carrying out the approved works.

153 Compulsory Informative (2)

The decision to GRANT planning permission has been taken having regard to the policies and proposals in the Hillingdon Local Plan Part 1 (2012) and Part 2 (2020) set out below, including Supplementary Planning Guidance, and to all relevant material considerations, including The London Plan - The Spatial Development Strategy for London consolidated with alterations since 2011 (2016) and national guidance.

DMAV 3	RAF Northolt
DMEI 10	Water Management, Efficiency and Quality
DMEI 7	Biodiversity Protection and Enhancement
DMEI 9	Management of Flood Risk
DMH 1	Safeguarding Existing Housing
DMH 2	Housing Mix
DMHB 11	Design of New Development
DMHB 12	Streets and Public Realm
DMHB 14	Trees and Landscaping
DMHB 16	Housing Standards
DMHB 17	Residential Density
DMHB 18	Private Outdoor Amenity Space
DMT 1	Managing Transport Impacts
DMT 2	Highways Impacts
DMT 5	Pedestrians and Cyclists
DMT 6	Vehicle Parking
LPP D3	(2021) Optimising site capacity through the design-led approach
LPP D5	(2021) Inclusive design
LPP D6	(2021) Housing quality and standards
LPP D7	(2021) Accessible housing
LPP G6	(2021) Biodiversity and access to nature
LPP G7	(2021) Trees and woodlands
LPP SI12	(2021) Flood risk management
LPP SI13	(2021) Sustainable drainage
LPP T5	(2021) Cycling
LPP T6	(2021) Car parking
LPP T6.1	(2021) Residential parking
NPPF11	NPPF 2021 - Making effective use of land
NPPF12	NPPF 2021 - Achieving well-designed places
NPPF14	NPPF 2021 - Meeting the challenge of climate change flooding
NPPF2	NPPF 2021 - Achieving sustainable development
NPPF4	NPPF 2021 - Decision-Making
NPPF5	NPPF 2021 - Delivering a sufficient supply of homes
NPPF9	NPPF 2021 - Promoting sustainable transport

3. CONSIDERATIONS

3.1 Site and Locality

The application site is located on the south-western side of Parkfield Road. It comprises a two and half storey detached property that is characterised by its hipped roof profile and cream render external finish. The property benefits from a garage which is attached to the host property by a front canopy. The property is set back from the highway by an area of hard standing which serves as on-site car parking provision. Access to the on-site car parking spaces is via the two existing crossovers emerging onto Parkfield Road.

The surrounding area is residential in nature and is defined by single and two storey detached and semi-detached houses of various architectural style. Over recent years, two and half storey replacement dwellings have been built on Parkfield Road, which now forms part of the emerging character of the street scene. No. 37 Parkfield Road is located to the south of the site, and comprises a detached bungalow that is sited broadly in alignment with the front building line of the existing property at the application site. No. 41 Parkfield Road is located to the north of the site, and also comprises a detached bungalow. However, this neighbouring bungalow is set-behind the front building line of the existing property at the application site.

The application site is not designated within a Conservation Area or an Area of Special Local Character. The site does not contain any Listed Buildings. The site is covered by Tree Preservation Order 514. The site lies in Flood Zone 1 and has a PTAL rating of 1a (very poor).

3.2 Proposed Scheme

The application proposes the erection of a replacement dwelling. It should be noted that revised drawings were submitted during the course of this application showing the following key changes:

- Siting the main front building line of proposed dwelling in alignment with No.37 Parkfield Road.
- Reducing the scale and massing of the crown roof profile.
- Submission of an Arboricultural Survey report, and subsequent Arboricultural Impact Assessment, Method Statement and Tree Protection Plan.
- * Submission of a Daylight Sunlight report

In accordance with Hillingdon Council Statement of Community Involvement, neighbouring residents were re-consulted on the revised drawings and were provided an additional 14-days to submit any additional comments they wished to make. Refer to Section 6 of this Committee Report for further details about the consultation.

3.3 Relevant Planning History

24825/78/0335	39 Parkfield Road Ickenham
	Single storey extension for lobby and new wall.
Decision: 12-05-1978	Approved

24825/TRE/2006/140 39 Parkfield Road Ickenham

TO CARRY OUT TREE SURGERY (TO REDUCE BY 4M TWO OVERLONG LIMBS FACING RECTORY WAY AND TO REDUCE BY 3M ONE OVERLONG LIMB, AND THIN BY 15% LOW GROWTH UP TO A HEIGHT OF 9M) TO ONE OAK (T4) ON TPO NO. 514

Decision: 14-02-2007 Approved

24825/TRE/2021/146 39 Parkfield Road Ickenham

To carry out tree surgery, including a reduction of the longest limbs by up to 4m on the south side of the crown, to One Oak, T4 on TPO 514

Decision: 12-08-2021 Approved

24825/TRE/2021/62 39 Parkfield Road Ickenham

To carry out tree surgery, including a reduction of the crown radius by approx. 6 metres, reshape including height reduction in proportion, thin central growth by removal of epicormic shoots, lift to height of lowest main limbs by removal of pendulous growth to one oak, T4 on TPO 514

Decision: 17-05-2021 Split Decision (P)

Comment on Relevant Planning History

The relevant planning history attached to this site is referenced above.

4. Planning Policies and Standards

Development Plan:

Planning law requires that applications for planning permission be determined in accordance with the development plan, unless material considerations indicate otherwise. The Development Plan for the London Borough of Hillingdon currently consists of the following documents:

The Local Plan: Part 1 - Strategic Policies (2012)

The Local Plan: Part 2 - Development Management Policies (2020)

The Local Plan: Part 2 - Site Allocations and Designations (2020)

The West London Waste Plan (2015)

The London Plan (2021)

Material Considerations:

The National Planning Policy Framework (NPPF) (2023) is also a material consideration in planning decisions, as well as relevant supplementary planning documents and guidance.

Local Plan Designation and London Plan

The following Local Plan Policies are considered relevant to the application:-

Part 1 Policies:

Part 2 Policies:

- DMEI 7 Biodiversity Protection and Enhancement
- DMEI 9 Management of Flood Risk
- DMEI 10 Water Management, Efficiency and Quality
- DMAV 3 RAF Northolt
- DMH 1 Safeguarding Existing Housing
- DMH 2 Housing Mix
- DMHB 11 Design of New Development
- DMHB 12 Streets and Public Realm
- DMHB 14 Trees and Landscaping
- DMHB 16 Housing Standards
- DMHB 17 Residential Density
- DMHB 18 Private Outdoor Amenity Space
- DMT 1 Managing Transport Impacts
- DMT 2 Highways Impacts
- DMT 5 Pedestrians and Cyclists
- DMT 6 Vehicle Parking
- LPP D3 (2021) Optimising site capacity through the design-led approach
- LPP D5 (2021) Inclusive design
- LPP D6 (2021) Housing quality and standards
- LPP D7 (2021) Accessible housing
- LPP G6 (2021) Biodiversity and access to nature
- LPP G7 (2021) Trees and woodlands
- LPP SI12 (2021) Flood risk management
- LPP SI13 (2021) Sustainable drainage

- LPP T5 (2021) Cycling
- LPP T6 (2021) Car parking
- LPP T6.1 (2021) Residential parking
- NPPF11 NPPF 2021 - Making effective use of land
- NPPF12 NPPF 2021 - Achieving well-designed places
- NPPF14 NPPF 2021 - Meeting the challenge of climate change flooding
- NPPF2 NPPF 2021 - Achieving sustainable development
- NPPF4 NPPF 2021 - Decision-Making
- NPPF5 NPPF 2021 - Delivering a sufficient supply of homes
- NPPF9 NPPF 2021 - Promoting sustainable transport

5. Advertisement and Site Notice

- 5.1** Advertisement Expiry Date: Not Applicable
- 5.2** Site Notice Expiry Date: Not applicable

6. Consultations

External Consultees

18 neighbouring properties were originally consulted by letters dated 13th January 2023.

Six objections were received (including from the lead petitioner), and their comments are summarised as follows:

- This is a characterful, sound and well kept family dwelling which is not in need of demolition.
- The current house is perfectly habitable and does not require demolishing. It is a house full of character and charm.
- Sitting between two bungalows, the proposed rebuild would dwarf both properties, not only affect their light, undoubtedly cause distress to the residents and also look out of place.
- This proposal is simply exploitation of the local amenity to the detriment of all the residents.
- The proposal is totally out of keeping with the surrounding properties, overbearing and excessively disproportionate.
- Its size and height will permit intrusive viewing into bedrooms, living rooms and gardens of adjacent bungalows and those across the street.
- Issues with loss of light for neighbouring residential occupiers.
- This particular proposal will rob all the bungalows opposite of any late afternoon sunshine making any future solar panel installations useless let alone casting a cold winter shadow into their rooms.
- The size and number of bedrooms would appear to indicate that this is intended to become a HMO in

which case parking would obviously be an issue.

- Concerns on whether adequate on-site car parking provision is being provided.
- The Planning Application states there are no trees and hedges on the site and no important habitats. This is incorrect as there is a large oak tree at the bottom of the garden which backs onto Rectory Way.
- There is an oak tree in the garden which will be subject to a TPO (as may be the case for several smaller trees - ash and plum) and must not be damaged.
- Bats are seen flying in the neighbourhood so a survey needs to be done to ascertain if they are nesting in the property.
- Parkfield Road is currently being plagued by unnecessary demolition of perfectly good properties to be replaced by high rise characterless buildings.
- Too many of the recent development on Parkfield Road are impacting on the individual character of the original houses.
- There is ample room for the current property to be extended if this is required.
- The construction is not a residential construction, but a commercial one.
- A three storey building which includes a guest suite, 4 double bedrooms including en-suite plus 3 habitable rooms in the loft with Velux windows built amongst bungalows is any of the following:- a boarding house, guest house, hotel or flats.
- In the event of planning permission being granted, the builders must adhere to the working hours as stated on the council website.
- Any damage to the adjoining fences or properties must be made good.
- Could the development dates be confirmed? They are stated to be April 2022 to August 2022.

PLANNING OFFICER COMMENT:

It should be clarified that this planning application is for a family sized dwelling. The proposal is not for a House in Multiple Occupation (HMO), boarding house, guest house, hotel or flats, nor does it constitute as a 'commercial construction'. Planning permission is not required to demolish the existing property at the site because the application site is not located within a Conservation Area.

It is noted that the proposed replacement dwelling would be sited between two bungalows. However, the scale of the proposed dwelling at two and half storeys (with accommodation within the roof space) would be in-keeping with the scale of the more recent replacement dwellings that have been constructed on Parkfield Road. Please refer to Sections 7.07 of the Committee Report for the assessment of the proposal's impact on the character and appearance of the area.

Due consideration has been given to the concerns raised about the impact the proposal would have on the residential amenities of neighbouring occupiers. However, for the reasons discussed in Section 7.08 of the Committee Report, it is considered that the proposal would not adversely impact on the living conditions of neighbouring occupiers, in respect light, outlook or sense of enclosure. In the event of an approval, a condition would be secured requiring the first floor side window (serving a bathroom) and the rear en-suite window to be obscure glazed and non-opening up to 1.8 metres of the finished floor level.

The Council's Highways Officer was consulted on this application and has raised no objection to the proposal, in respect to parking or highway safety (see previous section of this report for Highways Officer comments). The proposal would provide up to four car parking spaces, which exceeds the Council's maximum standard of two car parking spaces for new houses. Refer to Section 7.10 of the Committee Report for the assessment on access, parking and highway safety. In the event of an approval, a Construction Management Plan would be secured by condition, which would require details of construction hours to be submitted to the Council for consideration.

It is acknowledged that the application site contains a protected Oak tree. Upon Officer's request, an Arboricultural Impact Assessment, Method Statement and Tree Protection Plan was submitted by the

applicant. The report has been reviewed by the Council's Trees Officer who has raised no objection, subject to the construction works being carried out in accordance with the submitted details. No evidence has been provided to substantiate the claim that the site forms part of bats' commuting routes. Furthermore, Circular 06/05: Biodiversity and Geological Conservation- Statutory Obligations and their Impact within the Planning System' makes clear that, "... developers should not be required to undertake surveys for protected species unless there is a reasonable likelihood of the species being present and affected by the development." Refer to Section 7.14 of the Committee Report for further details in respect to ecology impacts.

If planning permission was to be granted, a condition would be secured requiring the works for the approved development to commence within three years of the date of the decision notice. The concerns raised about the potential damage to third party buildings or fences is not a planning consideration.

Two representations were received which raised no objection, in principle, to the proposal, but raised the following concerns:

- The rear elevation shows two skylights in the loft space. There is no objection to the roof lights, but this was similar to the application at No. 43 Parkfield Road which was granted and almost immediately turned into a large dormer window.
- Perhaps a 25 year moratorium on altering/improving light access to the loft area over and above the skylights to the rear elevation would be appropriate saving my property being potentially overlooked.
- The plans show roof lights facing the front and rear of the proposed development. There would be an objection to this arrangement, if this is in anticipation of further development to the roof space, due to concerns of overlooking.
- The site contains a large Oak tree which is covered by TPO 514. This Oak is on the rear boundary line of the property and with the size far reaching root system, any disturbance could endanger it's stability.
- Concerns that the foundations would interfere with the established root system of the tree certainly under the canopy of the Oak.

PLANNING OFFICER COMMENT: The submitted drawings show that the rear elevation of the proposed replacement dwelling would contain roof lights and not dormers. In the event of an approval, a condition would be secured removing permitted development rights which means that planning permission will be required should the applicant wish to erect any roof extensions or dormers in the future.

The submitted plans show habitable rooms within the attic space. However, there would be a separation distance in excess of 21 metres between the proposed front and rear roof lights and the neighbouring properties at Nos. 33 and 38 Parkfield Road and Nos. 42 to 46 Rectory Way. This would be in accordance with the guidelines stipulated in paragraph 5.38 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020). It is therefore considered that the proposal would not give rise to a loss of privacy or overlooking issues for these respective neighbouring properties.

As previously stated, Arboricultural reports have been submitted by the applicant, and the assessment on the protected tree at the site is covered in Section 7.14 of this Committee Report.

Four representations in support of the application were received, and their comments are summarised as follows:

- Nice modern design, it goes very well with the other new and replacement houses which are being built on Parkfield Road
- A number of developments have already taken place on the road, which have improved the stature and quality of housing
- The development provides a modern and positive contribution to the road. The changes to add rear dormers is in line with other properties along the road.

- It is in-keeping with other recent new building developments on the road.

PLANNING OFFICER COMMENT: For clarity, the proposed replacement dwelling does not include any rear dormers in its roof face, instead roof lights are being proposed.

Following the receipt of revised drawings, neighbouring properties were re-consulted on 6th June and 5th July. The consultation period expired on 27th July 2023. Six additional representations were received, and their comments are summarised as follows:

- Why is there a need to demolish and build a new dwelling?
- The road currently comprises predominately of bungalows; however, there is now a precedent of demolishing these bungalows and replacing them with larger 2/3 storey dwellings.
- The new plans show the garage is being replaced by a 2 story building which will have a detrimental affect on the light entering my property.
- Loss of outlook and light for neighbouring occupiers
- Has a bat survey been carried out? Bats have been seen flying in the vicinity of the dwelling.
- It is noted that the proposal is now for 6 bedrooms with the loft area now designated 5th bedroom with 4 on 1st floor and the en suite guest suite on ground floor.
- The "new" 5th bedroom on second floor still has 2 skylight type windows on the plan, but no windows on the rear elevation. If that is a clever ploy to say "oops" we meant to put dormer windows in - then I object in full on basis of being overlooked.
- The proposed amended drawing dated 23.05.2023 the top left view shows no roof lights to the rear, the block plan in the bottom right still shows 2 roof lights facing rear.
- Concerns about trees being removed
- I trust that the tree survey report including the Oak, confirms my concerns regarding the root structure and the need not to allow any building in this area.
- This building proposal is now a six bedroomed house not 5 as stated.
- Pleased to seek a detailed Arboricultural Method Statement and that tree protection for the oak will be in place and monitored throughout the works

PLANNING OFFICER COMMENT: Refer to the previous responses noted above in respect to the points raised about the demolition of the existing building, impact on neighbouring residential amenities, impact on the protected Oak tree and ecology. It is noted that the proposed replacement dwelling would contain six-bedroom plus occupancy. For the avoidance of doubt, the latest drawing (no. 39PR/P200 Rev. D, received on 09.10.23) show roof lights on the rear elevation of the property.

Following the receipt of a Daylight and Sunlight Assessment, neighbouring properties were re-consulted on 6th February 2024. The consultation period expired on 21st February 2024. Three additional representations were received, and their comments are summarised as follows:

- 41 Parkfield Window 1 in Figure 3 is a bedroom not a bathroom as stated in the report, therefore needs to be considered with regards to light (reference to Daylight and Sunlight Assessment)
- Concerns regarding accuracy of Daylight and Sunlight Assessment as two windows were not assessed for No.41
- The B.R.E. states that derived results are guidelines and not mandatory
- W2 does open into the hallway, but the loss of light into the main entrance of the property will make the long and thin passage very dark and gloomy and should be considered due to the loss of amenity.
- Concerns regarding recently built property at No.29 Parkfield Road and similarities
- Over dominant
- Out of character
- Overlooking
- Overshadowing

PLANNING OFFICER COMMENT: Refer to the previous responses noted above and main report in respect to the points raised. In regards to the Daylight and Sunlight Assessment, it is noted that the assessment was revised to include the obscured glazed side window in the calculations. W3 serves the hall area therefore there is no expectation of daylight, and there is no need to analyse as stated in the BRE guidelines.

ICKENHAM RESIDENTS ASSOCIATION: No comments received.

PETITION:

A petition against the application with 21 signatories was received by the Council. The desired outcome stated on the petition is as follows: "Planning application 24825-APP-2023-81, 39 Parkfield Road, Demolition of original building- Erection of 3 storey 5 bedroom house. Rejection or Conditions."

DEFENCE INFRASTRUCTURE ORGANISATION - MOD SAFEGUARDING- RAF NORTHOLT:

The Defence Infrastructure Organisation (DIO) Safeguarding Team represents the Ministry of Defence (MOD) as a consultee in UK planning and energy consenting systems to ensure that development does not compromise or degrade the operation of defence sites such as aerodromes, explosives storage sites, air weapon ranges, and technical sites or training resources such as the Military Low Flying System. The application is a proposal for the construction of a three-storey five bedroom dwelling (with maximum height not exceeding 8.3m) to replace the existing two-storey three bedroom dwelling.

The application site occupies the statutory safeguarding zones surrounding RAF Northolt - in particular, the aerodrome height, technical and birdstrike safeguarding zones surrounding the aerodrome - and it is approximately 2.62km from the centre of the airfield.

After reviewing the application documents, I can confirm the MOD has no safeguarding objections to this proposal.

The MOD must emphasise that the advice provided within this letter is in response to the data and information detailed in the developer's documents titled "Application Form" and "Plans and Elevations" dated January 2023 and May 2023 respectively. Any variation of the parameters (which include the location, dimensions, form, and finishing materials) detailed may significantly alter how the development relates to MOD safeguarding requirements and cause adverse impacts to safeguarded defence assets or capabilities. In the event that any amendment, whether considered material or not by the determining authority, is submitted for approval, the MOD should be consulted and provided with adequate time to carry out assessments and provide a formal response.

PLANNING OFFICER RESPONSE: It should be clarified that since the consultation response from the MOD revised drawings have been submitted showing a six-bedroom plus replacement dwelling, rather than the five-bedrooms originally proposed. However, given that the revised drawings did not result in an increase to the size or height of the proposed dwelling, it was not deemed necessary to re-consult the MOD.

Internal Consultees

COUNCIL'S ACCESS OFFICER:

This proposal for a new 5-bedroom residential dwelling has been reviewed with reference to London Plan

policy D7 with no accessibility concerns raised subject to the following conditions attached to any approval: Prior to any works on site above damp proof course level, details of step free access via all points of entry and exit shall be submitted to, and approved in writing, by the Local Planning Authority. Such provision shall remain in place for the life of the building.

Reason: To ensure housing of an inclusive design is achieved and maintained in accordance with Policies D5 and D7 of the London Plan (2021).

The dwelling hereby approved shall accord with the requirements of Policy D7 of the London Plan and shall not be occupied until certification of compliance with the technical specifications for an M4(2) dwelling, as set out in Approved Document M to the Building Regulations (2010) 2015, has been submitted to, and approved in writing, by the Local Planning Authority. All such provisions must remain in place for the life of the building.

Reason: To not only allow the Building Control body to require the development to comply with the optional Building Regulations standards, but to also ensure the appropriate quantity and standard of accessible and adaptable housing is constructed and maintained in accordance with policy D7 of the London Plan.

COUNCIL'S HIGHWAYS OFFICER:

Site Characteristics & Background:

The site is located within a residential catchment in Ickenham. The surrounding properties exhibit extensive frontages with generous on-plot parking facilities and the area is covered by parking controls operating for one hour of the day. The address is positioned some distance from convenient access to a local network of bus routes and local shops which is reflected by a public transport accessibility level (PTAL) rating of 3 which is considered as 'moderate' and as such heightens dependency on private car ownership and usage.

The site is occupied by a substantive detached property with a double garage and generous frontage area which is to be demolished and replaced with a 4/5-bedroom detached single tenure dwelling. The established dual carriageway crossings that serve the site envelope are to remain unaltered.

Parking Provision:

Hillingdon Local Plan: Part 2 Policy - DMT 6 requires that new development will only be permitted where it accords with the council's adopted parking standards unless it can be demonstrated that a deviation from the standard would not result in a deleterious impact on the surrounding road network.

London Plan (2021): Policy T6.1 (Residential Parking) requires that new residential development should not exceed the maximum parking standards as set out in table 10.3.

The maximum requirement for a 3 bedroom 'plus' dwelling is for up to 2 spaces to be provided on-site in order to comply with the adopted parking standard whilst the regional standard demands a lesser requirement of 1 space.

The submission indicates an on-plot parking space provision of a single garage and several additional spaces on the frontage area. This meets and exceeds both of the parking standards which, in this specific case, is considered acceptable as it reduces the potential for untoward on-street parking displacement onto the local roadways resulting from the higher dependency on the private motor car due to the moderate PTAL rating.

In terms of cycle parking there should be a provision of 2 secure and accessible spaces for this scale of rebuild in order to conform to the adopted borough cycle parking standard. This provision is likely to be located within the new garage which would be an acceptable arrangement however this has not been confirmed by the applicant - hence a suitable condition should be applied.

Electric Vehicle Charging Points (EVCP's):

In line with the London Plan (2021), within the final parking quantum there is a requirement for a minimum 20% 'active' EVCP provision with all remaining spaces being designated as 'passive' provisions. In this case, 1 'active' and 1 'passive' space should be provided but again this has not been confirmed by the applicant thereby prompting a relevant condition to be applied.

Vehicular Access Provision:

The existing carriageway crossings are to remain and are considered fit for purpose. There are no further observations.

Operational Refuse Requirements:

Refuse collection will continue via 'Parkfield Road' as is the case at present. In order to conform to accepted 'waste collection distances' from the public highway, the storage area should be positioned within 10m of a refuse vehicle. The applicant has not provided detail on this aspect hence a suitable condition should be applied.

Construction Logistics Plan (CLP):

A full and detailed CLP is a requirement given the constraints and sensitivities of the local road network in order to avoid/minimise potential detriment to the public realm. A further planning condition is therefore required.

Conclusion:

The application has been reviewed by the Highway Authority who are satisfied that the proposal would not discernibly exacerbate congestion or parking stress, and would not raise any measurable highway safety concerns, in accordance with Local Plan: Part 2 Development Plan (2020) Policies DMT 1, DMT 2 & DMT 6 and Policies T4 and T6 of the London Plan (2021).

7. MAIN PLANNING ISSUES

7.01 The principle of the development

Policy DMH 1 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that the net loss of existing self-contained housing, including affordable housing, will be resisted unless the housing is replaced with at least equivalent residential floorspace.

Policy DMH 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) adds that the Council will require the provision of a mix of housing units of different sizes in schemes of residential development to reflect the Council's latest information on housing need. The current information on housing need indicates a substantial borough-wide requirement for larger affordable

and private market units, particularly 3 bedroom properties, as identified in the Strategic Housing Market Assessment 2016.

The principle of residential use has already been established by the existing dwelling at the site. Whilst an existing family sized dwelling (defined in the London Plan (2021) as a three-bedroom or more unit) would be demolished, it would be replaced by a two and half storey, five-bedroom plus dwelling. The proposal would therefore not result in a net loss of family sized housing, in accordance with Policies DMH 1 and DMH 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

Notwithstanding the above, there are other planning considerations that need to be considered which are discussed in the following sections of this Committee Report, including the proposal's impact to the character and appearance of the area (including trees), neighbouring residential amenity, parking and highway safety.

7.02 Density of the proposed development

Policy D3 of the London Plan (2021) states that all development must make the best use of land by following a design-led approach that optimises the capacity of sites. Policy DMHB 17 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that all new residential development should take account of the Residential Density Matrix contained in Table 5.2.

The proposal is for a single replacement dwelling. Numerical density levels are considered to be more appropriate to larger sites and are not typically used in the assessment of schemes of less than 10 units. The key consideration is therefore whether the development would acceptably integrate with the character and appearance of the locality, and would respect residential amenity considerations. Refer to the other sections of this report which assess these planning considerations in further detail.

7.03 Impact on archaeology/CAs/LBs or Areas of Special Character

Not applicable.

7.04 Airport safeguarding

Policy DMAV 1 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that the Council will ensure that uses such as housing, education and hospitals are not located in areas significantly affected by aircraft noise without acceptable mitigation measures.

The application site is within 3km of the RAF Northolt Zone. However, as the site is within an established residential area within this zone, it is considered that visibility and audibility of aircraft operations associated with RAF Northolt would not be of significant harm to the living conditions of future occupiers. It is therefore considered that it would be unreasonable to refuse the application on the ground of harm to the residential amenity of the future occupiers, in respect to aircraft noise associated with RAF Northolt.

It should be noted that the Ministry of Defence Safeguarding Department - RAF Northolt was consulted, and no objection was raised by this Government Body.

7.05 Impact on the green belt

Not applicable.

7.07 Impact on the character & appearance of the area

Paragraph 131 of the NPPF (2023) seeks the creation of high quality, beautiful and sustainable

buildings. Parts b) and c) of paragraph 135 of the NPPF (2023) states that planning policies and decisions should ensure that developments are visually attractive as a result of good architecture and are sympathetic to local character and history, including the surrounding built environment.

Policies D3 and D4 of the London Plan (2021) require development proposals to be of high quality and to enhance the local context by delivering buildings and spaces that positively respond to local distinctiveness.

Policy BE1 of the Hillingdon Local Plan: Part One Strategic Policies (2012) seeks a quality of design in all new development that enhances and contributes to the area in terms of form, scale and materials; is appropriate to the identity and context of the townscape; and would improve the quality of the public realm and respect local character.

Policy DMHB 11 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) states that new development will be required to be designed to the highest standards and incorporate principles of good design.

Policy DMHB 12 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) states that development should be well integrated with the surrounding area.

Parkfield Road is characterised by single storey and two storey dwellings. Over recent years, bungalows on Parkfield Road have been demolished and replaced by two storey dwellings. These replacement dwellings forms part of the character of the area and should be taking into account as material consideration.

It is worth noting that planning application 14459/APP/2021/1343 was for the demolition of a bungalow in replacement of a two and half storey replacement dwelling at number 29 Parkfield Road. Although this application was refused by the Council on the grounds of harm to the character and appearance of the area, this decision was overturned by the Secretary of State by allowed Appeal Decision APP/R5510/W/21/3278249 on 12th November 2021. The Inspector for this allowed Appeal states the following in respect to the effect of the proposed development at No.29 Parkfield Road on the character and appearance of the area.

"4. The appeal property lies approximately mid-way along a long, and largely straight, residential cul-de-sac. There is general agreement between the main parties that Parkfield Road has no distinct or consistent architectural style or facing materials, whilst the scale of buildings is equally varied. Both parties refer to other planning application and appeal decisions for development along Parkfield Road which support this consensus.

5. Thus, although predominantly bungalow-style housing prevails closer to the entrance into Parkfield Road, the scale of housing subtly changes along its length. Thus, bungalows, some oriented with their gable-ends facing the road with first floor windows within the peak and other modest bungalows with hipped roofs, to two storey houses with upper-floor windows set wholly or partly within the roofline to the occasional, larger two storey house.

6. The appeal property and its immediately neighbours are a case in point in this respect. Nos. 27 and 29 are both modest hipped-roof bungalows, set close to one or, as in the case of No. 29 both, side plot boundaries. To one side of No. 27 lie a pair of chalet-bungalows with their two storey gable ends facing towards the road and their higher ridge lines extended rearwards, giving a sense of built depth to their plots. On the other side are three, larger and more dwellings.

7. Of a recognisably larger scale and massing, the latter present one-and-a-half storey (half-) hipped elevations towards the road, flanked by large expanses of pitched hipped roofs, within which a dormer window to each is positioned. However, despite the notable variations there are also some consistent characteristics displayed along the road's length. There is therefore a consistent building line along both sides of the road. Whilst not generous, this nevertheless ensures that there is a pleasant sense of spaciousness and openness about Parkfield Road.

8. The proposed replacement dwelling would be an altogether larger proposition than the existing bungalow and the neighbouring example at No. 27. Moreover, unlike the neighbouring properties to the north, the appeal proposal's full two-storey elevations would mark it out as of a different scale to Nos. 31, 31A and 33.

9. However, it would otherwise reflect other consistent characteristics found along Parkfield Road. It would respect and maintain the building line established by properties along the western side of the road and also maintain the gaps between frontages on either side of the road. The dwelling, despite its substantial overall height, would feature sloping roofs to both side-facing roof planes giving it a hipped-roof appearance, whilst the projecting two-storey bays would also incorporate hipped roofs over.

10. Whilst the transition from the vertical emphasis of the proposed two storey facade to the extensive sloping roof of No. 31 would be somewhat abrupt, it would not be harmfully so in the context of the projecting bays at the front of those neighbouring properties where such juxtaposition is not uncommon. Nor would the transition in scale from No. 27 to No. 29 be unduly harsh, despite their very different scale due to the flat-roofed single storey element at the side of the proposed dwelling. In the context of the street, such single storey flat roofed buildings, or parts of buildings, are not uncommon and this element of the proposed scheme would not appear any more or less so than others.

11. As a corollary of the proposal's greater scale, the Council are also concerned about the dwelling's greater depth; greater in comparison with the existing property and greater in comparison with those around it. It may well be the case that the proposed dwelling would have a deeper footprint in both cases, although such a claim is not entirely clear from the submitted site and block plans. In any event, even if that were the case, I am not persuaded that it would be harmful to the character or appearance of the area.

12. As described above, amongst the mix of buildings, there are those with clear and apparent depth, such as the chalet-bungalows whose gable elevations face the road and ridges run away perpendicular to the road. Approaching from the south, the view towards the appeal property is reasonably open above No. 27 from where the depth of the appeal proposal would be noticeable, but it would not be disruptive to the pattern of development along Parkfield Road. Nor would it be particularly evident when viewed from the north where the greater scale and bulk of Nos. 31 - 33 would screen its depth.

13. The proposed dwelling is not a beautiful or innovative dwelling, but nor does it purport to be. The front elevation would be well articulated and provide interest to the streetscene whilst incorporating features and details found variously along the road. It would not, as the Council assert, appear squat or bland, whilst final details of construction and fenestration materials could be satisfactorily dealt with by way of an appropriately worded planning condition.

14. Local Plan: Part 2 (LP2) policies DMHB11 and DMHB12 set out the Council's approach to securing high quality design. As I have set out above, the proposal would take account of the scale, architectural style and character of the surrounding area. As a consequence, I am satisfied that the proposal would not harm the character or appearance of the area and there would be no conflict with LP2 policies DMHB11 or DMHB12. Nor would there be with the design aims of policies D3, 4 or 6 of the London Plan or the Framework.

15. In noting the various appeal decisions and other schemes referred to by both main parties, I am conscious of the particular contexts within which those properties and sites lie. I accept that the properties at Nos. 2 and 58 lie more towards the opposite ends of Parkfield Road and are therefore not seen in quite the same mid-street context as the appeal site. Nevertheless, this does not diminish their respective assessments of the generally varied nature of Parkfield Road. I am satisfied though that, for the reasons I have set out above, the proposed dwelling would not be in conflict with the aims and provisions of LP2 policies DMHB11 or DMHB12. Nor would there be with the design aims of policies D3, 4 or 6 of the London Plan or the Framework."

Turning back to the current application, the proposed replacement dwelling at No.39 Parkfield Road would measure 15 metres wide, 16 metres deep and 8.6 metres high. Its dimensions would not be dissimilar to the scheme at No.29 Parkfield Road, which was allowed at Appeal and recently constructed. Additionally, it was observed during the Officer site visit that large replacement dwellings have been constructed at Nos. 54, 54a and 55 Parkfield Road. Given this context, and attaching weight to the above Appeal Decision, it is considered that the size of the proposed replacement dwelling at No.39 would not be out of character to the surrounding area. It is acknowledged that the proposed replacement dwelling would be two and half storeys and would be sited in between two bungalows at Nos. 37 and 41 Parkfield Road. However, it is worth highlighting that the existing dwelling at the application site is two storeys and approximately 8m in height. The scale of the proposed development would therefore not be substantially different to the existing dwelling at the site, albeit the proposed replacement dwelling would be larger in size. Furthermore, the proposed replacement dwelling would be set-in from the side boundaries shared with Nos. 37 and 31 by 1 and 1.5 metres, respectively. It is therefore considered that the scale of the proposed dwelling would be appropriate to its plot size and its surrounding context, noting the two storey dwellings at Nos. 33 and 43 Parkfield Road.

Following negotiations, revised drawings were submitted showing the proposed replacement dwelling sited in alignment with the front building line of No.37 Parkfield Road. As such, it is considered that the revised proposal would respect the pattern of development of the local area. The two storey rear building line of the proposed dwelling would be partly set-back from the ground floor rear building line. This helps to ensure that the overall bulk and massing of the proposed replacement dwelling is proportionately sized in relation to its site context. Furthermore, the ridge height of proposed dwelling would be the same height as the neighbouring two storey dwelling at No.43 Parkfield Road. Thus, ensuring that the varied but established roof lines on Parkfield Road remains intact.

Whilst crown roof profiles are generally resisted from a design perspective, the flat centre section of the proposed crown roof profile has been reduced during the course of this application. This, in turn, has helped to reduce its prominence when viewed from the street scene and from the rear gardens of neighbouring properties. Also, there are existing properties on Parkfield Road that contain crown roof profiles (notably Nos. 29, 55, 54 and 54a). It is therefore considered, on balance, that the proposed crown roof profile would not be harmful to the character of the street scene.

In terms of architectural style, the proposed replacement dwelling would include a two-storey front projection with full height glazing on its upper level. Two-storey front gable features are not an uncommon feature on the more recently constructed replacement dwellings on Parkfield Road. As such, it is considered that the design of the proposed dwelling would be in keeping with the emerging street scene character.

In the event of approval, a condition would be secured requiring details of the external finishes and materials to be submitted to the Council for consideration.

For the reasons above, it is considered that the proposed development would not cause harm to the character, appearance and visual amenities of the street scene or the surrounding area. The proposal therefore accords with Policy BE1 of the Hillingdon Local Plan: Part One Strategic Policies (2012), Policies DMHB 11 and DMHB 12 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) and Policies D3 and D4 of the London Plan (2021) and the NPPF (2023).

7.08 Impact on neighbours

Policy DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states: B) Development proposals should not adversely impact on the amenity, daylight and sunlight of adjacent properties and open space.

Specifically, paragraph 5.38 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states: "The Council will aim to ensure that there is sufficient privacy for residents and it will resist proposals where there is an unreasonable level of overlooking between habitable rooms of adjacent residential properties, schools or onto private open spaces. A minimum of 21 metres separation distance between windows of habitable rooms will be required to maintain levels of privacy and to prevent the possibility of overlooking. In some locations where there is a significant difference in ground levels between dwellings, a greater separation distance may be necessary."

Paragraph 5.40 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states: "For the purposes of this policy (Policy DMHB 11), outlook is defined as the visual amenity enjoyed by occupants when looking out of their windows or from their garden. The Council will expect new development proposals to carefully consider layout and massing in order to ensure development does not result in an increased sense of enclosure and loss of outlook."

The main two-storey front wall of the proposed replacement dwelling would be positioned in alignment with No. 37 Parkfield Road. It would project beyond the front elevation of No.41 Parkfield Road by approximately 3.7 metres. However, the proposed front wall would be set-in from the closest front elevation window at No.41 Parkfield Road by 4 metres, and would not breach their 45- degree sight line. The proposed two storey front gable feature would have a modest depth, projecting approximately 0.8 metre beyond the proposed main front wall. Taking these factors into account, it is considered that the proposed replacement dwelling would not adversely impact on the levels of natural light and outlook from the front elevation windows at Nos. 37 and 41 Parkfield Road.

At two storey level, the proposed rear building line would project beyond the extended rear wall of No.37 by 0.7 metres. The proposed single storey rear building line would project an additional 2 metres; however, this element of the proposal would be set-in from the shared boundary with No.37 in excess of 7 metres. Also, No.37 contains a detached outbuilding along the shared boundary which projects beyond the depth of the proposed single storey rear building line. Upon Officer's request, revised drawings were submitted to demonstrate that the proposed two-storey rear building line would

not breach the 45-degree line of sight taken from the nearest windows at No. 37 Parkfield Road. No.41 Parkfield Road is set deeper into its plot, which means its extended rear building line projects beyond that of the proposed replacement dwelling. Given these circumstances, it is considered that the proposal would not unduly impact on the levels of outlook and light from the rear elevation windows at Nos. 37 and 41 Parkfield Road.

During the Officer site visit it was observed that both Nos. 37 and 41 contain habitable windows within their respective flank walls which faces onto the side boundaries of the application site. However, these existing side windows face directly onto closed boarded fencing, which means the outlook from these windows is already limited. To support this, additional plans were requested (39PR/P100 Rev B) with 25-degree sight lines from the habitable neighbouring windows which demonstrate that with 2m high boundary fencing, views from the windows would be primarily of the fencing. In addition, the applicant has submitted a Daylight and Sunlight Assessment which demonstrates the proposed development would be compliant with Building Research Establishment (BRE) guidelines. Concerns regarding the assessment were raised by residents as it was established that the obscured glazed side window also serves a bedroom. A revised Daylight and Sunlight assessment was submitted which took this into account. Given Window 1 and Window 2 serve the same bedroom, the area weighted average was used to confirm compliance with BRE guidelines. Notwithstanding this, given the secondary flank window is already obscurely glazed, outlook is already compromised and it is considered by Officers that the level of daylight lost would not warrant a reason for refusal. As the applicant has also demonstrated that the bedroom as a whole would continue to comply with the guidance set out in the BRE Research Establishment which allows for average daylight calculation to be taken where a room is served by 2 windows on different elevations. It is considered that the proposal would result in a minor reduction of daylight and sunlight to the neighbouring windows of number 37 and 41 Parkfield Road. In all cases, the reduction remains better than the minimum recommendations of the BRE guidelines and would therefore not unduly impact on the levels of outlook and light.

In addition to this, the proposed replacement dwelling would be single storey along the mutual boundary shared with No.37, with the proposed two storey flank wall set-in 3.4 metres from this neighbour. Whilst the proposed-two storey flank wall closest to No.41 would be set-in 1.5 metres from the mutual boundary, there would be a total separation distance of 4 metres between the proposed dwelling and the side windows at No.41. It is therefore considered, on balance, that the proposal would not result in a significant loss of light or outlook for the side windows at Nos.37 and 41 over and above the existing situation. As such, a refusal on this particular ground would be difficult to defend in the event of an Appeal.

There would be a separation distance of approximately 23 metres between the front elevation of the proposed replacement dwelling and the neighbouring properties at Nos. 33 and 38 Parkfield Road. There would be a separation distance in excess of 30 metres between the rear elevation of the proposed dwelling and the rear gardens of the properties at 42 to 46 Rectory Way. Given these separation distances, it is considered that the proposal would not unduly impact on the residential amenities of these particular neighbouring properties.

If planning permission were to be granted, the first floor side window (which serves a bathroom) would be obscure glazed and non-opening up to 1.8 metres of the finished floor level. This condition is required to prevent any issues in respect to loss of privacy or overlooking for neighbouring occupiers. The condition would also be applicable to the proposed first floor en-suite window to protect the modesty of future occupiers.

The concerns raised by neighbouring residents have been duly noted. However, having regard to the above, it is considered that the proposal would not cause undue harm to the living conditions of neighbouring occupiers, in accordance with Policy DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

7.09 Living conditions for future occupiers

INTERNAL AMENITY SPACE:

Policy D6 of the London Plan (2021) sets out the minimum internal floor space standards required for residential developments in order to ensure that there is an adequate level of amenity for future occupants.

Policy DMHB 16 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that all housing development should have an adequate provision of internal space in order to provide an appropriate living environment.

Table 3.1 of the London Plan (2021) requires a three-storey dwelling containing a 3 bedroom, 5 person unit to have a minimum of 93 square metres. Policy D6 of the London Plan (2021) states that a one bed space single bedroom must have a floor area of at least 7.5 square metres, and a two bedroom space double (or twin room) must have at least 11.5 square metres.

It is recognised that the proposed attic rooms labelled as 'Storage' would be in excess of 11.5 square metres, and therefore could be used flexibly as additional double bedroom. Also, the proposed attic room labelled as 'Medium Room' could be flexibly used as a single bedroom as it would be in excess of 7.5 square metres. It is therefore considered prudent to assess their implications as bedrooms on the quality of living accommodation.

Policy D6 of the London Plan (2021) does not stipulate the minimum requirement for a house of the occupancy size being proposed. However, with the proposed replacement dwelling benefitting from a gross internal floor area of 449 square metres, its overall internal floor space is considered to be more than sufficient.

It is also noted that the gross internal floor area of the proposed replacement dwelling meet the minimum floor space standards in Table 3.1 of the London Plan (2021), i.e. 138 square metres for a 6 bedroom, 8-person three-storey dwelling. All the proposed bedrooms (including the habitable rooms within the loft space) would exceed the minimum floor area standards set out in parts 3) and 4) of the London Plan (2021).

It is noted that the habitable rooms within the loft space would be served by roof lights, and would therefore not be afforded with direct outlook. However, bedrooms are primarily used for sleeping and can be differentiated from the primary living space located on the ground floor level of the proposed dwelling. The guest bedroom at ground floor level and the primary proposed bedrooms at first floor level would be served by windows fitted in the front or rear elevation of the building. It is therefore considered, on balance, that the overall internal amenity space for future occupiers would be acceptable, in respect to outlook.

On the above basis, it is considered that the proposed replacement dwelling would provide a satisfactory provision of internal amenity space for future occupants, in accordance with Policy DMHB 16 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) and Policy D6 of the London Plan (2021).

EXTERNAL AMENITY SPACE:

Policy DMHB 18 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that all new residential development and conversions will be required to provide good quality and useable private outdoor amenity space. Amenity space should be provided in accordance with the standards set out in Table 5.3, which requires houses with four bedrooms or more should provide a minimum of 100 square metres of private usable amenity space.

The proposed replacement dwelling would be provided with private amenity space in the form of a rear garden which would be in excess of 500 square metres. Thus, far exceeding the minimum private amenity space standards found in Table 5.3 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020). It is considered that the proposed rear garden would provide a generous amount of space and opportunity for general outdoor activity that future occupants of the proposed family sized dwelling could reasonably expect. The proposal would provide the future occupiers of the proposed replacement dwelling with external private amenity provision that is of a sufficient size, usability and functionality, in accordance with Policy DMHB 18 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

7.10 Traffic impact, Car/cycle parking, pedestrian safety

ACCESS, PARKING AND HIGHWAY SAFETY:

Paragraph 115 of the NPPF (2023) states that development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.

Policy T4 of the London Plan (2021) states that where appropriate, mitigation, either through direct provision of public transport, walking and cycling facilities and highways improvements or through financial contributions, will be required to address adverse transport impacts that are identified.

Policy T6 of the London Plan (2021) states that new residential development should not exceed the maximum parking standards set out in Table 10.3. All residential car parking spaces must provide infrastructure for electric or Ultra Low Emission vehicles. At least 20 per cent of spaces should have active charging facilities, with passive provision for all remaining spaces.

Policy DMT 1 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that development proposals will be required to meet the transport needs of the development and address its transport impacts in a sustainable manner.

Policy DMT 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that proposals must ensure that safe and efficient vehicular access to the highway network is provided, schemes do not contribute to the deterioration of air quality, noise or local amenity or safety of all road users and residents. Also, that impacts on local amenity and congestion are minimised and there are suitable mitigation measures to address any traffic impacts in terms of capacity and functions of existing roads.

Policy DMT 6 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) requires that proposals comply with the Council's parking standards in order to facilitate sustainable development and address issues relating to congestion and amenity. There are two vehicle crossovers emerging onto Parkfield Road which serves the existing dwelling at the site. The proposal

does not seek to make any alterations to these existing crossovers, and it is considered by the Council's Highways Officer that their current dimensions are adequate to accommodate the vehicular activity associated with the proposed development.

The maximum requirement for a four-bedroom or more dwelling is up to 2 car parking spaces to be provided on-site to comply with the adopted parking standard set out in Appendix C, Table 1 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020). It is worth noting that Policy T6.1 of the London Plan (2021) states that development comprising 3 plus bed units in outer London with a PTAL rating of 3 should provide a maximum of up to 1 space dwelling, which is a lesser requirement than the Council's adopted car parking standards. The submitted plans shows that there is sufficient space to on the front court to accommodate three car parking spaces owing to the generous plot size. An additional parking space would be provided within the proposed garage. The proposal would therefore provide up to 4 car parking spaces. Whilst this would exceed the maximum car parking standards set at local and regional level, the number of proposed car parking spaces would not be dissimilar to the existing provision at the site.

The Council's Highways Officer has commented that in this individual case the proposed on-site car parking is acceptable as it would reduce the potential for untoward on-street parking displacement resulting from the higher dependency on the private motor car, due to the site's level 3 PTAL rating (Moderate). In line with Policy T6.1 of the London Plan (2021), there is a requirement for a minimum of 20% 'active' electric vehicle charging points provision within the final parking quantum of development, with all remaining spaces being designated as 'passive' provisions. In the event of an approval, a condition would be secured requiring 1 active and 3 passive electric vehicle charging points (in compliance with Policy T6.1 of the London Plan (2021)). When comparing the proposed development to the scale and single tenure of the existing dwelling, it is considered that there would be no significant difference in vehicle bound activity. As such, the vehicular generated activity associated with the proposed development could be absorbed within the local road network without notable detriment to traffic congestion and road safety.

In the event of an approval, a condition would be secured requiring the submission of a Construction Management Plan to be submitted to the Council for consideration. This condition is considered to be necessary in order to minimise/avoid potential detriment to the public realm and local highway network. Accordingly, it is considered, on balance, that the proposed development would provide an acceptable number of on-site car parking spaces, and that the demand for street parking would not be exacerbated to such an extent that would prejudice highway safety.

The proposal would therefore accord with the objectives of Policies DMT 1, DMT 2 and DMT 6 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), Policies T4, T6 and T6.1 of the London Plan (2020) and paragraph 111 of the NPPF (2023).

BICYCLE PROVISION:

Policy DMT 5 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) requires development proposals to provide a provision of cycle parking in accordance with the standards set out in Appendix C, Table 1 (i.e. 2 cycle spaces per 3 or more bed unit). Policy T5 of the London Plan (2021) also requires two cycle spaces to be provided for a proposal development of this housing size.

No details have been submitted in respect to on-site cycle store provision. However, Officers are fully satisfied that this matter could be dealt with through a condition as there is more than sufficient space

within the rear garden to accommodate a cycle store, without undermining the minimum requirement of 100 square metres of private external amenity space.

Subject to the above condition, it is considered that the proposal would provide covered, secure and accessible cycle parking spaces for future occupants in compliance with Policy DMT 5 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) and Policy T5 of the London Plan (2021).

7.11 Urban design, access and security

Covered in other sections of this Committee Report.

7.12 Disabled access

Policy D5 of the London Plan (2021) states that development proposals should achieve the highest standards of accessible and inclusive design. Policy D7 of the London Plan (2021) states that at least 10 per cent of dwellings (which are created via works to which Part M volume 1 of the Building Regulations applies) must meet Building Regulation requirement M4(3) 'wheelchair user dwellings'. All other dwellings (which are created via works to which Part M volume 1 of the Building Regulations applies) must meet Building Regulation requirement M4(2) 'accessible and adaptable dwellings'.

In the event of an approval, a condition would be secured requiring the proposed replacement dwelling to meet Category M4(2) of Approved Document M to the Building Regulations (2010) 2015. Also, a condition would be secured requiring details showing step-free access via the entrance points of the proposed dwelling to be submitted to the Council for consideration. Subject to such conditions, the proposed development would comply with Policies D5 and D7 of the London Plan (2021).

7.13 Provision of affordable & special needs housing

Policy H2 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012) states that housing provision is expected to include a range of housing to meet the needs of all types of households, and the Council will seek to maximise the delivery of affordable housing from all sites over the period of the Local Plan. For sites with a capacity of 10 or more units, the Council will seek to ensure that the affordable housing mix reflects housing needs in the borough, particularly the need for larger family units. This is supported by Policy DMH 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

The proposal is for a single replacement dwelling and therefore does not meet the threshold in order to require affordable housing provision. As such, the proposal is not contrary to Policy H2 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012) and Policy DMH 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) in respect of affordable housing provision.

7.14 Trees, landscaping and Ecology

TREES AND LANDSCAPING:

Policy G1 of the London Plan (2021) states that development proposals should incorporate appropriate elements of green infrastructure that are integrated into London's wider green infrastructure network.

Policy DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that landscaping and tree planting should enhance amenity, biodiversity and green infrastructure.

This is supported by Policy DMHB 14 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) which states:

A) All developments will be expected to retain or enhance existing landscaping, trees, biodiversity or other natural features of merit.

B) Development proposals will be required to provide a landscape scheme that includes hard and soft landscaping appropriate to the character of the area, which supports and enhances biodiversity and amenity particularly in areas deficient in green infrastructure.

The site is covered by Tree Preservation Order 514. Upon Officers' request, the applicant submitted a report comprising an Arboricultural Survey to BS5837:2012 report. The report states that a total of 4 No. individual trees, 1 No. group of trees and 3 No. hedges were surveyed. From the trees surveyed, there was 1 x Category A tree (English Oak), 2 x Category C trees (Dappled Willow and Viburnum), a group of mixed Category C trees and 1 x U Category tree (Japanese Cherry). Upon Officers' request, an Arboricultural Method Statement, Impact Assessment and Tree Protection Plan was submitted. The Method Statement no individual or groups of trees are proposed to be removed. A hedge would be removed. There is no objection to the loss of this hedge, given its relatively low visual amenity value. Replacement planting would be secured through a landscaping condition, in the event of an approval. The Tree Protection Plan shows that the footprint of the proposed dwelling would not encroach the root protection area of the retained trees, and the locations where protection fencing would be erected. A compliance condition requiring the construction works to be carried out in accordance with the tree protections measures outlined in the Arboricultural Method Statement, Impact Assessment and Tree Protection Plan would be secured by condition, if planning permission were to be granted. Subject to the above conditions, it is considered that the proposed development would not cause harm to any trees of high visual amenity value, and that the proposed soft landscaping would help to soften the overall appearance of the proposed development. The proposal would therefore accord with Policies DMHB 11 and DMHB 14 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) and Policy G1 of the London Plan, in this respect.

ECOLOGY:

Policy DMEI 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that if development is proposed on or near to a site considered to have features of ecological or geological value, applicants must submit appropriate surveys and assessments to demonstrate that the proposed development will not have unacceptable effects. The development must provide a positive contribution to the protection and enhancement of the site or feature of ecological value.

The site does not contain any ponds, open woodland or dense scrub and shrubbery. There are no protected sites of ecological interest adjacent to the site. It is therefore considered that the likelihood of protected species being present at the site is low, and as such, an ecology assessment is not required. This position would be in accordance with 'Circular 06/05: Biodiversity and Geological Conservation- Statutory Obligations and their Impact within the Planning System' which states that, "...bearing in mind the delay and cost that may be involved, developers should not be required to undertake surveys for protected species unless there is a reasonable likelihood of the species being present and affected by the development." In the event of an approval, an informative would be secured advising that should protected species be found at the site, the applicant(s) must fulfil their duties under the Wildlife and Countryside Act 1981 (as amended) and the Conservation of Habitats and Species Regulations 2017 (as amended)

7.15 Sustainable waste management

Policy DMHB 11 part (d) of the Hillingdon Local Plan: Part 2 - Development Management Policies

(2020) states that development proposals should make sufficient provision for well designed internal and external storage space for general, recycling and organic waste, with suitable access for collection. External bins should be located and screened to avoid nuisance and adverse visual impacts to occupiers and neighbours.

The Council's Highways Officer has commented that in order to conform to accepted 'waste collection distances' from the public highway, the storage area should be positioned within 10 metres of a refuse vehicle. Whilst no specific details have been provided in respect to refuse and recycling storage, Officers are satisfied that this matter could be dealt with by way of a condition, if planning permission were to be granted. The servicing of the proposed replacement dwelling by the Council's refuse collection crew would be in the same manner as the existing properties on Parkfield Road (which includes the existing property at the application site). Subject to the above condition, it is considered that the proposed development would be able to provide a convenient location for refuse and recycling facilities in accordance with Policy DMHB 11 part (d) of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

7.16 Renewable energy / Sustainability

In order to meet sustainability requirements a condition would be secured requiring the proposed dwelling to achieve as a minimum, a water efficiency standard of no more than 110 litres per person per day maximum water consumption (to include a fixed factor of water for outdoor use of 5 litres per person per day in accordance with the option requirement defined within Approved Document G of the Building Regulations). Further conditions are also outlined within the air quality section of this report which would further contribute to reducing carbon and providing more sustainable energy provision.

Subject to the above conditions, the proposal would be compliant with Policies DMEI 2 and DMEI 10 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) and Policy SI 2 of the London Plan (2021).

7.17 Flooding or Drainage Issues

Policy SI 12 of the London Plan (2021) states that development proposals should ensure that flood risk is minimised and mitigated, and that residual risk is addressed. Policy SI 13 of the London Plan (2021) states that development proposals should aim to achieve greenfield run-off rates and ensure that surface water run-off is managed as close to its source as possible.

Policy DMEI 9 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that proposals that fail to make appropriate provision for flood risk mitigation, or which would increase the risk or consequences of flooding, will be refused.

Policy DMEI 10 states that development within areas identified at risk from surface water flooding, which fail to make adequate provision for the control and reduction of surface water runoff rates will be refused.

The site lies within Flood Zone 1 of the Environment Agency's Flood Risk Map. This means the site is classified as being at low risk and defined as having a less than 1 in 1,000 probability of fluvial and tidal flooding. As such, there are no restrictions on development, including more vulnerable uses such as Use Class C3 (dwellinghouses), in this location, in terms of fluvial and tidal flood risk. In the event of an approval, a condition would be secured requiring the submission of a sustainable water management scheme, that incorporates sustainable urban drainage systems (SuDs), to be submitted to the Council for consideration. Also, the landscaping condition has been worded in such a manner

to ensure that permeable hard surfacing is used for the parking areas serving the proposed dwelling.

Subject to such conditions, it is considered that the proposed development would not increase the risk of flooding on the site or elsewhere, in accordance with Policies DMEI 9 and DMEI 10 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) and Policies SI 12 and SI 13 of the London Plan (2021).

7.18 Noise or Air Quality Issues

NOISE:

Policy D14 of the London Plan (2021) states that in order to reduce, manage and mitigate noise to improve health and quality of life, residential and non-aviation development proposals should manage noise by avoiding significant adverse noise impacts on health and quality of life. The site would be used in an exclusively residential capacity, as per the existing situation. As such, in terms of the operational phase of the proposed development, no significant issues are considered to be raised by the proposal, in respect to noise.

Notwithstanding the above, a condition would be secured requiring the submission of a Construction Management Plan, in the event of an approval. This condition is necessary to ensure that noise and pollution are minimised as far as practicable during the construction phase.

AIR QUALITY:

Policy DMEI 14 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that proposals should demonstrate appropriate reductions in emissions. It adds that, development proposals should, as a minimum:

- i) be at least "air quality neutral";
- ii) include sufficient mitigation to ensure there is no unacceptable risk from air pollution to sensitive receptors, both existing and new; and
- iii) actively contribute towards the improvement of air quality, especially within the Air Quality Management Area.

The London Plan has produced further guidance on Air Quality Neutral in February 2023. Paragraph 2.3 of the guidance states that for minor developments "a full air quality assessment will likely not be required. The guidance provides a simplified procedure for developments in meeting Air Quality Neutral.

The simplified procedure states where minor developments include new heating systems, they can be assumed to meet the Building Emissions Benchmark (BEB). These new heating systems could include one of the following

- * heat pump or other zero-emission heat source
- * the new heating system includes one or more individual gas boiler with NOx emissions rated at less than 40mg/kWh
- *the development is connecting to an existing heat network.

The London Plan guidance also states in para 4.1 where minor developments include new parking, they can be assumed to be meet Transport Emissions Benchmark (TEB) if the maximum parking standards set out in policies T6 are not exceeded.

In regards the current application, the proposal involves replacing an existing dwelling with no net increase in the numbers of units within the site curtilage. The development would continue to provide similar parking arrangement to the existing dwelling and the overall parking movements associated

with the development would be limited. As such, the development is unlikely to have any significant adverse air quality impact.

In terms of building emissions, a condition has been included requiring any new heating system to the new dwelling to be from a zero-emission rated at less than 40mg/kWh; or to be connected to an existing heat network. This would ensure an Air Quality Neutral minor development, in accordance with London Plan Guidance: Air Quality Neutral.

Also, a condition would be secured requiring the submission of a Construction Management Plan (as noted above) to minimise air and other emissions caused during the construction phase. In light of these conditions, and noting the minor scale of the proposal, it is considered that it would not be necessary, fair or reasonable to require an air quality contribution to be secured through a legal agreement.

7.19 Comments on Public Consultations

Covered in other sections of this Committee Report.

7.20 Planning obligations

Policy DMCI 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that to ensure development is sustainable, planning permission will only be granted for development that clearly demonstrates there will be sufficient infrastructure of all types to support it. Infrastructure requirements will be predominantly addressed through the Council's Community Infrastructure Levy (CIL).

The Council adopted its own Community Infrastructure Levy (CIL) on 1st August 2014. The Hillingdon CIL charge for residential developments is £95 per square metre of additional floor space. This is in addition to the Mayoral CIL charge of £60 per square metre. CIL rates are index linked. The proposal involves the erection of a new replacement dwelling and is therefore CIL liable, if planning permission were to be granted.

7.21 Expediency of enforcement action

Not applicable.

7.22 Other Issues

None.

8. Observations of the Borough Solicitor

General

Members must determine planning applications having due regard to the provisions of the development plan so far as material to the application, any local finance considerations so far as material to the application, and to any other material considerations (including regional and national policy and guidance). Members must also determine applications in accordance with all relevant primary and secondary legislation.

Material considerations are those which are relevant to regulating the development and use of land in the public interest. The considerations must fairly and reasonably relate to the application concerned.

Members should also ensure that their involvement in the determination of planning applications adheres to the Members Code of Conduct as adopted by Full Council and also the guidance contained in Probity in Planning, 2009.

Planning Conditions

Members may decide to grant planning consent subject to conditions. Planning consent should not be refused where planning conditions can overcome a reason for refusal. Planning conditions should only be imposed where Members are satisfied that imposing the conditions are necessary, relevant to planning, relevant to the development to be permitted, enforceable, precise and reasonable in all other respects. Where conditions are imposed, the Council is required to provide full reasons for imposing those conditions.

Planning Obligations

Members must be satisfied that any planning obligations to be secured by way of an agreement or undertaking pursuant to Section 106 of the Town and Country Planning Act 1990 are necessary to make the development acceptable in planning terms. The obligations must be directly related to the development and fairly and reasonably related to the scale and kind to the development (Regulation 122 of Community Infrastructure Levy 2010).

Equalities and Human Rights

Section 149 of the Equalities Act 2010, requires the Council, in considering planning applications to have due regard to the need to eliminate discrimination, advance equality of opportunities and foster good relations between people who have different protected characteristics. The protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The requirement to have due regard to the above goals means that members should consider whether persons with particular protected characteristics would be affected by a proposal when compared to persons who do not share that protected characteristic. Where equalities issues arise, members should weigh up the equalities impact of the proposals against the other material considerations relating to the planning application. Equalities impacts are not necessarily decisive, but the objective of advancing equalities must be taken into account in weighing up the merits of an application. The weight to be given to any equalities issues is a matter for the decision maker to determine in all of the circumstances.

Members should also consider whether a planning decision would affect human rights, in particular the right to a fair hearing, the right to respect for private and family life, the protection of property and the prohibition of discrimination. Any decision must be proportionate and achieve a fair balance between private interests and the public interest.

9. Observations of the Director of Finance

Not applicable.

10. CONCLUSION

For the reasons set out in this Committee Report, it is considered that the revised proposal would comply with the objectives of national, regional and local planning policies and guidance. It is therefore recommended that the application be approved, subject to the imposition of the conditions set out in Section 2 of the Committee Report.

11. Reference Documents

National Planning Policy Framework (December 2023)

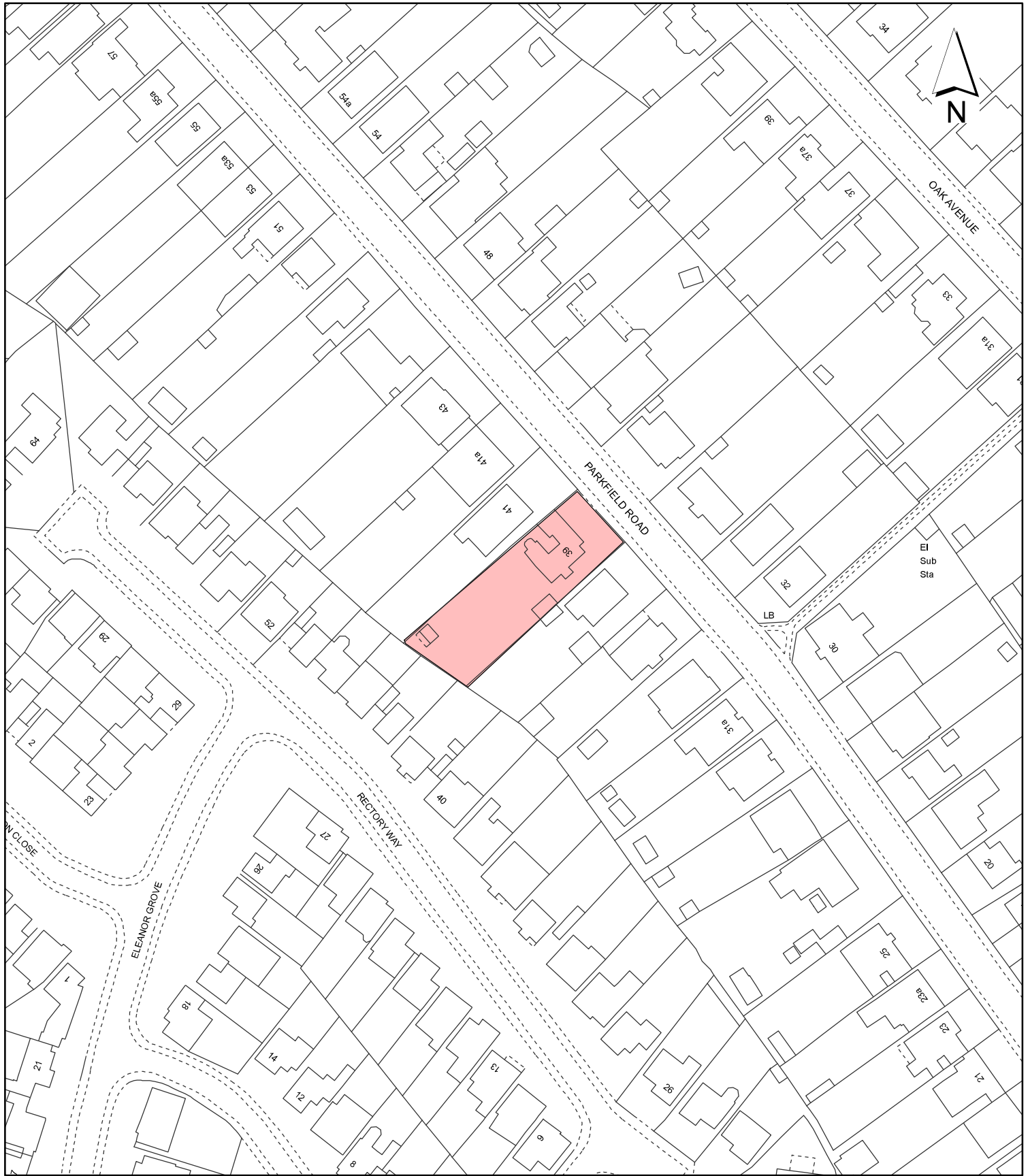
The London Plan (March 2021)

Hillingdon Local Plan: Part One - Strategic Policies (November 2012)

Hillingdon Local Plan: Part Two - Development Management Policies (January 2020)

Hillingdon Local Plan Accessible Hillingdon Supplementary Planning Document (September 2017)
Technical Housing Standards - Nationally Described Space Standard (March 2015)
Circular 06/05: Biodiversity and Geological Conservation - Statutory Obligations and Their Impact
Within The Planning System

Contact Officer: Emilie Bateman **Telephone No:** 01895 250230



Notes:

 Site boundary

For identification purposes only.
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Site Address:

39 Parkfield Road

LONDON BOROUGH OF HILLINGDON
Residents Services Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW
 Telephone No.: Uxbridge 01895 250111

Planning Application Ref:
24825/APP/2023/81

Scale:
1:1,250

Planning Committee:
Minor Page 100

Date:
March 2024



Report of the Head of Development Management and Building Control Committee Report Part 2 – Application Report

Case Officer: Chris Brady	4482/APP/2022/213
----------------------------------	--------------------------

Date Application Valid:	24-01-22	Statutory / Agreed Determination Deadline:	12/08/24
Application Type:	Full	Ward:	Belmore

Applicant: Sivakumaran, Yoganathan & Jeyakumar
 Site Address: **152-154 Uxbridge Road, Hayes, UB4 OJH**

Proposal: **Erection of three storey mixed use retail and 9 residential apartments with ancillary parking, amendments to dropped kerbs, refuse and bicycle storage, following the demolition of existing buildings.**

Summary of Recommendation: **GRANT planning permission subject to section 106 legal agreement and conditions**

Reason Reported to Committee: **Required under Part 3 of the Planning Scheme of Delegation (Petition received)**



Summary of Recommendation:

GRANT planning permission subject to the conditions set out in Appendix 1.

That delegated powers be given to the Director for Planning, Regeneration and Public Realm to GRANT planning permission subject to the following:

A) That the Council enter into a legal agreement with the applicant under Section 106 of the Town and Country Planning Act 1990 (as amended) or any other legislation to secure the following:

1. Restriction upon future residents preventing them from obtaining an on-street parking permit within the existing adjoining Parking Management Scheme and any future expanded Scheme.

2. Air Quality Offsetting Contribution of £28,088.

3. Amendment to the traffic order to facilitate servicing and delivery to be undertaken on street funded by the applicant.

4. Project Management & Monitoring Fee: A financial contribution equal to 5% of the total cash contributions.

B) That in respect of the application for planning permission, the applicant meets the Council's reasonable costs in the review and preparation of the legal agreement and any abortive work as a result of the agreement not being completed.

C) That Officers be authorised to negotiate and agree the detailed terms of the proposed agreement and conditions of approval.

D) If the Legal Agreements have not been finalised within 6 months of 02/10/24, delegated authority be given to the Director of Planning, Regeneration and Public Realm to refuse the application for the following reason:

'The applicant has failed to provide measures to mitigate the impacts of the development in relation to the highway network and air quality as a consequence of demands created by the proposed development. The proposal therefore conflicts with the Town and Country Planning (General Permitted Development) (England) Order 2015, Policies DMEI 7, DMEI 14, DMT 6 and of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), the Council's Planning Obligations SPD and the Hillingdon

Hillingdon Planning Committee – 16th July 2024

PART 1 – Members, Public & Press

Local Plan: Part 1 - Strategic Policies (November 2012), the London Plan (2021) and the NPPF.'

E) That if the application is approved, the following conditions be imposed subject to changes negotiated by the Director of Planning, Regeneration and Public Realm prior to issuing the decision.

1 Executive Summary

- 1.1 Planning permission has been sought for the erection of a three-storey mixed use building following the demolition of existing buildings on the site. The proposed building would comprise a retail unit at ground floor with 9 residential apartments above. The proposal also includes ancillary parking, amendments to dropped kerbs and the provision of refuse and bicycle storage.
- 1.2 In terms of the principle of the development, the proposal includes a net increase of 8 additional dwellings on the site, 4 of which would be family sized units therefore contributing towards addressing an identified need within the Borough. The provision of additional housing at the site is therefore supported. The development would also include an enlarged retail space at ground floor. The enlarged space would cater for a broader range of occupiers and would provide them with a contemporary shop, with private service yard. The enlarged and improved retail space would support the local economy as well as the vitality and viability of the local shopping parade.
- 1.3 Turning to design, during the processing of the application, concerns were raised regarding the design of the proposed new building. To address the design concerns revised plans have been submitted which illustrate detailing has been added to the gable ends and the external finishes of the building have been altered to break up the bulk of its side elevation and to simplify its design. The design of the building is now considered to be acceptable. The building has also been carefully designed to imitate the existing building's appearance when viewed from Uxbridge Road, it would therefore have an acceptable impact on the appearance of the terrace in which it is sited. Furthermore, the building would replace the unsightly rear service yard, fencing and signage that currently exist at the site, cluttering and harming the area's appearance. The development is therefore considered to enhance the appearance of the area.
- 1.4 Conditions (requiring obscure glazing and privacy screens in appropriate locations) are recommended to ensure that the development causes no harm to neighbouring amenities and that future occupiers of the building have acceptable levels of privacy.
- 1.5 Adequate parking would be provided for the development and conditions are recommended to ensure that the new and altered accesses meet the council's standards.
- 1.6 In the event, that the application is approved a legal agreement will be secured securing air quality contributions and parking restrictions for future residents.

Hillingdon Planning Committee – 16th July 2024

PART 1 – Members, Public & Press

- 1.7 Overall, it is considered that subject to the recommended conditions the proposed development would cause no harm to the character and appearance of the area or the host building, nor would it cause significant harm to neighbouring amenities or the local highways network. Residents of the new flats would be provided with an acceptable standard of internal and external living accommodation. Additionally, the development would provide family sized housing on previously developed land, which is supported by the Local Plan and London Plan.
- 1.8 The planning application is therefore recommended for approval subject to the conditions set out in Appendix 1.

2 The Site and Locality

- 2.1 The development site is located on the corner of Uxbridge Road and Brookside Road.
- 2.2 At present the site comprises a three-storey end of terrace building. The ground floor of the building is in retail use and its upper floors are in residential use as 1 x 5-bedroom apartment. The building is finished in a mixture of brick and tiling and has high level gabled features. It is set back from Brookside Road and Uxbridge Road and forms part of small parade of three storey buildings (No's 124 -154 Uxbridge Road) which are of similar use, size and design. The development site projects beyond the side elevation of the parade to the back edge of Brookside Road and along this road frontage forms a yard area.
- 2.3 The area surrounding the development site is of mixed use. To the north are pairs of two storey semi-detached dwellings. To the south is Uxbridge Road and beyond that Hyatt Place Hotel and Springfield Road Retail Park. To the west of the site are two storey semi-detached properties and terraces and to the east (beyond the parade) are two storey terraces.
- 2.4 The site forms part of the 124 - 152 Uxbridge Road Local Parade, as designated in the Local Plan: Part 2 (2020). It also forms part of Hillingdon's Air Quality Management Area and is located within an Air Quality Focus Area. The site is within a 250-metre buffer zone of a former landfill site and a gas pipeline runs along Brookside Road.

Figure 1: Location Plan (application site edged red)

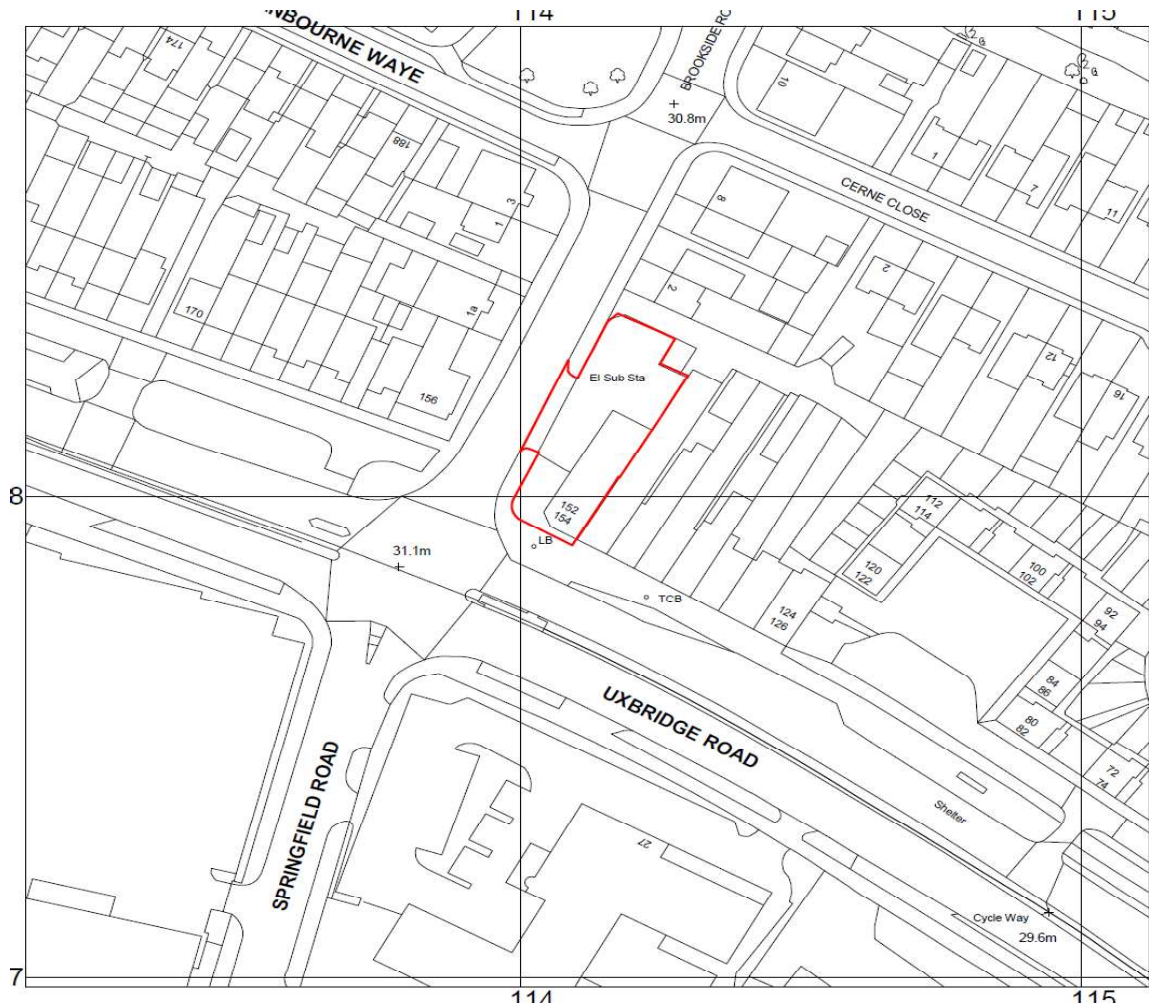


Figure 2: Street View Image of the Application Property



3 Proposal

3.1 Planning permission has been sought for the erection of a three-storey mixed use building following the demolition of existing buildings on the site. The proposed building would comprise a retail unit at ground floor with 9 residential apartments above. The proposal also includes ancillary parking, amendments to dropped kerbs and the provision of refuse and bicycle storage.

3.2 The proposed building mix is as follows:
5 x 2-bedroom units
4 x 3-bedroom units

Figure 3: Proposed Plan (please note – larger version of plan can be found in the Committee Plan Pack)

Proposed Ground Floor Plan



Proposed First Floor Plan



Proposed Second Floor Plan



Hillingdon Planning Committee – 16th July 2024

PART 1 – Members, Public & Press

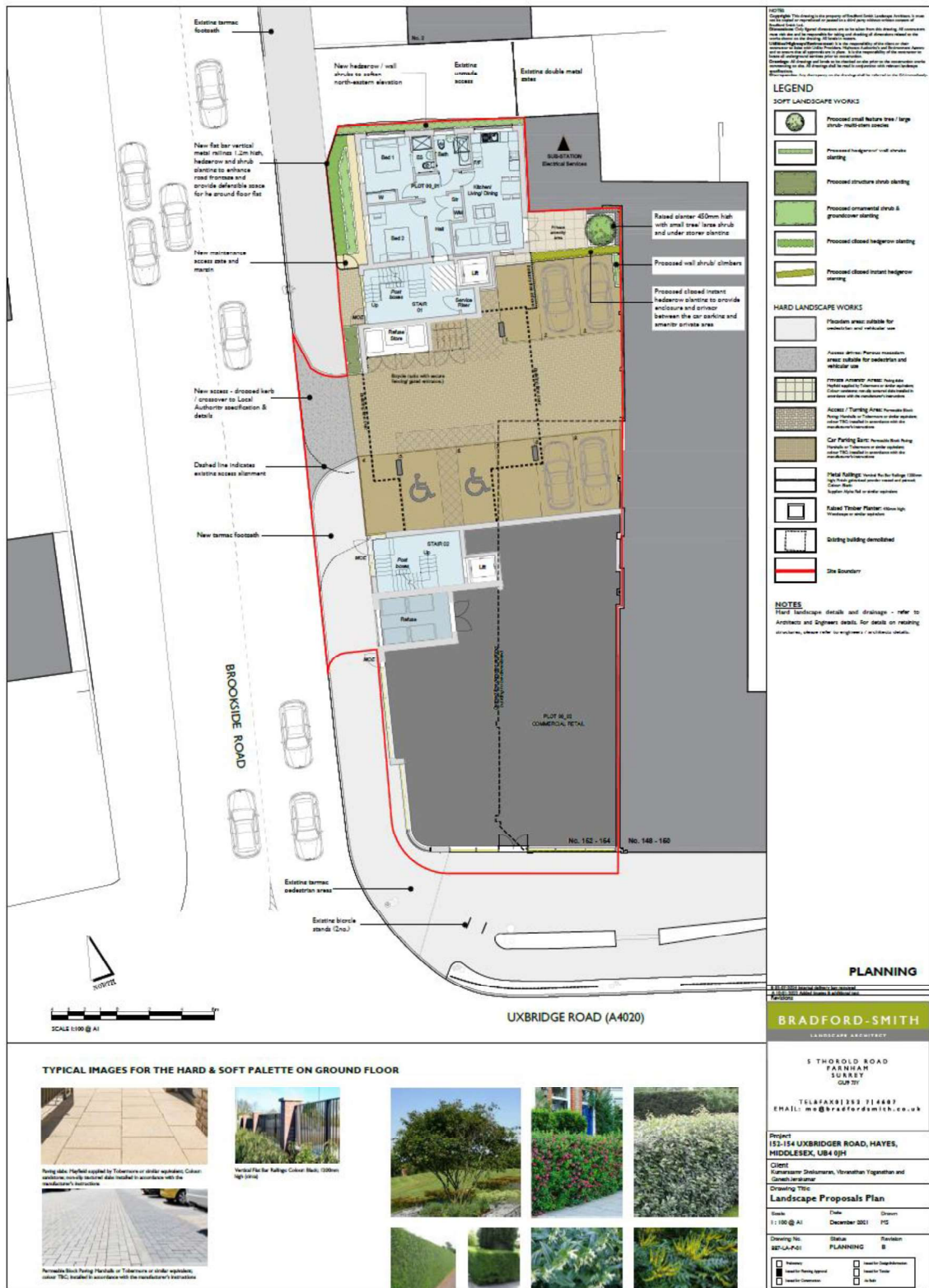
Proposed Roof Plan



Proposed Ground Floor Landscaping Plan

Hillingdon Planning Committee – 16th July 2024

PART 1 – Members, Public & Press



Proposed Front and Side Elevation



Front Elevation
1:100



Side Elevation (Left)
1:100

SCALE: 1 to 100
NOTE: THIS INCLUDES A PROPOSED GREEN SPACE, CHECK EXISTING DRAWING

Directions to be checked on site before fabrication.
Contractors must verify all dimensions on site before setting out. Commencing work, or making any other drawings.
- All settings shall be checked on site.
- All work shall be checked on site and then signed confirmed.
- This drawing must be used for construction with the relevant 100% final approval.
- This drawing must not be used for any other purposes.

- MATERIALS LEGEND
1. Facing Bricks - Red or light colored
 2. Cladding Bricks - Cladding
 3. Zinc Cladding - Zinc
 4. Aluminium window - UPVC
 5. Aluminium window - Dark Grey
 6. Concrete Handrailings post and base - Silver
 7. Aluminium window - Dark Grey
 8. Cladding Bricks - Red or light colored
 9. Metal cladding - Zinc
 10. PVC Cladding - Black
 11. PVC Cladding - White
 12. Cladding - Zinc
 13. Cladding - Zinc
 14. Cladding - Zinc
 15. Zinc Cladding - Zinc

Item	Description	Quantity	Unit	Material
1	Facing Bricks	1000	m ²	Red or light colored
2	Cladding Bricks	500	m ²	Cladding
3	Zinc Cladding	200	m ²	Zinc
4	Aluminium window	10	nos	UPVC
5	Aluminium window	10	nos	Dark Grey
6	Concrete Handrailings	10	nos	Silver
7	Aluminium window	10	nos	Dark Grey
8	Cladding Bricks	1000	m ²	Red or light colored
9	Metal cladding	200	m ²	Zinc
10	PVC Cladding	100	m ²	Black
11	PVC Cladding	100	m ²	White
12	Cladding	100	m ²	Zinc
13	Cladding	100	m ²	Zinc
14	Cladding	100	m ²	Zinc
15	Zinc Cladding	200	m ²	Zinc

Client Name
Kumarasamy Sankaranarayanan
Vignarathnam Vignarathnam
Ganesh Jayakumar
Site No
152-154 Uxbridge Road, Hayes
Drawing No
Proposed Elevations - Sheet 1

Scale
1:100 @A1
Drawing No
5005 400
Title
PLANNING SUBMISSION P8



Proposed Rear and Side Elevation



Rear Elevation
1:100



Side Elevation (Right)
1:100

SCALE: 1 to 100
NOTE: THIS INCLUDES A PROPOSED GREEN SPACE, CHECK EXISTING DRAWING

NOTE: Report all errors and omissions to the architect. All dimensions to be checked on site before fabrication.
Contractors must verify all dimensions on site before setting out. Commencing work, or making any other drawings.
- All dimensions shall be checked on site.
- All settings shall be checked on site and then signed confirmed.
- This drawing must be used for construction with the relevant 100% final approval.
- This drawing must not be used for any other purposes.

- MATERIALS LEGEND
1. Facing Bricks - Red or light colored
 2. Cladding Bricks - Cladding
 3. Zinc Cladding - Zinc
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 5. Aluminium window - Dark Grey
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 7. Aluminium window - Dark Grey
 8. Cladding Bricks - Red or light colored
 9. Metal cladding - Zinc
 10. PVC Cladding - Black
 11. PVC Cladding - White
 12. Cladding - Zinc
 13. Cladding - Zinc
 14. Cladding - Zinc
 15. Zinc Cladding - Zinc

Item	Description	Quantity	Unit	Material
1	Facing Bricks	1000	m ²	Red or light colored
2	Cladding Bricks	500	m ²	Cladding
3	Zinc Cladding	200	m ²	Zinc
4	Aluminium window	10	nos	UPVC
5	Aluminium window	10	nos	Dark Grey
6	Concrete Handrailings	10	nos	Silver
7	Aluminium window	10	nos	Dark Grey
8	Cladding Bricks	1000	m ²	Red or light colored
9	Metal cladding	200	m ²	Zinc
10	PVC Cladding	100	m ²	Black
11	PVC Cladding	100	m ²	White
12	Cladding	100	m ²	Zinc
13	Cladding	100	m ²	Zinc
14	Cladding	100	m ²	Zinc
15	Zinc Cladding	200	m ²	Zinc

Client Name
Kumarasamy Sankaranarayanan
Vignarathnam Vignarathnam
Ganesh Jayakumar
Site No
152-154 Uxbridge Road, Hayes
Drawing No
Proposed Elevations - Sheet 2

Scale
1:100 @A1
Drawing No
5005 401
Title
PLANNING SUBMISSION P8



4 Relevant Planning History

4.1 There is no relevant planning history for the development site.
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5 Planning Policy

5.1 A list of planning policies relevant to the consideration of the application can be found in Appendix 3.

6 Consultations and Representations

6.1 19 neighbouring properties were directly notified of the proposal on 09/02/22.

6.2 Representations received in response to public consultation are summarised in Table 1 (below). Consultee responses received are summarised in Table 2 (below). Full copies of the responses have also separately been made available to Members.

Table 1: Summary of Representations Received

Representations	Summary of Issues Raised	Planning Officer Response
A petition of 28 signatures from a total of 16 different households has been received against the application	1. Traffic/congestion	Discussed in section 7 paragraph 7.72
	2. Noise pollution	Discussed in section paragraphs 7.52 and 7.53
	3. Late night disturbances from the shop's gym and nearby hotel	Disturbances from other properties are not material to the assessment of this application
8 Individual letters of objection have been received.	I. Traffic	Discussed in section 7 paragraph 7.72
	II. Noise	Discussed in section paragraphs 7.52 and 7.53
	III. Out of character	Discussed in section 7 paragraphs 7.17 to 7.26

Table 2: Summary of Consultee Responses

Consultee and Summary of Comments	Planning Officer Response
Statutory Consultation	
Thames Water: No objection subject to a standard informative to ensure that the developer applies for a permit should any ground water or surface water be proposed to be discharged into the public sewer network and a	Noted

condition pertaining to the submission of a piling method statement.	
Cadent Gas: No objection	Noted
Scottish Southern Electric: No comments received	
Internal Consultation	
<p>Highway Officer:</p> <p>The Highway Authority is aware that this planning application has generated several objections, with regards highway matters this concerns traffic congestion and vehicular noise. The Highway Authority anticipates that the amount of road traffic generated by the proposal would not be insignificant and not have any noticeable impact upon the free flow of traffic or road noise.</p> <p>A total of 9 parking spaces, including 2 disabled accessible spaces are proposed which is below the maximum standards set out within the London Plan. The parking proposed is therefore acceptable.</p> <p>The proposal would include the carrying out of alterations to a vehicular access. On page 12 of the Design and Access Statement plan Ref: 5005_DAS V1.0 (Design + Access Statement) shows that the vehicle crossover would extend across the entrance to the residents parking.</p> <p>The initial proposal included a new servicing and delivery bay within the site boundary which would have required the construction of an additional access. This has now been removed and deliveries will take place on street. The applicant will be required to apply to the Local Highway Authority for an amendment to the traffic order to permit deliveries to take place on street.</p> <p>The Highway Authority requires a planning condition that prohibits the occupiers of the new dwellings from applying to join any car parking management scheme in operation within the vicinity of the site.</p> <p>The proposal provides a minimum of 2no. long-stay cycle parking spaces per dwelling or 18no. in total as well as 2no. short-stay cycle parking spaces. The</p>	The relevant conditions have been added

<p>10no. proposed is significantly below this standard and as submitted is unacceptable. However, the Highway Authority considers that there is scope to increase cycle parking provision thereby allowing this issue to be resolved by way of a planning condition. The long-stay cycle parking spaces should be provided in accordance with the London Cycle Design Standards for the short-stay parking Sheffield stands are considered suitable.</p>	
<p>Access Officer:</p> <p>No objection subject to conditions pertaining to the requirements to conform to the relevant M4(2) and the provision of an evacuation lift in order to accord with D5, D7 and D12 of the London Plan (2021).</p>	<p>The relevant conditions have been added</p>
<p>Tree Officer:</p> <p>There are no TPO's or Conservation Area designations affecting the site. No trees or landscape features of merit will be affected by the proposal. The proposals include a hard and soft landscape strategy and layout plans by Bradford-Smith Landscape Consultants. The scheme introduces planting at ground level and on green roofs - including an intensive green roof providing amenity space for residents and an extensive roof with wildflower meadow. No objection subject to a condition to secure more comprehensive landscaping details.</p>	<p>The relevant conditions have been added</p>
<p>Noise Officer:</p> <p>The noise assessment report ref Rep: 112421 / 152 - 154 Uxbridge Road, Hayes by Sonic Element dated 24 Nov 2021 has been reviewed. This characterises the existing noise environment that would likely affect the proposed development, in particular the residential use. Existing noise levels during the night/day periods are reported as around 60/63 dB(A) respectively and have been adjusted upwards using the BS4142 approach. An attempt was made to consider the noise from the existing electricity sub-station, but this was not apparent, indicating that its enclosure has been effective in mitigating any noise it produces. This is a relatively noisy area affected by traffic on the main roads, it is adjacent to a busy road junction, and local commercial activity. The report recommends a corresponding noise insulation and ventilation design to mitigate the effects of noise within the residential units to levels consistent with</p>	

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<p>relevant guidance following recognised methods. No objection subject to a condition to restrict noise levels to a specific level above background noise levels.</p>	<p>The relevant conditions have been added</p>
<p>Air Quality Officer:</p> <p>The proposed development is located within the LBH Air Quality Management area (AQMA), and within the Ossie Garvin LBH Focus Area (FA), bringing additional traffic emissions which will add to current poor air quality. Developments in these areas need to be neutral as minimum and positive in Focus Areas, contributing to the reduction of air pollutant emissions in these sensitive locations.</p> <p>The application is supported by an Air Quality Assessment which has been reviewed. In this case the level of mitigation does not result in an Air Quality positive development, as such a damage contribution of £28,088 will need to be secured by way of a s106 legal agreement. Subject to the agreement of the damage cost and conditions to secure an exposure reduction strategy, implementation of a mechanical ventilation system and submission of a low emissions plan, no objection is raised.</p>	<p>The relevant conditions have been added and heads of term proposed for approval by committee.</p>
<p>Urban Design Officer:</p> <p>The design is now accepted; however, I am against the demolition of the existing building. Demolition runs contrary to sustainability principles.</p>	<p>The existing building is not considered to be of any architectural merit and whilst the demolition of the existing building may be regrettable it is not a listed heritage asset.</p>

7 Planning Assessment

7.1 Principle of Development

7.2 *Retail Impact*

7.3 Policy SD7 and E9 of the London Plan and Policy DMTC 3 of the Hillingdon Local Plan Part 2 – Development Management Policies (2020) recognise the contribution that retail properties make to local communities and infrastructure. Furthermore, that seek to safeguard retail floor space within designated areas such as town centres and local shopping parades. The policies require new retail space to be designed to be flexible to enhance long term occupation.

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- 7.4 The application site is not located within a local town centre, or a local shopping parade designated within the local plan. However, the site does form part of a non-designated parade which fronts the Uxbridge Road and comprises of ground floor commercial uses with residential above. As such the retention of the commercial space is not only supported in principle but it's enhancement is considered to be a benefit which weighs in favour of the proposal.
- 7.5 At present the site comprises approximately 180m² of retail space at ground floor. The application proposes 226m² of retail space at ground floor level therefore resulting in a net increase of 46m². The existing frontage comprises of an unattractive shop front which extends around the corner junction between Uxbridge Road and Brookside Road. The existing retail unit occupies the entire frontage with what appears to be storage to the rear in the form of a white painted brick building which would have been active in terms of views across the service yard at some point, but the windows have been boarded up adding to the poorly maintained and decayed elevations which front Brookside Road.
- 7.6 In comparison, the proposal would result in an enlarged retail space with a consistent fascia for signage, increased glazing thus making a more attractive and active shop frontage. As referred to above the development would also replace unsightly advertisements and an open service yard which is used for storage. Above the enlarged retail space would be new residential units bringing new customers to the local shopping parade. It is therefore considered that the proposal would enhance and support the viability and attractiveness of the local parade. Policy SD7 and E9 of the London Plan (2021) support the redevelopment of out of centre retail space for mixed uses and the proposal would achieve this.
- 7.7 *Housing Provision*
- 7.8 The site comprises a building in mixed use (retail at ground floor with 1x 5 bed maisonette above). The site would remain in mixed used post development but would be of higher density, comprising a larger retail unit and 9 dwellings (resulting in a net increase of 8 dwellings at the site). Mixed used development is supported in this location as mentioned in the retail impact assessment above. Furthermore, new residential development is supported, subject to compliance with other material planning considerations.
- 7.9 *Housing Mix*
- 7.10 The proposed residential mix is as follows:
- 1 x 2 bed, 4 person apartment at ground floor
- 2 x 2 bed, 4 person apartments and 2 x 3 bed, 5 person apartments at first floor
- 2 x 2 bed, 4 person and 2 x 3 bed, 5 person apartments at second floor
- 7.11 The Councils Strategic Housing Marketing Assessment states there to be an underlying need for family sized units which are 3-bed units and above in accordance with the London Plan 2021 glossary definition.

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Four of the nine proposed dwellings would be family sized (3 bedrooms or more), therefore, not only would the proposal conform with DMH 1 and DMH 2 in terms of providing a broad mix of unit sizes, but it would also contribute towards addressing the identified need for family sized units within Hillingdon. As such the proposed housing mix is therefore considered to be acceptable.

7.12 *Density of Development*

7.13 Numerical densities are more appropriate to larger sites and what is considered of greater significance to the determination of this application is the local contextual factors. The key consideration is therefore whether the development would acceptably integrate with the character and appearance of the area, and would respect residential amenity considerations, rather than the consideration of the numerical density of the proposal.

7.14 Design / Impact on the Character and Appearance of the Area

7.15 Policies D1, D3 and D4 of the London Plan (2021) require development proposals to be of high quality and to enhance the local context by delivering buildings and spaces that positively respond to local distinctiveness.

7.16 Policies BE1 of the Hillingdon Local Plan Part 1 – Strategic Policies (2012), DMHB 11, DMHB 12 and DMHD 1 of the Hillingdon Local Plan Part 2 – Development Management Policies (2020) in summary seek to secure a high quality of design that enhances and contributes to the area in terms of form, scale and materials, is appropriate to the identity and context of the townscape and would improve the quality of the public realm and respect local character. These aims are also supported by Chapter 12 of the NPPF (2023).

7.17 At present the site comprises a three-storey end of terrace building with associated yard to the rear fronting Brookside Road. The ground floor of the building is in retail use and its upper floors are in residential use as 1 x 5-bedroom apartment. The building is finished in a mixture of brick and tiling and has high level gabled features. It is set back from Brookside Road and Uxbridge Road and forms part of small parade of three storey buildings (Nos. 124 -154 Uxbridge Road) which are of similar use, size and design. The development site projects beyond the side elevation of the parade to the back edge of Brookside Road and along this road frontage forms a yard area.

7.18 The area surrounding the development site is of mixed use. To the north are pairs of two storey semi-detached dwellings. To the south is Uxbridge Road and beyond that Hyatt Place Hotel and Springfield Road Retail Park. To the west of the site are two storey semi-detached properties and terraces and to the east (beyond the parade) are two storey terraces.

7.19 The proposal seeks to demolish the existing building and to rebuild a sympathetic more contemporary replacement building. The ridge height of the replacement building would be of similar height to the existing building and to

other properties in the terrace where it would be sited. Its height is therefore considered to be acceptable.

- 7.20 The frontage of the replacement building (facing Uxbridge Road) would comprise a large gable end and dormer at roof level with a balanced first floor fenestration below, finished with shopfront at ground floor. The new building would not extend beyond the frontages of its neighbours; thus, retaining the areas building line. Front elevation dormers are common features within the terrace, as such those proposed would be in keeping with the appearance of the terrace and neighbouring buildings. Furthermore, large gables exist on the existing building and at either end of the terrace, the replacement building would have a similar feature, retaining the visual symmetry of the terrace. The larger shop frontage would be somewhat out of character with other shops in the terrace, however the existing shopfront is out of character as it wraps around the building, unlike others in the terrace. The new shopfront is therefore not considered to be significantly harmful when compared with the existing situation at the site and in fact any potential harm would be outweighed by the contribution the new building would make to removing the existing shop front which is cluttered with signage, some of which is no longer relevant to the use of the site and the poorly maintained structure. The limited visual impact of the replacement building on Uxbridge Road and the neighbouring terraces is therefore considered to be acceptable.
- 7.21 The side and rear elevation of the building (facing Brookside Road) would be of more contemporary design. The ridge height of the side and rear would be consistent with the rest of the terrace and existing building. The lift shafts would extend above the height of the terrace; however, they would be well set back to reduce their visual impact from the street. The height and scale of the building's side elevation is therefore considered to be acceptable.
- 7.22 During the processing of the application concerns were raised regarding the complex material palette of the building's side elevation. To address these concerns the zinc cladding window surrounds have been omitted and replaced with glazed bricks, which are present within the upper floor feature panels of the proposed building. This would bring continuity to the material palette of the building, whilst also breaking up the visual bulk of the buildings side elevation by having bricks of different colours. Mock Tudor beaming and render detailing have been added to the side and front gable ends of the building to ensure that it better matches the existing building and terrace.
- 7.23 A more active frontage would be introduced along Brookside due to the new ground floor residential development and shopfront, improving the buildings relationship with the road. Although the side elevation which would be prominently viewed within Brookside Road particularly when travelling towards the Uxbridge Road, the high-level windows would provide a reasonable degree of articulation. The material pallet is indicated on the submitted drawings and the external brick finish is welcomed. It is considered that this elevation is broadly acceptable and a condition to secure high quality materials is to be attached to the decision notice and this will include further improvements to this elevation.

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- 7.24 Furthermore, the existing site comprises no meaningful soft landscaping and is dominated by hardstanding. Due to its set back along Brookside Road, the proposed development allows for improved soft landscaping at the site, which would be secured via condition, improving the areas appearance.
- 7.25 Additionally, the development would result in the removal of the unsightly raised signage, including that which advertised services which are not relevant to the uses of the actual site, the cages to the front of the commercial unit and exposed service yard which are visible from Uxbridge Road and Brookside Road, further improving the appearance of the area.
- 7.26 Overall and for the reasons outlined above, the proposal is considered to have an acceptable impact on the visual amenities of the area and would therefore comply with Policies D1, D3 and D4 of the London Plan (2021), Local Plan Part 1: Strategic Policies (2012) Policy BE1, Policies DMHB 11 and DMHB 12 of Hillingdon Local Plan: Part 2 - Development Management Policies (2020) and Paragraph 135 of the NPPF (2023).
- 7.27 Residential Amenity
- 7.28 Policy DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) seeks to ensure a satisfactory relationship with adjacent dwellings with no unacceptable loss of outlook, amenity, daylight and sunlight to neighbouring occupiers.
- 7.29 Policy D14 of the London Plan (2021) requires that proposals minimise noise pollution and Policy EM8 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012) promotes the maximum possible reduction in noise levels and seeks to ensure that noise impacts can be adequately controlled and mitigated.
- 7.30 Paragraph 135 of the NPPF (2023) states 'Planning policies and decisions should ensure that developments create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users; and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.'
- 7.31 *Privacy*
- 7.32 Due to their separation distance from the properties on the other side of Uxbridge Road, the south facing windows of the proposed development would cause no loss of privacy to neighbouring properties.
- 7.33 The site is bounded by 156 Uxbridge Road, which is in use as a business referred to as DHOOT HGV LGV Training London. Although it has a residential appearance, it is unclear whether the property is in use as a form of floorspace to support a commercial operation or a residential property. Notwithstanding this point, the first and second floor windows proposed would provide views into the side facing windows of this property at a distance of approximately 19.5m. As set

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out above, Policy DMHB 11 and the pre-text to the policy set a guideline of 21 metres as an adequate separation distance between habitable neighbouring windows. The proposal would fall 1.5 metres short of this separation distance, however it is important to note that existing windows of the building look toward the neighbouring property and that the second-floor windows would look downward, toward the mentioned windows therefore increasing their distance from the proposed second floor windows. Taking this into consideration and the minor shortfall when measures as the crow flies, it is not considered that a reason for refusal on this basis would be upheld at appeal.

- 7.34 The remaining west facing windows would look toward other properties on Brookside Road at a distance well over 21m where they would cause no harmful overlooking or adverse privacy impacts.
- 7.35 The proposed development includes north facing windows which would look toward the side elevation windows of No.2 Brookside Road at approximately 5.5m. It is important to note that the principles of DMHB 11 which refer to retaining adequate levels of privacy between new and existing residential properties, applies to habitable room windows. Photos of the site, as well as evidence submitted by the applicant suggest that the 3 side windows serve a hallway, bathroom and toilet, with the latter two being obscure glazed. Nevertheless, all north facing windows proposed are secondary windows which are recommended to be conditioned to be obscure glazed, therefore preventing any overlooking. The development is therefore considered to have an acceptable impact on the privacy of No.2 Brookside Road.
- 7.36 The proposed east facing windows (serving the bedroom and living rooms of Flat 01_02 at first floor and Flat 02_02 at second floor) would face toward the rear service yard areas and would provide some views toward the rear elevations of the flats above the shopping parade. Notably, the nearest habitable room window (of the proposed development) would be 17m away from the rear habitable room windows of No.148-150 Uxbridge Road. However the angle of the windows is such that any overlooking would be very minimal. Therefore due to the separation distance and fact that the habitable room windows do not directly face each other the development would cause no loss of privacy to its occupiers. The east facing windows of the development would therefore cause no unacceptable loss of privacy to the flats above the shopping parade.
- 7.37 Furthermore, many of the first-floor windows of the shopping parade already have limited privacy, as a roof garden exists at the development site, providing existing views into these windows.
- 7.38 Conditions would be added to ensure that privacy screening is installed around upper floor walkways and the terraces of Flats 1.04 and 2.04, to protect the amenities of the flats above the shopping parade.
- 7.39 The roof level gardens would provide similar views and would therefore cause no harmful loss of privacy to neighbouring properties. Revised plans have been submitted which include a wildflower area on roof level which will be secured and

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inaccessible in order to provide a set back from the roof margins to reduce the potential impacts from overlooking from the roof garden. Therefore the overlooking of any gardens would be no different to the general overlooking of gardens which most residential properties which have a direct neighbour to the east and or west would experience.

7.40 Taking into consideration these points the development would cause no harmful loss of privacy to neighbours.

7.41 *Light and Outlook*

7.42 A Day and Sunlight Assessment has been submitted in support of the proposal (215516DAY-Rev1). The specialist report assesses the daylight and sunlight impact of the proposed development on No.2 Brookside Rd, No.156 Brookside Rd, and 148-150 Uxbridge Road, which are the nearest neighbours and therefore would be the most likely to be affected.

7.43 The report provides an assessment in accordance with the most recent BRE Guidance which was adopted in 2022. It should be noted that in terms of Vertical Sky Component (VSC) which is a key indicator in the assessment of daylight and sunlight impact, a proposed development should retain at least 80% of the existing pre-development light values.

7.44 The assessment reviews 17 neighbouring windows across the properties mentioned above and refers to them as receptors. Receptors 1-5 are windows located within the rear elevation of No.156, the report concludes that the windows tested would retain at least 80% of the existing light values therefore this property would not be unduly affected by the development.

7.45 Receptors 6-10 are located at the front of No.1 Brookside Road; these would also retain at least 80% of the existing light values therefore would not be unduly affected by the proposed development.

7.46 Turning to receptors 11-13 these are located to the side elevation of No.2 Brookside Road; these windows would be subject to a reduction in light levels below 80% (68-75%) therefore these windows would be subject to a negligible to minor impact in terms of daylight.

7.47 Finally, receptors 14-17 are located to the rear of 148-150 Uxbridge Road which adjoins the existing building on the site. These windows would retain at least 80% of the existing light values therefore would not be unduly affected by the proposed development.

7.48 Whilst the proposal would result in a minor loss of light below the guideline set out in the BRE guidance 2022, the window which is subject to the minor loss is in the side elevation of No.2 Brookside. Having considered the location of the window, it is likely to serve either as a secondary window or a stairwell therefore would not result in a detrimental loss of outlook or daylight/sunlight such as to warrant a recommendation for refusal.

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- 7.49 In terms of outlook, the upper floors of the building would be set away from the upper floor windows of No.148-50 Uxbridge Road (nearest flat above shopping parade), by approximately 5m. Due to the separation distance as well as the fact that the upper floor flats windows would still have direct views, north and east. The ground floor west facing windows of No.148-150 Uxbridge Road are obscure glazed and receive limited light. They would be unaffected by the development and still openable into the sites new parking area. The development would cause no harmful loss of outlook to its nearest neighbour.
- 7.50 The habitable windows of No.156 and No.2 Brookside Rd (other nearest neighbours) are a considerable distance from the new building and would therefore not be subjected to any adverse loss of outlook.
- 7.51 *Noise, Activity and Disturbance*
- 7.52 The development site is located within a dense, busy, mixed-use area next to a busy road where activity and noise levels within the area are high. The addition of 9 additional dwellings and a larger retail unit is therefore unlikely to cause any significant increase in activity or noise at the site which would be harmful to neighbouring amenities. The opening hours of the commercial unit would be secured via condition. In terms of noise mitigation, a condition relating to materials will be imposed however the detailed design stage which will be submitted for building regulations review will include details to ensure the units appropriately noise insulated. Additionally, a construction management plan will be secured via condition to minimise construction disturbance.
- 7.53 Overall, when considering the proposal, it is clear the scheme has been designed to fit the constraints of the site. The proposal would not lead to a significant loss of outlook, daylight, or sunlight to neighbouring habitable rooms. Furthermore, the development would not involve a significant increase in noise disturbance in an area which is bounded by a busy road and a large retail/commercial park on the opposite side of the road that generate significant levels of background noise. Taking into consideration the above and subject to conditions the development would have an acceptable impact on neighbouring amenities.
- 7.54 Quality of Residential Accommodation (Internal and External)
- 7.55 *Internal*
- 7.56 Policy D6 of the London Plan (2021) requires that all housing should be of high-quality design and provide adequately sized rooms with comfortable and functional layouts which are fit for purpose and meet the needs of all Londoners without differentiating between tenures.
- 7.57 Policy DMHB 16 of the Hillingdon Local Plan: Part Two – Development Management Policies (2020) requires that all housing developments should have an adequate provision of internal space in order to provide an appropriate living environment.

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- 7.58 The proposed development comprises the following internal accommodation:
 Ground Level:
 Plot 00_01 (2b4p) 70m²
 First Level:
 Plot 01_01 (2b4p) 70m²
 Plot 01_02 (3b5p) 87m²
 Plot 01_03 (2b4p) 70m²
 Plot 01_04 (3b5p) 91m²
 Second Level:
 Plot 02_01 (2b4p) 70m²
 Plot 02_02 (3b5p) 87m²
 Plot 02_03 (2b4p) 70m²
 Plot 02_04 (3b5p) 86m²
- 7.59 The dwellings would therefore comply with or exceed required internal floor space standards.
- 7.60 The ground floor flat would be set back from the street, creating a privacy buffer for its future residents. Its northern windows would be obscure glazed, again ensuring privacy. The habitable rooms within the ground floor flat that will have obscure glazed windows, will have other windows providing light and outlook for future residents. The development would provide future residents of the ground floor flat with acceptable living conditions. The other new dwellings would also have habitable rooms with good access to light and privacy.
- 7.61 In reviewing the internal spaces, it is noted that there are two bedrooms out of the 20 to be provided across the entire development which would feature partially obstructed primary windows. The windows of bedrooms 2 of Flats 02_03 and 01_03 would be partially obscured by a privacy screen which is located 2m from the window and would obscure views of the bedrooms from the external walkway which runs along the outer wall of the unit. The principle of DMHB 11 which refers to separation distances and DMHB 16 is to ensure that adequate levels of privacy are reflected in the design. When assessing privacy officers consider the overlooking which could potentially occur between habitable room windows. This is due to the fact that these areas would be subject to longer timeframes of occupation than an external non habitable area such as an external walkway which provides access to a unit. Taking this into consideration it would not be necessary to place a privacy screen to obscure views from the walkway into the bedrooms. However, given that they are proposed and would only partially obscure the views out of the windows and users of the flats would not be confined to their rooms, but would have the entirety of the flat, where access to light and outlook would be gained from numerous windows, the level of overall outlook to these bedrooms is acceptable.
- 7.62 Overall, it is therefore considered that the new dwellings would provide future residents with an acceptable level of internal living accommodation.

7.63 *External Amenity*

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- 7.64 Policy DMHB 18 of the Hillingdon Local Plan Part 2: Development Management Policies (2020) requires all new residential developments to provide good quality and usable private outdoor amenity space. 25m² of amenity space is required for 2bed flats and 30m² for 3 bed flats.
- 7.65 The proposed amenity space provisions are as follows:
 Ground Level:
 Plot 00_01 (2b4p) 16.5m²
 First Level:
 Plot 01_01 (2b4p) 8.4m²
 Plot 01_02 (3b5p) 16m²
 Plot 01_03 (2b4p) 6.5m²
 Plot 01_04 (3b5p) 9.4m²
 Second Level:
 Plot 02_01 (2b4p) 8.4m²
 Plot 02_02 (3b5p) 16m²
 Plot 02_03 (2b4p) 5.5m²
 Plot 02_04 (3b5p) 3.4m²
 Third Level:
 Rooftop Communal Amenity Gardens - 155m²
- 7.66 Taking into consideration the proposed unit mix, 245sqm of amenity space is required in total. The proposal would provide approximately 245sqm which complies with Policy DMHB 18. Whilst the bulk of the amenity space is provided at roof level, the units would also be provided with modest private individual amenity areas. The proposed amenity space provisions are therefore considered to be acceptable and to provide residents with a good standard of external living accommodation.
- 7.67 Highways and Parking
- 7.68 Policies T4, T6, T6.2 of the London Plan (2021), DMT 1 and DMT 2 of the Hillingdon Local Plan: Part 2 (2020) are all directly relevant to the proposed development. These policies can be read in full in the Committee Report Part 3 - Policy Appendix, and in summary, seek to deliver development which is sustainable in transport terms and safeguards highway and pedestrian safety.
- 7.69 These aims are also supported by the NPPF (2023) at Chapter 9, which states that development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.
- 7.70 *Car Parking*
- 7.71 The development site is located within PTAL 3, on the border of PTAL 2.
- 7.72 The London Plan (2021) advises that for residential development in this location the maximum amount of car parking that should be provided is 0.75 spaces per

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two-bed unit and 1no. space per three-bed unit. A maximum of 9 parking spaces are therefore required for the proposed development. 9 parking spaces would be provided, as such the proposed parking provisions are acceptable. Each of these car parking spaces would be allocated to a specific unit which is supported. The objections which have been received during the public consultation have raised concerns regarding congestion, however the Highway Authority anticipates that the amount of road traffic generated by the proposal would not be significant and would therefore not have any noticeable impact upon the free flow of traffic or road noise.

7.73 *Electrical Charging Points*

7.74 Part G) of Policy T6 and part C) of Policy T6.1 of the London Plan (2021) state that all residential car parking spaces must provide infrastructure for electric or Ultra-Low Emission vehicles. A condition has been recommended to secure electric vehicle charging points for the new properties, in the event of approval.

7.75 *Bicycle Parking*

7.76 Appendix C, Table 1 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) requires the provision of cycle parking facilities for new residential units.

7.78 The London Plan (2021) would require that a minimum of 2no. long-stay cycle parking spaces be provided per dwelling. 18 spaces should therefore be provided, as well as 2no. short-stay cycle parking spaces. 10 spaces are proposed, and 8 additional spaces will be secured via condition. Space exists within amenity spaces to secure these.

7.79 *Refuse/Recycling Collection*

7.80 Policy DMHB 11-part (d) of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that development proposals should make sufficient provision for well-designed internal and external storage space for general, recycling, and organic waste, with suitable access for collection. Waste storage for the residential building will be sited within the under-croft section of the car park. Refuse storage for the retail unit will be located within the service yard. Full details of the residential waste storage facilities will be secured through condition in the event of an approval.

7.81 *Access*

7.82 The existing access would be altered and repositioned to create an access for the new parking area. The works required to the public highway would be secured by a separate application made to the Local Highway Authority. In terms of servicing and deliveries for the retail unit, initially a delivery bay was proposed with a new access set to be provided to allow vehicles to pull up to the delivery bay and unload. However, this would have resulted in vehicles crossing the footway and possibly obstructing pedestrian movement. As such an amendment has been

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made to the plans to show no new access for vehicles to the delivery bay and vehicles will now stop on street. Given the limited number of deliveries required to service the retail space, this is not considered to give rise to a significant increase in congestion and therefore subject to the applicant agreeing to fund the necessary amendment to the traffic order, the proposal is acceptable.

7.83 *Construction*

7.84 A condition has been recommended for inclusion requiring a Construction Management Plan. This would ensure that the development would have no significant adverse impact on traffic and pedestrian safety during construction, given the constraints of the site.

7.85 Overall, subject to the conditions, it is concluded that the proposal would not discernibly exacerbate congestion or parking stress, and would not raise any measurable highway safety concerns, in accordance with the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), Policies DMT 1, DMT 2 and DMT 6 and Policies T4, T5 and T6 of the London Plan (2021).

7.86 Noise

7.87 Policy D14 of the London Plan (2021) requires that proposals minimise noise pollution and Policy EM8 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012) promotes the maximum possible reduction in noise levels and seeks to ensure that noise impacts can be adequately controlled and mitigated.

7.88 The provision of 9 additional residential units is not considered to lead to such a significant change in the local noise environment as to warrant a refusal of planning permission on this ground. The necessity to comply with Building Regulation standards would ensure that the development is appropriately noise insulated.

7.89 Air Quality

7.90 Local Plan Policy DMEI 14 states:
Policies SI 1 of the London Plan (2021), EM8 of the Hillingdon Local Plan: Part 1 (2012) and DMEI 14 of the Hillingdon Local Plan: Part 2 (2020) are all directly relevant to the proposal. These policies can be read in full in the Committee Report Part 3 - Policy Appendix, and in summary, seek to safeguard and improve air quality in order to protect existing and new sensitive receptors. These aims are also supported by the NPPF (2023) at chapter 15.

7.91 The development site is located within an Air Quality Management Area and Air Quality Focus Area, as such the development should not only provide evidence to demonstrate it would be air quality neutral, but it is required to demonstrate it would be air quality positive. The applicant has submitted an Air Quality Assessment which indicates the proposal would be air quality neutral but not air quality positive in terms of onsite mitigation. The Borough's Air Quality Officer has advised that if the application is recommended for approval, conditions and a legal agreement

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securing a £28,088 damage cost to offset the on-site mitigation. The conditions and damage cost are to ensure that the development causes no harm to its future residents and that adverse air quality impacts are mitigated for.

7.92 The applicant has confirmed agreement to pay the damage cost, and this will be secured via a legal agreement should planning permission be granted and the recommended conditions will also be added.

7.93 Accessibility

7.94 Policy D5 and Policy D7 of the London Plan (2021) aims to provide suitable housing for London's diverse population including disabled people, older people and families with young children.

7.96 The proposal has been reviewed by the Council's Access Officer who has raised no objection to the principle of either uses or the overall design of the development. As such subject to conditions to secure the implementation of an evacuation lift the requirement for the units to meet the M4(2) accessible standards in accordance with Policies D7, D5 and D12 of the London Plan (2021).

7.97 Security

7.98 The proposed development is not considered to compromise the security of the application site or adjoining sites.

7.99 Trees and Landscaping

7.1.1 Policy D5 of the London Plan (2021) states that development proposals should integrate green infrastructure to contribute to urban greening, including the public realm.

7.1.2 Policy DMHB 14 of the Hillingdon Local Plan: Part Two – Development Management Policies (2020) requires that all development retains or enhances existing landscaping, trees, and biodiversity.

7.1.3 The proposed development would result in no loss of trees and would therefore have no adverse tree impact.

7.1.4 A good mix of soft and hard landscaping is proposed in and around the site, as well as on the roof of the building. The landscaping would ensure that the proposal integrates with its surroundings. To ensure that an appropriate level of greenery is provided at the site, a landscaping condition is recommended for inclusion on the decision - if the application is approved.

7.1.5 With regards to Urban Greening as set out above, Policy G5 of the London Plan relates to developments of a major scale only. As this is a minor scale development the applicant is not required to provide an Urban Green Factor assessment or achieve the 0.4 score applicable to major developments.

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- 7.1.6 The proposal is therefore considered to be acceptable.
- 7.1.7 Ecology
- 7.1.8 Policy DMEI 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that if development is proposed on or near to a site considered to have features of ecological or geological value, applicants must submit appropriate surveys and assessments to demonstrate that the proposed development will not have unacceptable effects. The development must provide a positive contribution to the protection and enhancement of the site or feature of ecological value.
- 7.1.9 The site comprises buildings and hard surfacing. It does not contain any trees, ponds, open woodland, dense scrub, or shrubbery. There are no protected sites of ecological interest adjacent to or near to the site. It is therefore considered that the likelihood of protected species being present at the site is low.
- 7.1.10 The ecological value of the site would be enhanced by greenery on the roof off the building as well as around the periphery of the site. A condition requiring a landscaping plan would aid in ensuring that the proposed greenery is appropriate and supportive of ecology. With regards to Biodiversity Net Gain (BNG), this policy was formally adopted for the development on small sites in April 2024. The submission of this application pre-dates the adoption of the BNG small sites policy therefore a BNG on this site is not required to be formally demonstrated. Notwithstanding this point, the submission of a landscaping scheme which provides an increase in soft landscaping above the existing as shown on the landscaping strategy drawing, would lead to a reasonable increase in ecological value.
- 7.1.11 In the event of an approval, an informative would be secured advising that should protected species be found at the site, the applicant(s) must fulfil their duties under the Wildlife and Countryside Act 1981 (as amended) and the Conservation of Habitats and Species Regulations 2010.
- 7.1.12 The proposal is therefore considered to be acceptable.
- 7.1.13 Sustainability
- 7.1.14 Policy SI 2 of the London Plan (2021) states residential development should achieve at least a 10% improvement beyond Building Regulations 2013.
- 7.1.15 Policy DMEI 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) requires all developments to make the fullest contribution to minimising carbon dioxide emissions in accordance with the London Plan targets
- 7.1.16 The proposed development is of a minor scale therefore whilst the principle of SI 2 (carbon reduction) is applicable, the London Plan Policy applies more specifically to major scale applications. The applicant is therefore not required to

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submit an energy statement with the application or demonstrate a policy level of on-site savings.

- 7.1.17 Notwithstanding this point, the modern construction of the development would be considered as providing sufficient energy savings itself and therefore the development would comply with the principles of the carbon saving development plan policies.
- 7.1.18 The proposal would therefore be compliant with Policy SI 2 of the London Plan (2021) and Policy DMEI 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).
- 7.1.19 Flooding and Drainage
- 7.1.20 Policy DMEI 9 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that proposals that fail to make appropriate provision for flood risk mitigation, or which would increase the risk or consequences of flooding, will be refused. Policy DMEI 10 states that development within areas identified at risk from surface water flooding which fail to make adequate provision for the control and reduction of surface water runoff rates will be refused.
- 7.1.21 Policy SI12 and SI13 of the London Plan (2021) require, in summary, that flood risk is minimised and mitigated, and that surface water runoff is managed close to source.
- 7.1.22 The application site is located in Flood Zone 1 where there is a low probability of risk of fluvial flooding. As such, all forms of development including residential development (which is classified as a 'more vulnerable use') is acceptable in this location, in terms of fluvial flood risk.
- 7.1.23 In the event that the development is approved, a comprehensive drainage strategy will be secured via condition. The strategy will ensure that surface water run-off and grey water is appropriately managed in line with the drainage hierarchy set out in the London Plan.
- 7.1.24 The proposals are therefore considered to be in compliance with Policies DMEI 9 and DMEI 10 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) and Policies SI 12 and SI 13 of the London Plan (2021).
- 7.1.25 Airport Safeguarding
- 7.1.26 The development would have no impact on airport safeguarding.
- 7.1.27 Land Contamination
- 7.1.28 The is identified as being located within an area potentially at risk of land contamination according to the Council's mapping system. Having consulted the Councils Contaminated Land specialist the historical data indicates that the site is subject to a very low risk and therefore no objection has been raised subject to a

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condition restricting the importation of soils and other materials unless they are clean and free of contamination. Subject to this condition the development is acceptable.

7.1.29 Electricity Infrastructure

7.1.30 It is noted that the development is to be constructed close to the boundary of an electricity substation located to the rear of the site. A consultation letter was issued to the electricity provider (SSE) dated 15-05-24 after undertaking a land register search to identify the owner of the asset. No comments have been received therefore it is not considered that there would be an objection to the construction of the development.

8 **Other Matters**

8.1 Human Rights

8.2 The development has been assessed against the provisions of the Human Rights Act, and in particular Article 1 of the First Protocol and Article 8 of the Act itself. This Act gives further effect to the rights included in the European Convention on Human Rights. In arriving at this recommendation, due regard has been given to the applicant's reasonable development rights and expectations which have been balanced and weighed against the wider community interests, as expressed through third party interests / the Development Plan and Central Government Guidance.

8.3 Equality

8.4 Due consideration has been given to Section 149 of the Equality Act with regard to the Public Sector Equality Duty in the assessment of this planning application. No adverse equality impacts are considered to arise from the proposal.

8.5 Local Finance Considerations and CIL

8.6 The Council adopted its own Community Infrastructure Levy (CIL) on August 1st 2014 and the Hillingdon CIL charge for residential developments is £95 per square metre of additional floorspace. This is in addition to the Mayoral CIL charge of £60 per sq metre.

8.7 The proposal involves the creation of residential units and is CIL liable

9 **Conclusion / Planning Balance**

9.1 Planning permission has been sought for the erection of a three-storey mixed use building following the demolition of existing buildings on the site. The proposed building would comprise a retail unit at ground floor with 9 residential apartments above. The proposal also includes ancillary parking, amendments to dropped kerbs and the provision of refuse and bicycle storage.

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- 9.2 The proposed new building is considered to improve the appearance of this deteriorating corner plot. The scale bulk and mass are appropriate and from a policy perspective the optimisation of a brownfield site to provide additional housing stock is supported. Furthermore, the unit mix which includes the provision of new family sized housing is considered to be a public benefit which weighs in favour of the development.
- 9.3 Overall, it is considered that the proposal would provide adequate levels of living accommodation for the occupants of the development whilst retaining the levels of living accommodation for existing properties.
- 9.4 The proposed development would not result harm to the character and appearance of the area or the local highway network.
- 9.5 Where required mitigation has been agreed and secured via a legal agreement and the suggested conditions to be attached to the decision notice. As such the proposal is considered to bring forward public benefits which would outweigh any potential harm identified, which as set out in this report would be limited.
- 9.6 It is therefore recommended that the committee grant planning permission for the proposed development subject to conditions and a legal agreement.

10 Background Papers

- 10.1 Relevant published policies and documents taken into account in respect of this application are set out in the report. Documents associated with the application (except exempt or confidential information) are available on the [Council's website here](#), by entering the planning application number at the top of this report and using the search facility. Planning applications are also available to inspect electronically at the Civic Centre, High Street, Uxbridge, UB8 1UW upon appointment, by contacting Planning Services at planning@hillingdon.gov.uk.

APPENDICES

Planning Application

4482/APP/2022/213

Appendix 1: Recommended Conditions and Informatives

Conditions

1. RES3 **Time Limit**

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

REASON

To comply with Section 91 of the Town and Country Planning Act 1990.

2. RES4 **Accordance with Approved Plans**

The development hereby permitted shall not be carried out except in complete accordance with the details shown on submitted plans numbers:

5005_200 Rev P7
5005_201 Rev P8
5005_202 Rev P8
5005_203 Rev P7
887- LA-P-01 REV B
887- LA-P-02 REV C
5005_402 Rev P2
5005_400 Rev P8
5005_401 Rev P8
5005_101 Rev P3

and shall thereafter be retained/maintained for as long as the development remains in existence.

REASON

To ensure the development complies with the provisions of the Hillingdon Local Plan Part 1 (2012), Part 2 (2020) and the London Plan (2021).

3. RES5 **General compliance with supporting documentation**

The development hereby permitted shall not be occupied until the following has been completed in accordance with the specified supporting plans and/or documents:

Air Quality Assessment - 215516AQA
241121 Noise impact assessment
215516DRA FRA and SUDs Report
215516TRA Transport Assessment
Landscape Strategy January 2022
21CLR5516NH Land contamination assessment

87 - REV B Urban Greening Statement

Thereafter the development shall be retained/maintained in accordance with these details for as long as the development remains in existence.

REASON

To ensure the development complies with the provisions of the Hillingdon Local Plan Part 1 (2012), Part 2 (2020) and the London Plan (2021).

4. OM19 Construction Management Plan

No development shall take place until a Demolition and Construction Management Plan has been submitted to and approved in writing by the Local Planning Authority. The plan shall detail:

- a) The phasing of development works
- b) Types of vehicles accessing the site, including their ability to enter the shared driveway without affecting neighbouring properties
- c) The hours during which development works will occur
- d) How vehicles will access the site whilst protecting neighbouring sites
- e) Measures to prevent mud and dirt tracking onto footways and adjoining roads (including wheel washing facilities)
- f) Traffic management and access arrangements (vehicular and pedestrian) and parking provisions for contractors during the development process (including measures to reduce the numbers of construction vehicles accessing the site during peak hours)
- g) Measures to reduce the impact of the development on local air quality and dust through minimising emissions throughout the demolition and construction process
- h) The storage of demolition/construction materials on site

The approved details shall be implemented and maintained throughout the duration of the demolition and construction process.

REASON

To ensure the development causes no harm to the local highways network or pedestrian and road user safety and to safeguard the amenity of surrounding areas in accordance with Policies DMT 2 and DMT 6 of the Hillingdon Local Plan Part 2 (2020), as well as Policies D3 and T6 of the London Plan (2021).

5. D2 Obscure Glazing

The south facing ground floor window of Flat 00_01 (facing the communal car park) and all north facing windows of Flats 00_01, 01_01 and 02_01 (facing No.2 Brookside Road) shall be glazed with obscure glass to at least scale 4 on the Pilkington scale and be non-opening except at top vent level for so long as the development remains in existence.

REASON

To prevent overlooking to adjoining properties in accordance with Policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

6. NONSC Lift Access

The development hereby approved shall accord with London Plan policy D5(B5) and D12(A) to include an evacuation lift designed to meet the technical standards set out in BS EN 81-76, BS 9991 and/or BS 9999, as shown on the approved plans.

REASON

To ensure the development provides reliable, convenient and dignified means of escape for all building uses in accordance with London Plan Policies D5 and D12.

7. NONSC Accessibility Compliance M4(2)

The dwellings hereby approved shall accord with the requirements of Policy D7 of the London Plan, and shall not be occupied until certification of compliance with the technical specifications for an M4(2) dwelling, as set out in Approved Document M to the Building Regulations (2010) 2015, has been submitted to, and approved in writing, by the Local Planning Authority. All such provisions must remain in place for the life of the building.

REASON

To not only allow the Building Control body to require the development to comply with the optional Building Regulations standards, but to also ensure the appropriate quantity and standard of accessible and adaptable housing is constructed and maintained in accordance with Policy D7 of the London Plan (2021).

8. RES6 Levels

No development shall take place until plans of the site showing the existing and proposed ground levels and the proposed finished floor levels of all proposed buildings have been submitted to and approved in writing by the Local Planning Authority. Such levels shall be shown in relation to a fixed and known datum point. Thereafter, the development shall not be carried out other than in accordance with the approved details.

REASON

To ensure that the development presents a satisfactory appearance in accordance with Policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

9. RES7 Materials (Submission)

No development above ground level shall take place until details of all materials and external surfaces (associated with the proposed development) have been submitted to and approved in writing by the Local Planning Authority. Thereafter the development shall be constructed in accordance with the approved details and be retained as such.

Details should include information relating to make, product/type, colour and

photographs/images.

REASON

To ensure that the development presents a satisfactory appearance in accordance with Policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

10. RES9 Landscaping (car parking & refuse/cycle storage)

No development above ground level shall take place until a landscape scheme has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include:

1. Details of Soft Landscaping

1.a Planting plans (at not less than a scale of 1:100)

1.b Written specification of planting and cultivation works to be undertaken

1.c Schedule of plants giving species (including pollution absorbing plants), plant sizes, and proposed numbers/densities where appropriate

2. Details of Hard Landscaping

2.a Refuse Storage (including dimensions and material finishes)

2.b Secure and enclosed cycle Storage units capable of storing 18 long stay cycles (including dimensions and material finishes), details of 2 short stay cycles spaces to be provided on site

2.c Means of enclosure/boundary treatments

2.d Hard Surfacing Materials and Car Parking Layouts (including the provision of 2 active electric vehicle charging points and 7 passive electrical vehicle charging points)

2.e External Lighting

2.f Other structures

3. Living Walls and Roofs

3.a Details of the inclusion of living walls and roofs

4. Details of Landscape Maintenance

4.a Landscape Maintenance Schedule for a minimum period of 5 years.

4.b Proposals for the replacement of any tree, shrub, or area of surfing/seeding within the landscaping scheme which dies or in the opinion of the Local Planning Authority becomes seriously damaged or diseased.

5. Schedule for Implementation

6. Other

6.a Existing and proposed functional services above and below ground

6.b Proposed finishing levels or contours

Thereafter the development shall be carried out and maintained in full accordance with the approved details.

REASON

To ensure that the proposed development will preserve and enhance the visual amenities of the locality and provide adequate facilities in compliance with Policies DMHB 11, DMHB 12, DMHB 14, DMEI 1 and DMT 2 of the Hillingdon Local Plan Part 2 (2020).

11. NONSC Thames Water Piling Condition

No piling shall take place until a Piling Method Statement (detailing the depth and type of piling to be undertaken and the methodology by which such piling will be carried out, including measures to prevent and minimise the potential for damage to subsurface sewerage infrastructure, and the programme for the works) has been submitted to and approved in writing by the local planning authority in consultation with Thames Water. Any piling must be undertaken in accordance with the terms of the approved piling method statement.

REASON

To ensure that the proposed works, in close proximity to underground sewerage utility infrastructure, will not adversely impact / cause failure of local underground sewerage utility infrastructure in accordance with Policies DMEI 11 and DMEI 12 of the Hillingdon Local Plan: Part 2 (2020).

12. NONSC Retail Operation and hours

The hereby approved retail premises shall only be used for retail purposes (Use Class E(a)) between the hours of:

0800 to 2300 Monday - Friday
0800 to 2300 Sundays
1000 to 2200 on Public or Bank Holidays

REASON

To safeguard the residential amenity of the occupiers of adjoining and nearby properties in accordance with Policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

13. NONSC Privacy Screens and Balconies

Notwithstanding the approved drawings, prior to the occupation of the hereby approved dwellings, details of the siting, design and materials of all privacy screens to balconies and external walkways shall be submitted to and approved in writing by the Local Planning Authority. Furthermore design and material finish details of privacy screens to be installed on all external balconies situated on the north facing elevation of the building shall be submitted to and approved in writing by the Local Planning Authority.

Thereafter, the screens shall be installed as per the consented details prior to the first occupation of the development here by approved and the privacy screens shall be retained

on site for so long as the development remains in existence.

REASON

To prevent overlooking to adjoining properties and to protect the privacy of occupiers of the flats in accordance with Policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

14. NONSC Noise

For the lifetime of the development hereby permitted the noise level shall not exceed 35 dB LAeq 16 hrs 0700-2300 and 30 dB LAeq 8 hrs 2300-0700 measured inside any habitable room of any permitted dwelling having regard to the guidance set out in British Standards 8233: 2014 and 4142: 2014 whilst achieving acceptable internal living conditions with respect to ventilation and temperature.'

REASON

To protect adjoining properties from adverse noise impacts in accordance with Policy D14 of the London Plan (2021) and paragraph 135 of the NPPF (2023).

15. NONSC Soil Testing

Before any part of the development is occupied, site derived soils and imported soils shall be tested for chemical contamination, and the results of this testing shall be submitted and approved in writing by the Local Planning Authority. All soils used for gardens and/or landscaping purposes shall be clean and free of contamination. Before any part of the development is occupied, site derived soils and imported soils shall be tested for chemical contamination, and the results of this testing shall be submitted and approved in writing by the Local Planning Authority. All soils used for gardens and/or landscaping purposes shall be clean and free of contamination.

REASON

To ensure that the occupants of the development are not subject to any risks from soil contamination in accordance with Policies DMEI 11 and DMEI 12 of The Local Plan: Part 2 - Development Management Policies (2020).

16. NONSC Mechanical Ventilation

No development shall commence until proof that the mechanical ventilation approved by the LA (in condition above) has been installed at the premises with the systems / filters required to extract NOx/NO2 from outdoor ambient air and secure indoor NO2 levels below 36ug/m3 has been submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be carried out before the use/operation commences and be thereafter maintained.

REASON

As the application site is within an Air Quality Management Area, and to reduce the impact on air quality in accordance with policy EM8 of the Local Plan: Part 1 (November 2012),

Policy DMEI 14 of the London Borough of Hillingdon Local Plan (part 2), the London Borough of Hillingdon Air Quality Action Plan 2019-2023, London Plan (2021) Policy S11 and T4, and the National Planning Policy Framework (2023).

17. NONSC Dust Control

A. No development shall commence until a Plan has been submitted to, and approved in writing by, the LPA. This must demonstrate compliance (drawn up accordance with) the GLA Control of Dust and Emissions from Construction and Demolition SPG (or any successor document).

B. All Non-Road Mobile Machinery (NRMM) of net power of 37kW and up to and including 560kW used during the course of the demolition, site preparation and construction phases shall comply with the emission standards set out in the GLA's supplementary planning guidance "Control of Dust and Emissions During Construction and Demolition" dated July 2014 (SPG), or subsequent guidance. Unless it complies with the standards set out in the SPG, no NRMM shall be on site, at any time, whether in use or not, without the prior written consent of the local planning authority. The developer shall keep an up-to-date list of all NRMM used during the demolition, site preparation and construction phases of the development on the online register at <https://nrmm.london/>."

REASON

To reduce the developments impact on air quality in accordance with the London's Low Emission Zone for non-road mobile machinery as per requirements of the London Environment Strategy, Policy EM8 of the Local Plan: Part 1 (November 2012), Policy DMEI 14 of the London Borough of Hillingdon Local Plan (part 2), the London Borough of Hillingdon Air Quality Action Plan 2019-2023, and London Plan (2021) Policies S11 and T4.

18. NONSC Accesses, Footways and Crossovers

Notwithstanding any of the hereby approved plans and prior to the commencement of the hereby approved development, a plan should be submitted which demonstrates that the new vehicle accesses (as well as crossovers) would comply with the London Borough of Hillingdon Domestic Vehicle Crossover Policy 2022. Furthermore details of how all redundant crossovers will be reinstated as footpaths shall be submitted.

Thereafter the accesses and footpaths shall be constructed in accordance with the approved plans and maintained throughout the life time of the development.

REASON

To ensure that the local highways network is protected from adverse impacts in compliance with Policies DMT 2 and DMT 6 of the Hillingdon Local Plan Part 2 (2020) and Policy T6 of the London Plan (2021).

19. NONSC SUDS and Drainage

No development approved by this permission shall be commenced until a scheme for the provision of sustainable water management and water efficiency has been submitted to and approved in writing by the Local Planning Authority. The scheme shall:

- i. Provide information about the design storm period and intensity, the method employed to delay and control the surface water discharged from the site and the measures taken to prevent pollution of the receiving groundwater and/or surface waters;
- ii. Include a timetable for its implementation; and
- iii. Provide a management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption by any public authority or statutory undertaker and any other arrangements to secure the operation of the scheme throughout its lifetime.

The scheme shall also demonstrate the use of methods to minimise the use of potable water through water collection, reuse and recycling and will:

- iv. Provide details of water collection facilities to capture excess rainwater;
- v. Provide details of how rain and grey water will be recycled and reused in the development;
- vi. Provide details of how the dwellings will achieve a water efficiency standard of no more than 110 litres per person per day maximum water consumption.

Thereafter the development shall be implemented and retained/maintained in accordance with these details for as long as the development remains in existence.

REASON

To ensure the development does not increase the risk of flooding in accordance with Policy DME1 10 of the Hillingdon Local Plan Part 2 (2020) and Policies SI2 and SI 13 of the London Plan (2021).

20. NONSC Fire Strategy

Prior to above ground level works, an updated Fire Strategy shall be submitted to and approved in writing by the Local Planning Authority. The updated Fire Strategy shall include details of the materials to be used to construct the development. The development shall thereafter be completed in accordance with the approved updated Fire Strategy.

REASON

To ensure that the development meets Fire Safety Standards in accordance with Policy D12 of the London Plan (2021).

Informatives

1. I52 Compulsory Informative (1)

The decision to GRANT planning permission has been taken having regard to all relevant

planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

2. I73 Community Infrastructure Levy (CIL) (Granting Consent)

Under the terms of the Planning Act 2008 (as amended) and Community Infrastructure Levy Regulations 2010 (as amended), this development is liable to pay the London Borough of Hillingdon Community Infrastructure Levy (CIL) and the Mayor of London's Community Infrastructure Levy (CIL). This will be calculated in accordance with the London Borough of Hillingdon CIL Charging Schedule 2014 and the Mayor of London's CIL Charging Schedule 2012. Before commencement of works the development parties must notify the London Borough of Hillingdon of the commencement date for the construction works (by submitting a Commencement Notice) and assume liability to pay CIL (by submitting an Assumption of Liability Notice) to the Council at planning@hillingdon.gov.uk. The Council will then issue a Demand Notice setting out the date and the amount of CIL that is payable. Failure to submit a valid Assumption of Liability Notice and Commencement Notice prior to commencement of the development may result in surcharges being imposed.

The above forms can be found on the planning portal at:
www.planningportal.gov.uk/planning/applications/howtoapply/whattosubmit/cil

Pre-Commencement Conditions: These conditions are important from a CIL liability perspective as a scheme will not become CIL liable until all of the pre-commencement conditions have been discharged/complied with.

3. I15 Control of Environmental Nuisance from Construction Work

Nuisance from demolition and construction works is subject to control under The Control of Pollution Act 1974, the Clean Air Acts and other related legislation. In particular, you should ensure that the following are complied with:-

A. Demolition and construction works which are audible at the site boundary shall only be carried out between the hours of 08.00 and 18.00 hours Monday to Friday and between the hours of 08.00 hours and 13.00 hours on Saturday. No works shall be carried out on Sundays, Bank or Public Holidays.

B. All noise generated during such works shall be controlled in compliance with British Standard Code of Practice BS 5228:2009.

C. Dust emissions shall be controlled in compliance with the Mayor of London's Best Practice Guidance 'The Control of dust and emissions from construction and demolition.

D. No bonfires that create dark smoke or nuisance to local residents.

You are advised to consult the Council's Environmental Protection Unit (www.hillingdon.gov.uk/noise Tel. 01895 250155) or to seek prior approval under Section 61 of the Control of Pollution Act if you anticipate any difficulty in carrying out construction other than within the normal working hours set out in (A) above, and by means that would minimise disturbance to adjoining premises.

4. I23 Works affecting the Public Highway - Vehicle Crossover

The development requires the installation of a vehicular crossover, which will be constructed by the Council. This work is also subject to the issuing of a separate licence to obstruct or open up the public highway. For further information and advice contact: - Highways Maintenance Operations, 4W/07, Civic Centre, Uxbridge, UB8 1UW.

5. I52 Asbestos informative

Appropriate testing, demolition and/or removal of any asbestos containing material (ACM) should be carried out strictly in accordance with guidance from the Health and Safety Executive.

6.

A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. We would expect the developer to demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer. Permit enquiries should be directed to Thames Water's Risk Management Team and application forms should be completed on line.

I53 Compulsory Informative (2)

The decision to GRANT planning permission has been taken having regard to the policies and proposals in the Hillingdon Local Plan Part 1 (2012) and Part 2 (2020) set out below, including Supplementary Planning Guidance, and to all relevant material considerations, including The London Plan (2021) and national guidance.

DMCI 7	Planning Obligations and Community Infrastructure Levy
DMEI 10	Water Management, Efficiency and Quality
DMEI 2	Reducing Carbon Emissions
DMEI 9	Management of Flood Risk
DMH 2	Housing Mix
DMHB 11	Design of New Development
DMHB 12	Streets and Public Realm

DMHB 14	Trees and Landscaping
DMHB 15	Planning for Safer Places
DMHB 16	Housing Standards
DMHB 17	Residential Density
DMHB 18	Private Outdoor Amenity Space
DMT 1	Managing Transport Impacts
DMT 2	Highways Impacts
DMT 5	Pedestrians and Cyclists
DMT 6	Vehicle Parking
DMTC 3	Maintaining the Viability of Local Centres and Local Parades
DMTC 4	Amenity and Town Centre Uses
LPP D1	(2021) London's form, character and capacity for growth
LPP D3	(2021) Optimising site capacity through the design-led approach
LPP D4	(2021) Delivering good design
LPP D5	(2021) Inclusive design
LPP D6	(2021) Housing quality and standards
LPP D7	(2021) Accessible housing
LPP E9	(2021) Retail, markets and hot food takeaways
LPP H1	(2021) Increasing housing supply
LPP H2	(2021) Small sites
LPP SD7	(2021) Town centres: development principles and Development Plan Documents
LPP SI12	(2021) Flood risk management
LPP SI13	(2021) Sustainable drainage
LPP SI2	(2021) Minimising greenhouse gas emissions
LPP T5	(2021) Cycling
LPP T6	(2021) Car parking
LPP T6.1	(2021) Residential parking
NPPF11 -23	NPPF11 23 - Making effective use of land
NPPF12 -23	NPPF12 23 - Achieving well-designed and beautiful places
NPPF2 -23	NPPF2 2023 - Achieving sustainable development
NPPF4 -23	NPPF4 23 - Decision making
NPPF5 -23	NPPF5 23 - Delivering a sufficient supply of homes
NPPF6 -23	NPPF6 23 - Building a strong, competitive economy
NPPF9 -23	NPPF9 23 - Promoting sustainable transport

Appendix 2: Relevant Planning History

4482/A/82/1130 152 Uxbridge Road Hayes

New shopfront garages and internal alterations.

Decision: 02-11-1982 Approved

4482/B/82/9074 152 Uxbridge Road Hayes

Erection of two internally illuminated fascia signs and one illuminated box sign.

Decision: 02-11-1982 Approved

4482/C/92/3532 152 Uxbridge Road Hayes

Erection of an advertisement hoarding (retrospective application)

Decision: 05-05-1992 Refused **Appeal:** 24-11-1992 Dismissed

4482/PRC/2021/60 152-154 Uxbridge Road Hayes

Demolition of existing building and erection of a 3 storey building containing 9 x 2 bed apartments, 1 no retail unit, car and cycle parking, bin stores and associated development thereto

Decision: 19-07-2021 Objection

Appendix 3: List of Relevant Planning Policies

The following Local Plan Policies are considered relevant to the application:-

Part 1 Policies:

PT1.BE1 (2012) Built Environment

PT1.H1 (2012) Housing Growth

Part 2 Policies:

DMCI 7 Planning Obligations and Community Infrastructure Levy

DMEI 2 Reducing Carbon Emissions

DMEI 9 Management of Flood Risk

DMEI 10 Water Management, Efficiency and Quality

DMH 2 Housing Mix

DMHB 11 Design of New Development

DMHB 12 Streets and Public Realm

DMHB 14 Trees and Landscaping

DMHB 15 Planning for Safer Places

DMHB 16 Housing Standards

DMHB 17 Residential Density

DMHB 18 Private Outdoor Amenity Space

DMT 1 Managing Transport Impacts

DMT 2 Highways Impacts

DMT 5 Pedestrians and Cyclists

DMT 6 Vehicle Parking

LPP E9 (2021) Retail, markets and hot food takeaways

LPP SD7	(2021) Town centres: development principles and Development Plan Documents
NPPF11 -23	NPPF11 23 - Making effective use of land
NPPF12 -23	NPPF12 23 - Achieving well-designed and beautiful places
NPPF2 -23	NPPF2 2023 - Achieving sustainable development
NPPF4 -23	NPPF4 23 - Decision making
NPPF5 -23	NPPF5 23 - Delivering a sufficient supply of homes
NPPF6 -23	NPPF6 23 - Building a strong, competitive economy
NPPF9 -23	NPPF9 23 - Promoting sustainable transport
LPP D1	(2021) London's form, character and capacity for growth
LPP D3	(2021) Optimising site capacity through the design-led approach
LPP D4	(2021) Delivering good design
LPP D5	(2021) Inclusive design
LPP D6	(2021) Housing quality and standards
LPP D7	(2021) Accessible housing
LPP H1	(2021) Increasing housing supply
LPP H2	(2021) Small sites
LPP SI12	(2021) Flood risk management
LPP SI13	(2021) Sustainable drainage
LPP SI2	(2021) Minimising greenhouse gas emissions
LPP T5	(2021) Cycling
LPP T6	(2021) Car parking
LPP T6.1	(2021) Residential parking
DMTC 3	Maintaining the Viability of Local Centres and Local Parades
DMTC 4	Amenity and Town Centre Uses

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Report of the Head of Development Management and Building Control Committee Report Part 2 – Application Report

Case Officer: Rhian Thomas	17997/APP/2024/1610
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Date Application Valid:	15-07-24	Statutory / Agreed Determination Deadline:	04/10/24
Application Type:	Full	Ward:	Hayes

Applicant: **Ardesb Sarangam**

Site Address: **Yeading Infant School, Carlyon Road, Hayes**

Proposal: **Installation of solar panels on the school roofs.**

Summary of Recommendation: **GRANT planning permission subject to conditions**

Reason Reported to Committee: **Required under Part 6 of the Planning Scheme of Delegation (the Council is the Applicant)**



Summary of Recommendation:

GRANT planning permission subject to the conditions set out in Appendix 1.

1 Executive Summary

- 1.1 Planning permission is sought for the installation of 344 solar panels on the pitched roofs of the existing Locally Listed school building.
- 1.2 The proposed development complies with the relevant planning policies at Local, National and Regional level. The proposed development would not cause harm to the visual amenities of the street scene, nor would it harm the character and appearance of the Locally Listed Building. There would be no harm caused to the amenities of neighbouring properties nor the local highway network.
- 1.3 A full assessment of the application has been made in the below sections of this Committee report.
- 1.4 As such the application is recommended for approval subject to the conditions set out in Appendix 1.

2 The Site and Locality

- 2.1 The development site is located on the south side of Carlyon Road. The site comprises the Yeading Infant and Nursery School Campus. The site is made up of numerous single and two storey buildings which are both Locally Listed and finished in various materials, as well as benefiting from both soft and hard landscaping.
- 2.2 The application building is locally listed and based on the Council's GIS system, part of development site and school grounds is located within Flood Zone 2.
- 2.3 The site has a Public Transport Accessibility Level (PTAL) of 1b and approximately 50 metres north of the site is an area of open space which is designated as Green Belt land.

Figure 1: Location Plan (application site edged red)

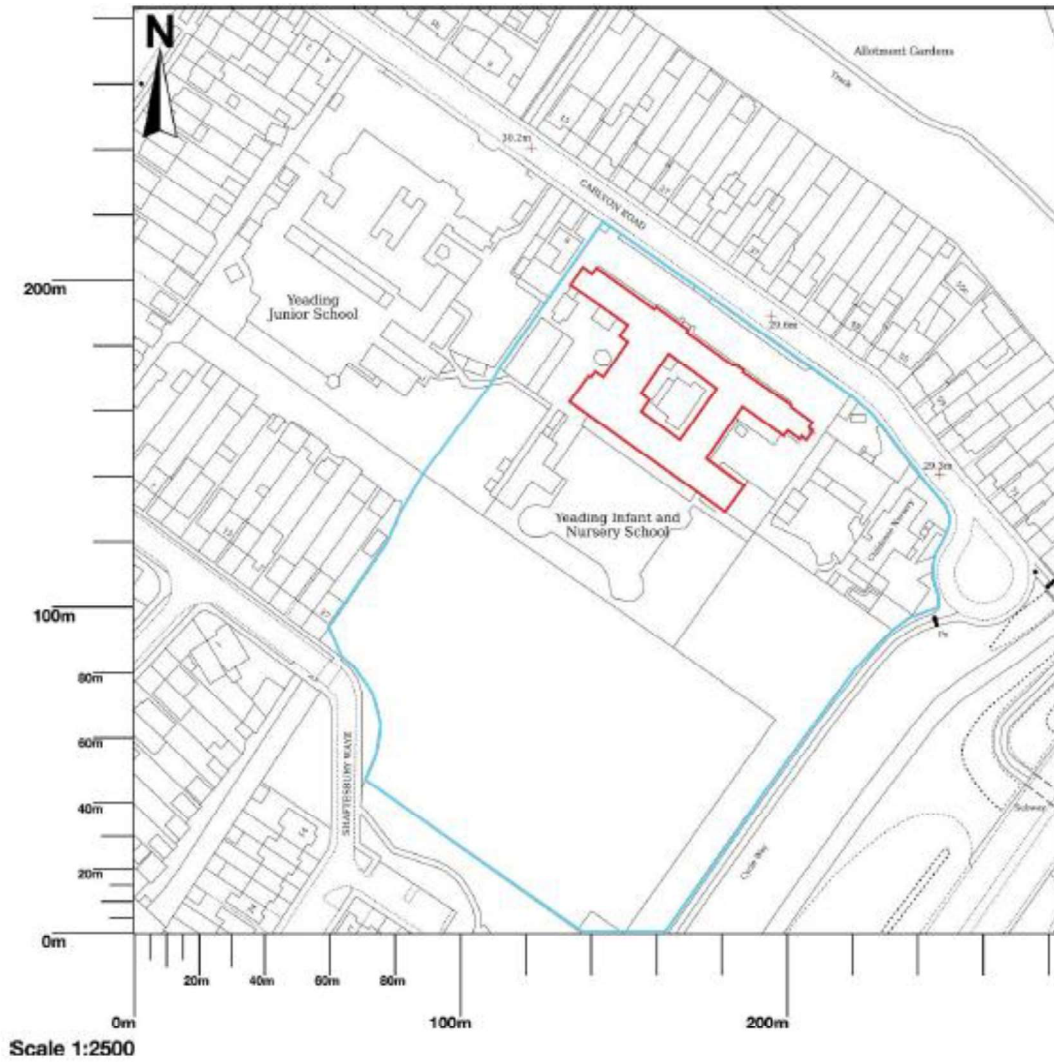


Figure 2: Street View Images of the Application Property



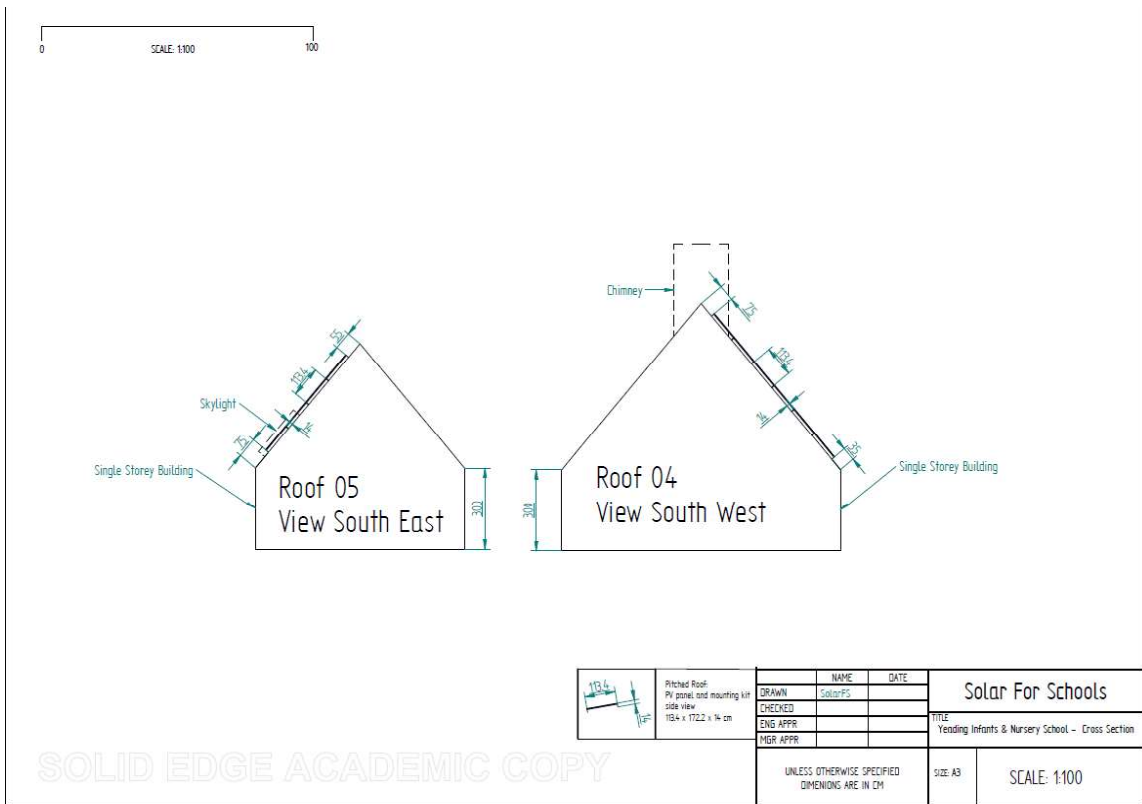
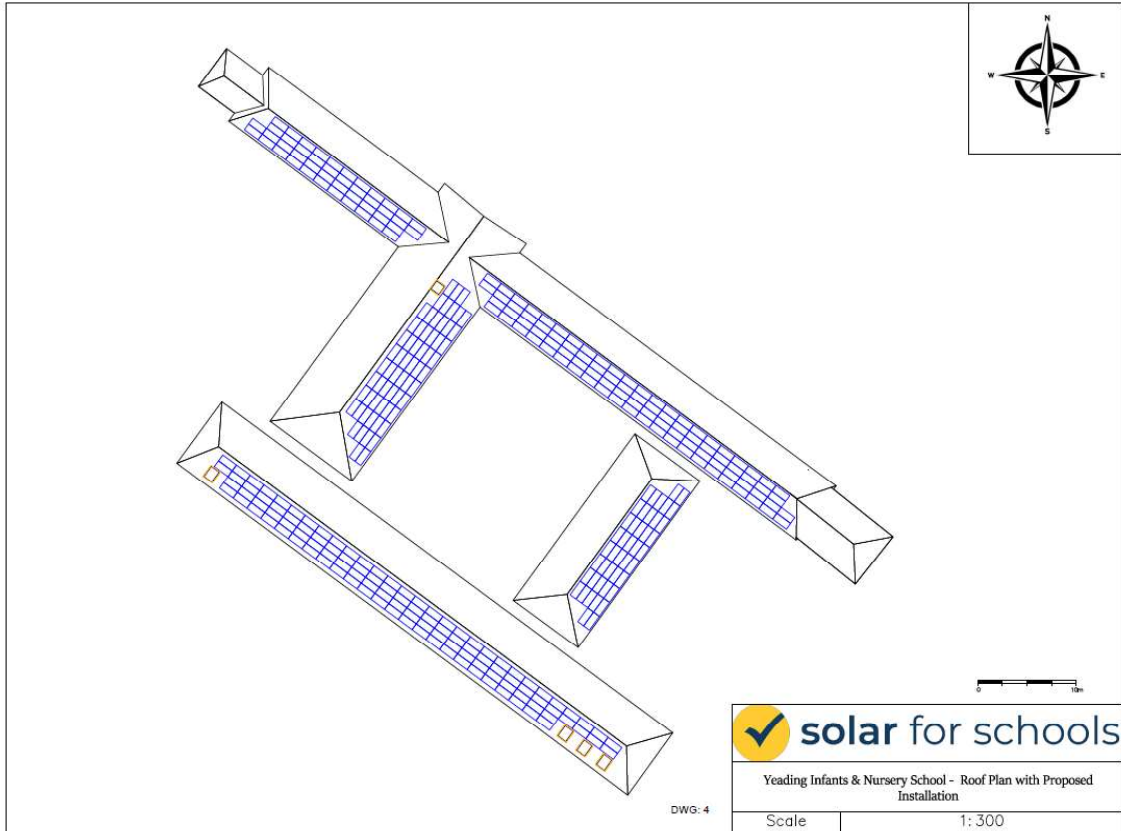
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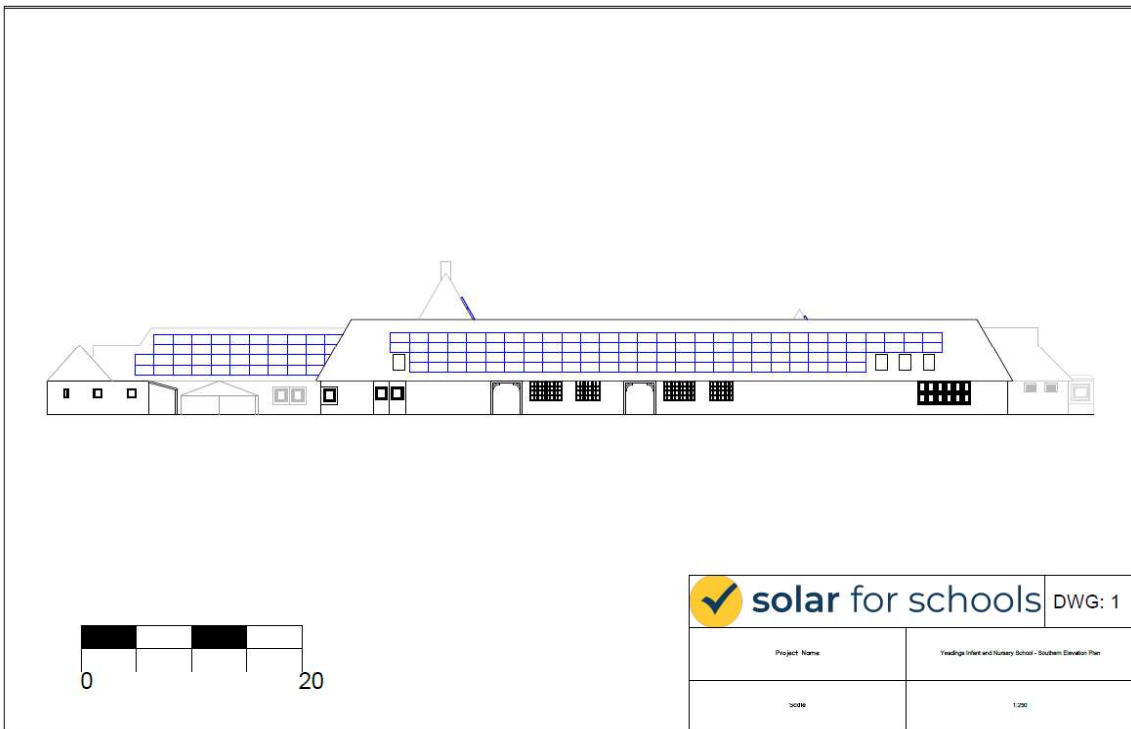
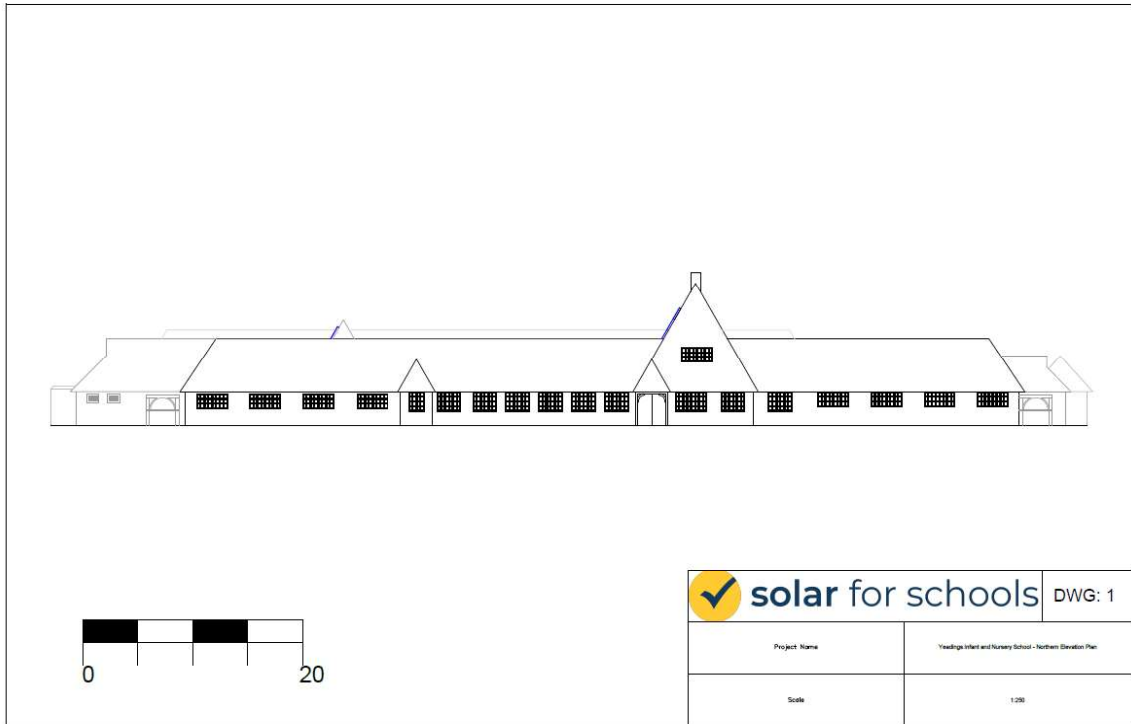
3 Proposal

- 3.1 Planning permission is sought for the installation of solar panels to the roof of the school.
- 3.2 During the process of the application, updated drawings were sought and received to include drawing numbers to ensure that a clear and concise decision notice can be issued.
- 3.3 **Figure 3: Proposed Plan** (please note – larger version of plan can be found in the Committee Plan Pack)



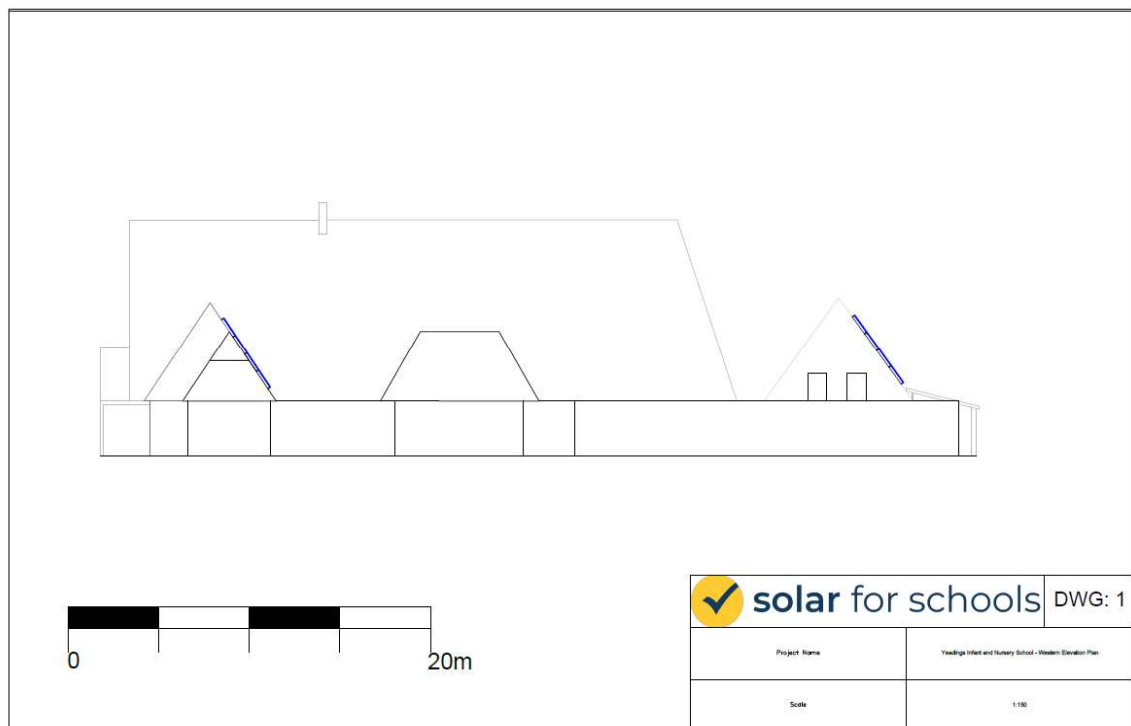
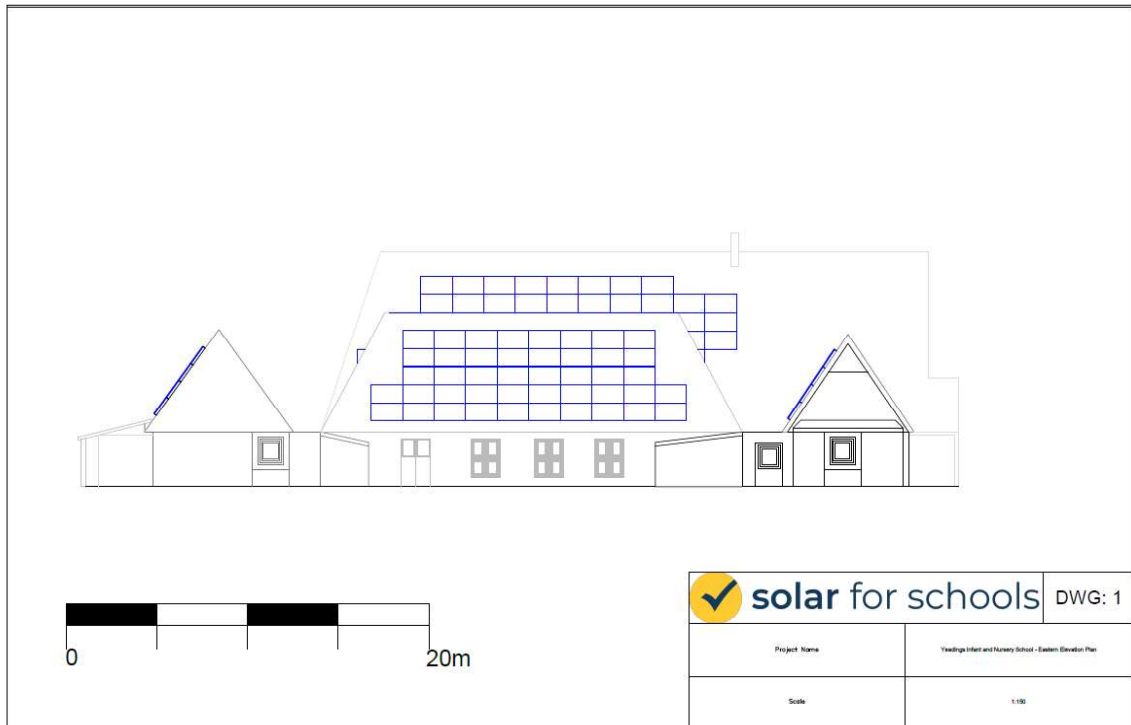
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4 Relevant Planning History

4.1 A list of the relevant planning history related to the property can be found in Appendix 2.

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5 Planning Policy

5.1 A list of planning policies relevant to the consideration of the application can be found in Appendix 3.

6 Consultations and Representations

6.1 48 neighbouring properties were consulted on the application by letter dated 05-08-24. The consultation period expired 27-08-24. No representations have been received.

6.2 Consultee responses received are summarised in Table 2 (below). Full copies of the responses have also separately been made available to Members.

Table 2: Summary of Consultee Responses

Consultee and Summary of Comments	Planning Officer Response
<p><u>Highways Officer</u></p> <p>Access to the application site would be unaffected, however, whilst the proposed development would be unlikely to impact upon the local highway network, concerns would be raised regarding additional vehicle movements generated by the construction works which would be likely to impact on access and parking for the school and local highway network, therefore a condition should be attached to any approval which requires the submission of a CLP to concur with Construction Logistics Planning (CLP) Guidance Version: v1.2 (April 2021) issued by Construction Logistics and Community Safety (CLOCS) which must include, but would not be restricted to, the following:</p> <ul style="list-style-type: none">• Phasing of works which should occur during school holidays• Site hours• Vehicular access and pedestrian access during the construction phase• Car parking for vehicles displaced during the construction phase• Contractor parking and off-street parking facilities for all vehicles linked to the site• Contractor compound including office, welfare facilities and materials storage.• Wheel wash facilities and road sweep• Dust suppression	<p>The consultation response from the Highways Officer has been noted.</p>

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<ul style="list-style-type: none"> • HGV routes to and from the site including swept path analysis for large vehicles particularly for solar panel delivery • HGV routes within the site which should include on-site turning • HGV delivery hours avoiding peak hours and school hours <p>Recommendation The Highway Authority are satisfied that the proposal would not discernibly exacerbate congestion or parking stress and would not raise any measurable highway safety concerns and would therefore offer no objection to the application but would require the following conditions to be applied to any approval:</p> <p>Conditions No works shall commence on site until a Construction Logistics Plan to concur with Construction Logistics Planning (CLP) Guidance Version: v1.2 (April 2021) has been submitted to and approved in writing by the LPA. REASON: To be in accordance with the London Plan (2021) Policy T4 Assessing and Mitigating Transport Impacts and Policy T7 Deliveries, Servicing and Construction</p> <p><u>Conservation and Urban Design Officer</u></p> <p>This is a 1930s purpose-built School single storey with clay tiled roof and larger gabled central section. It is locally listed and makes a positive contribution to the streetscape. The solar panels are located within the courtyard and to the rear facing slopes. Whilst not visually ideal and causing a low level of harm they have been located as sensitively as possible to the rear and away from the principal elevation. Given that they would provide clean energy and tackle climate change we believe that the harm is balanced with positive benefits. As such we would not object.</p>	<p>The comments from the Conservation and Urban Design Officer are noted.</p>
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7 Planning Assessment

7.1 Principle of Development

7.2 The application site accommodates a school therefore benefiting from education use. There is significant policy support within the development for the

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enhancement of schools and other educational facilities. Given that the application does not propose to alter the existing use of the site and would provide considerable energy efficiency benefits therefore an enhancement to an existing education facility, the principle of development is acceptable.

7.3 Residential Amenity

7.4 Policy BE1 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012) states that all new development should seek to protect the amenity of surrounding land and buildings, particularly residential properties.

7.5 Policy DMHB 11 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) states that development proposals should not adversely impact on the amenity, daylight and sunlight of adjacent properties and open space.

7.6 Given the nature of the proposal and the separation from residential properties, it is considered that the proposal would not adversely affect the amenities of neighbouring occupiers.

7.7 The proposed solar panels would be fitted with anti-glare technology by including a roughness to the glass panel to diffuse the light reflection. It is not considered that there would be an unacceptable level of glare created to the adjacent school building at Yeading primary School nor the residential property at number 6 Carlyon Road, given the location of the solar panels on the rear roof slopes.

7.8 Design / Impact on the Character and Appearance of the Area and Locally Listed Building

7.9 Policy DMHB 11 of the Hillingdon Local Plan: Part Two - Development Management Policies (January 2020) requires all development to be designed to the highest standards and incorporate principles of good design, either complementing or improving the character and appearance of the area.

7.10 Policy DMHB 3 of the Local Plan part 2 2020) states that:

A) There is a general presumption in favour of the retention of buildings, structures and features included in the Local List. The Council will take into account the effect of a proposal on the building's significance and the scale of any harm of loss when considering planning applications, including those for major alterations and extensions. Proposals will be permitted where they retain the significance, appearance, character or setting of a Locally Listed Building.

B) Applications should include a Heritage Statement that demonstrates a clear understanding of the importance of the structure and the impact of the proposals on the significance of the Locally Listed Building.

7.11 The proposed development includes the provision of 344 Solar panels to the pitched roof of the existing infant and nursery school. The school is a Locally Listed Building. The proposed panels as shown on the proposed roof plan (drawing 4) would be located on the rear (south) and side (east) roof slopes and would not be

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readily visible from the front, principal elevation, preserving its character and appearance. The panels would be set in from the external edge of the pitch roofs and would be set down from the main ridgeline of the building.

- 7.12 The Council's Conservation and Urban Design Officer was consulted on the application. In summary the Conservation Officer has no objection to their siting and appearance given their location on the rear and side roof slopes. The development has been designed in a way to minimise the impact to the Locally Listed Building and street scene image as they have been positioned away from the principal elevation
- 7.13 It should be noted that in terms of its design, the proposed solar panels would mostly comply with permitted development under Part 14, Class J. However, as the panels would be located within one meter of the external edge of the roof, planning permission is required. As discussed in the above paragraphs, it is not considered that the proposed solar panels would cause significant harm to the character and appearance or the historic fabric of the Locally Listed Building nor the surrounding area given their location of the side and rear roof slopes away from the principal elevation.
- 7.14 Any potential harm would be limited and where harm is identified the development plan encourages decision makers to apply the planning balance. In this case there are significant benefits in terms of energy efficiency and carbon reduction which would clearly outweigh the limited harm identified.
- 7.15 Overall, it is considered that the proposed development complies with the objectives of Policies DMHB 3 and DMHB 11 of the Local Plan Part 2 (2020).
- 7.16 Highways and Parking
- 7.17 The parking provision and traffic generation would remain unaffected by the proposal, in accordance with Policy DMT 6 of the Hillingdon Local Plan Part 2: Development Management Policies (2020). Furthermore, the Highways Officer has no objections to the proposal, subject to the submission of a Construction Logistics Plan as there are concerns over the additional vehicle movements generated by the construction works which have the potential to impact access and parking for the school and local highway network and the functionality of the school.
- 7.18 Noise
- 7.19 It is not considered that the proposed development would lead to an increase in noise generated from the site. As such no further comment is made.
- 7.20 Air Quality
- 7.21 The application site is not located within an Air Quality Focus Area. The proposed development would provide a source of renewable energy helping towards the

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zero-carbon emission scheme. As such no further information is required in respect of air quality.

7.22 Accessibility

7.23 Given the proposed development is solely for solar panels to the roof of the existing school. It is not considered that there would be any accessibility concerns generated.

7.24 Trees and Landscaping

7.25 The proposed development would not have a negative impact on any trees or landscaping within the site. Any trees are located a sufficient distance away from the proposed development as to not be impacted. The site is not located within an area covered by a tree preservation order nor is it located within a Conservation Area. As such, no further information is required.

7.26 Ecology

7.27 It is not considered the proposed development would have a negative impact on ecology in the surrounding area. As such, no further information is required.

7.28 Flooding and Drainage

7.29 Part of the application site is located within Flood Zone 2. However, as the proposed development is located on the roof of the existing school, there would be no chance of increased flooding. As such, no further information is required.

7.30 Land Contamination

7.31 The application site is not located upon contaminated land, and given the nature of the proposed development on the roof slopes of the existing school, there is no requirement for further information.

7.32 Energy

7.33 Policy DME1 2 of the Local Plan Part 2 (2020) states that:

A) All developments are required to make the fullest contribution to minimising carbon dioxide emissions in accordance with London Plan targets.

B) All major development proposals must be accompanied by an energy assessment showing how these reductions will be achieved.

C) Proposals that fail to take reasonable steps to achieve the required savings will be resisted. However, where it is clearly demonstrated that the targets for carbon emissions cannot be met onsite, the Council may approve the application and seek an off-site contribution to make up for the shortfall.

7.34 Policy SI 2 and 3 of the London Plan (2021) supports the provision of renewable energy to minimise greenhouse gas emissions.

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- 7.35 Policy DMCI 2 of the Local Plan Part 2 (2020) supports refurbishment, re-use and provision of new community infrastructure.
- 7.36 The proposed development is for the installation of 344 solar panels to the pitched roof of the existing school with a capacity of 155kWp. The applicant has provided a product specification sheet which shows that the proposed solar panels will provide between 420-440W of power output, include an anti-glare design. The proposed development will contribute towards a zero-carbon electricity scheme and the principle of the development will comply with the overall objectives of Policies SI 2 and 3 of the London Plan (2021) and Policies DMEI 2 and DMCI 2 of the Hillingdon Local Plan Part 2 (2020).

8 Other Matters

8.1 Human Rights

8.2 The development has been assessed against the provisions of the Human Rights Act, and in particular Article 1 of the First Protocol and Article 8 of the Act itself. This Act gives further effect to the rights included in the European Convention on Human Rights. In arriving at this recommendation, due regard has been given to the applicant's reasonable development rights and expectations which have been balanced and weighed against the wider community interests, as expressed through third party interests / the Development Plan and Central Government Guidance.

8.3 Equality

8.4 Due consideration has been given to Section 149 of the Equality Act with regard to the Public Sector Equality Duty in the assessment of this planning application. No adverse equality impacts are considered to arise from the proposal.

8.5 Local Finance Considerations and CIL

8.6 Not applicable. The proposed development is not CIL liable.

9 Conclusion / Planning Balance

9.1 Planning permission is sought for the Installation of solar panels on the school roofs.

9.2 The proposal would not cause harm to the visual amenity of the street scene, nor would it have detrimental harm to the character and appearance of the Locally Listed Building. The proposed solar panels would provide the school with a renewable energy source and would assist in reducing carbon emissions.

9.3 The proposal is considered to comply with the Development Plan and no material considerations indicate that a contrary decision should be taken. Consequently, the application is recommended for approval subject to the conditions set out in Appendix 1 (below).

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PART 1 – Members, Public & Press

10 Background Papers

- 10.1 Relevant published policies and documents taken into account in respect of this application are set out in the report. Documents associated with the application (except exempt or confidential information) are available on the [Council's website here](#), by entering the planning application number at the top of this report and using the search facility. Planning applications are also available to inspect electronically at the Civic Centre, High Street, Uxbridge, UB8 1UW upon appointment, by contacting Planning Services at planning@hillington.gov.uk.

APPENDICES

Planning Application

17997/APP/2024/1610

Appendix 1: Recommended Conditions and Informatives

Conditions

1. COM3 **Time Limit**

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

REASON

To comply with Section 91 of the Town and Country Planning Act 1990.

2. COM4 **Accordance with Approved Plans**

The development hereby permitted shall not be carried out except in complete accordance with the details shown on the submitted plans

Location Plan

Yeading Infants and Nursery - Scaled Map

Yeading Infants and Nursery School - Cross Section

Yeading Infant and Nursery School - Western elevation Plan - DWG 1

Yeading Infant and Nursery School - Northern elevation Plan - DWG 1

Yeading Infant and Nursery School - Southern elevation Plan - DWG 1

Yeading Infant and Nursery School - Eastern elevation Plan - DWG 1

Yeading Infant and Nursery School -Roof Plan with Proposed Installation - DWG 4

and shall thereafter be retained/maintained for as long as the development remains in existence.

REASON

To ensure the development complies with the provisions of Hillingdon Local Plan Parts 1 (November 2012) and 2 (January 2020) and the London Plan (2021).

3. NONSC **Removal of PV panels when no longer required**

The solar PV equipment on the main roof will be removed as soon as reasonably practicable when no longer required.

REASON

To safeguard the visual amenities of the area and to ensure that the proposed development does not have an adverse effect upon the appearance of the existing building and conservation area in accordance with Policies DMHB 3 and DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

4. OM19 **Construction Management Plan**

Prior to development commencing, the applicant shall submit a construction management plan to the Local Planning Authority for its approval. The plan shall detail:

- (i) The phasing of development works
- (ii) The hours during which development works will occur (please refer to informative I15 for maximum permitted working hours).
- (iii) A programme to demonstrate that the most valuable or potentially contaminating materials and fittings can be removed safely and intact for later re-use or processing.
- (iv) Measures to prevent mud and dirt tracking onto footways and adjoining roads (including wheel washing facilities).
- (v) Traffic management and access arrangements (vehicular and pedestrian) and parking provisions for contractors during the development process (including measures to reduce the numbers of construction vehicles accessing the site during peak hours).
- (vi) Measures to reduce the impact of the development on local air quality and dust through minimising emissions throughout the demolition and construction process.
- (vii) The storage of demolition/construction materials on site.

The approved details shall be implemented and maintained throughout the duration of the demolition and construction process.

REASON

To safeguard the amenity of surrounding areas in accordance with Policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

Informatives

1.

The Equality Act 2010 seeks to protect people accessing goods, facilities and services from discrimination on the basis of a 'protected characteristic', which includes those with a disability. As part of the Act, service providers are obliged to improve access to and within the structure of their building, particularly in situations where reasonable adjustment can be incorporated with relative ease. The Act states that service providers should think ahead to take steps to address barriers that impede disabled people.

2. I15 **Control of Environmental Nuisance from Construction Work**

Nuisance from demolition and construction works is subject to control under The Control of Pollution Act 1974, the Clean Air Acts and other related legislation. In particular, you should ensure that the following are complied with:-

A. Demolition and construction works which are audible at the site boundary shall only be carried out between the hours of 08.00 and 18.00 hours Monday to Friday and between the hours of 08.00 hours and 13.00 hours on Saturday. No works shall be carried out on

Sundays, Bank or Public Holidays.

B. All noise generated during such works shall be controlled in compliance with British Standard Code of Practice BS 5228:2009.

C. Dust emissions shall be controlled in compliance with the Mayor of London's Best Practice Guidance 'The Control of dust and emissions from construction and demolition.

D. No bonfires that create dark smoke or nuisance to local residents.

You are advised to consult the Council's Environmental Protection Unit (www.hillingdon.gov.uk/noise Tel. 01895 250155) or to seek prior approval under Section 61 of the Control of Pollution Act if you anticipate any difficulty in carrying out construction other than within the normal working hours set out in (A) above, and by means that would minimise disturbance to adjoining premises.

3. I52 Compulsory Informative (1)

The decision to GRANT planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

I53 Compulsory Informative (2)

The decision to GRANT planning permission has been taken having regard to the policies and proposals in the Hillingdon Local Plan Part 1 (2012) and Part 2 (2020) set out below, including Supplementary Planning Guidance, and to all relevant material considerations, including The London Plan (2021) and national guidance.

DMCI 2	New Community Infrastructure
DMEI 2	Reducing Carbon Emissions
DMEI 9	Management of Flood Risk
DMHB 11	Design of New Development
DMHB 12	Streets and Public Realm
DMHB 14	Trees and Landscaping
DMHB 3	Locally Listed Buildings
DMT 6	Vehicle Parking
LPP D3	(2021) Optimising site capacity through the design-led approach
LPP D8	(2021) Public realm
LPP SI2	(2021) Minimising greenhouse gas emissions

LPP SI3 (2021) Energy infrastructure
NPPF12 -23 NPPF12 23 - Achieving well-designed and beautiful places
NPPF14 -23 NPPF14 23 - Meeting the challenge of climate change, flooding and coastal change
NPPF4 -23 NPPF4 23 - Decision making

Appendix 2: Relevant Planning History

- 17997/AA/98/2214 Yeading Junior School Carlyon Road Hayes
Erection of a single storey extension to provide an assembly hall addition, two changing rooms and toilet
Decision: 09-03-1999 Approve
Deemed Hill.
- 17997/AB/99/0138 Yeading Junior School Carlyon Road Hayes
Renewal of planning permission ref.17997M/93/1919 dated 25/01/94; Retention of double mobile classroom
Decision: 23-02-1999 Approve Limited
Time
- 17997/APP/2000/2437 Yeading Infants School Carlyon Road Hayes
RENEWAL OF PLANNING PERMISSION REF. 17997T/95/782 DATED 03/07/95;
RETENTION OF A MOBILE CLASSROOM
Decision: 12-01-2001 Approve Limited
Time
- 17997/APP/2001/252 Yeading Infant School Carlyon Road Hayes
ERECTION OF A SINGLE STOREY EXTENSION WITHIN THE SCHOOL COURTYARD
Decision: 09-04-2001 Approve
Deemed Hill.
- 17997/APP/2002/2443 Yeading Junior School Carlyon Road Hayes
ERECTION OF A TWO STOREY AND SINGLE STOREY EXTENSION (INVOLVING
DEMOLITION OF EXISTING MODULAR CLASSROOM)
Decision: 21-01-2003 Approve
Deemed Hill.
- 17997/APP/2007/159 Yeading Junior School Carlyon Road Hayes
CHANGE OF USE OF CARETAKERS DWELLING (CLASS C3) TO ANCILLARY
EDUCATION USE TO PROVIDE MEETING ROOM, CRÈCHE FACILITY FOR 4/5
CHILDREN AND OFFICE
Decision: 14-06-2007 Approved

17997/APP/2007/2169 Yeading Junior School Carlyon Road Hayes
ERECTION OF A SINGLE STOREY SIDE EXTENSION TO THE SOUTH-EAST
ELEVATION OF THE EXISTING JUNIOR SCHOOL BUILDING.

Decision: 13-09-2007 Approved

17997/APP/2007/2334 Yeading Junior School Carlyon Road Hayes
DETAILS OF HOURS OF USE - CONDITION 2, ACCESS ARRANGEMENTS -
CONDITION 3, AND SECURITY MEASURES - CONDITION 8 IN COMPLIANCE WITH
PLANNING PERMISSION REF.17997/APP/2007/159 DATED 14/06/2007:
CHANGE OF USE OF CARETAKERS DWELLING (CLASS C3) TO ANCILLARY
EDUCATION USE TO PROVIDE MEETING ROOM, CRECHE FACILITY FOR 4/5
CHILDREN AND OFFICE.

Decision: 17-09-2007 Approved

17997/APP/2009/1148 Yeading Infants School Carlyon Road Hayes
Single storey building for use as childrens and adults centre, with associated parking and
play area.

Decision: 31-07-2009 Approved

17997/APP/2010/902 Yeading Infants School Carlyon Road Hayes
Details in compliance with conditions 3 (ground levels), 4 (storage of refuse bins), 5
(boundary treatment), 6 (tree survey), 8 (tree protection), 9 (landscape scheme), 11
(landscape maintenance), 13 (access to building), 15 (security measures), 16 (parking
arrangements), 17 (demolition and construction management plan), 18 (secure cycle
storage), 19 (sustainable urban drainage) and 20 (materials) of planning permission ref:
17997/APP/2009/1148 dated 31/07/2009: Single storey building for use as childrens and
adults centre, with associated parking and play area.

Decision: 04-07-2012 Approved

17997/APP/2011/2029 Yeading Junior School Carlyon Road Hayes
Erection of single storey rear extension to educational premises (former caretaker's house).

Decision: 01-11-2011 Approved

17997/APP/2011/2923 Yeading Infants School Carlyon Road Hayes
Single storey conservatory extension and enclosed link between existing school and
childrens centre.

Decision: 25-04-2012 Approved

17997/APP/2013/1343 Yeading Infant School Carlyon Road Hayes

Extension of existing school to provide a new classroom and a new reading room, with associated internal and external works.

Decision: 18-07-2013 Approved

17997/APP/2013/3039 Yeading Infants School Carlyon Road Hayes

Details pursuant to condition 7 (Sustainable Water Management) of planning permission 17997/APP/2013/1343 dated 18/07/2013 for the extension of existing school to provide a new classroom and a new reading room, with associated internal and external works.

Decision: 10-12-2013 Approved

17997/APP/2020/1111 Yeading Infant School Carlyon Road Hayes

Demolition of an existing modular classroom unit and erection of a similar modular unit comprising of two classrooms with all associated external works

Decision: 27-05-2020 Approved

17997/APP/2020/4257 Yeading Infant School Carlyon Road Hayes

Proposed alteration and extension of the existing kitchen and dining hall building

Decision: 11-02-2021 Approved

17997/APP/2023/3294 Yeading Infant School Carlyon Road Hayes

Demolition of existing single storey buildings and the erection of a new single storey kitchen and dining hall facility with associated external works.

Decision: 08-04-2024 Approved

17997/B/84/1220 Yeading Infant & Junior Schools Carlyon Road Hayes

Formation of access (P)

Decision: 12-09-1984 Approve
Deemed Hill.

17997/C/84/1950 Yeading Infant & Junior Schools Carlyon Road Hayes

Householder dev. (small extension,garage etc) (P)

Decision: 04-01-1985 Approve
Deemed Hill.

17997/E/86/2194 Yeading Infant & Junior Schools Carlyon Road Hayes

Installation of self closing doors

Decision: 21-01-1987 Approve
Deemed Hill.

17997/F/87/2179 Yeading Infant & Junior Schools Carlyon Road Hayes
Erection of chain link fence to a height of 3.6M along the frontage of school to Carlyon Road
Decision: 14-12-1987 Approved

17997/G/90/0256 Yeading Primary School Carlyon Road Hayes
Erection of a 3.6m high chain link fence to enclose school playing fields
Decision: 21-03-1990 Approve
Deemed Hill.

17997/K/92/0515 Yeading Junior School Carlyon Road Hayes
Erection of a radio base station including a prefabricated equipment cabin, 2 microwave
dishes and security fencing
Decision: 08-07-1992 Approved

17997/L/93/1850 Yeading Junior School Carlyon Road Hayes
Erection of a front boundary wall
Decision: 10-01-1994 Approved

17997/M/93/1919 Yeading Infants School Carlyon Road Hayes
Erection of a double mobile classroom unit
Decision: 25-01-1994 Approve Limited
Time

17997/N/93/1976 Yeading Infant School Carlyon Road Hayes
Erection of a nursery school
Decision: 20-04-1994 Approve
Deemed Hill.

17997/PRC/2018/190 Yeading Infant School Carlyon Road Hayes
Replacing temporary classrooms; new kitchen and dining hall.
Decision: 20-08-2020 No Further
Action(P)

17997/S/94/1215 Yeading Infants School Carlyon Road Hayes

Details of external materials in compliance with condition 5 of planning permission ref. 17997N/93/ 1976 dated 20/04/94; Erection of a nursery school

Decision: 05-10-1994 Approve
Deemed Hill.

17997/T/95/0782 Yeading Infants School Carlyon Road Hayes
Erection of a double mobile classroom

Decision: 03-07-1995 Approve Limited
Time

17997/X/95/1836 Yeading Infant & Junior Schools Carlyon Road Hayes
Improvements to existing educational premises involving a single storey extension to Infants School, part single storey, part two storey extension to Junior School, toilet block extension, demolition of old nursery and Junior School single buildings, construction of hard play areas, formation of vehicular turning circle on Carlyon Road and change of use of vacant land to form enlarged school playing field

Decision: 16-02-1996 Approved

17997/Y/96/1082 Yeading Infant & Junior Schools Carlyon Road Hayes
Details of landscaping scheme including siting of hard play areas in compliance with condition 3 of planning permission ref.17997X/95/1836 dated 16/02/96; Improvements to existing educational premises involving a single storey extension to Infants School, part single storey, part two storey extension to Junior School, toilet block extension, demolition of old nursery and Junior School single buildings, construction of hard play areas, formation of vehicular turning circle on Carlyon Road and change of use of vacant land to form enlarged school playing field

Decision: 24-09-1999 Approved

Appendix 3: List of Relevant Planning Policies

The following Local Plan Policies are considered relevant to the application:-

Part 1 Policies:

- PT1.HE1 (2012) Heritage
- PT1.BE1 (2012) Built Environment

Part 2 Policies:

- DMEI 2 Reducing Carbon Emissions
- DMEI 9 Management of Flood Risk
- DMCI 2 New Community Infrastructure
- DMHB 3 Locally Listed Buildings
- DMHB 11 Design of New Development
- DMHB 12 Streets and Public Realm
- DMHB 14 Trees and Landscaping
- DMT 6 Vehicle Parking
- NPPF12 -23 NPPF12 23 - Achieving well-designed and beautiful places
- NPPF14 -23 NPPF14 23 - Meeting the challenge of climate change, flooding and coastal change
- NPPF4 -23 NPPF4 23 - Decision making
- LPP D3 (2021) Optimising site capacity through the design-led approach
- LPP D8 (2021) Public realm
- LPP SI2 (2021) Minimising greenhouse gas emissions
- LPP SI3 (2021) Energy infrastructure

Report of the Head of Development Management and Building Control Committee Report Part 2 – Application Report

Case Officer: Christos Chrysanthou	11242/APP/2024/1302
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Date Application Valid:	26-07-24	Statutory / Agreed Determination Deadline:	04-10-24
Application Type:	Full	Ward:	Pinkwell

Applicant: **London Borough of Hillingdon**

Site Address: **Pinkwell Primary School, Pinkwell Lane, Hayes, Hillingdon, UB3 1PG**

Proposal: **Replacement of the existing timber fence with 3.0m high V mesh security fencing.**

Summary of Recommendation: **GRANT planning permission subject to conditions**

Reason Reported to Committee: **Required under Part 6 of the Planning Scheme of Delegation (the Council is the Applicant)**



Summary of Recommendation:

GRANT planning permission subject to the conditions set out in Appendix 1.

1 Executive Summary

- 1.1 The application seeks planning permission for the Replacement of the existing timber fence with 3.0m high V mesh security fencing. The proposal is minor in nature and is not considered harmful to the character and appearance of the area or the openness of the adjacent Green belt.
- 1.2 More importantly the proposal will facilitate an improvement to the school which is supported within the Development Plan at local and regional level and at national level through the provisions of the Framework.
- 1.3 The planning application is therefore recommended for approval subject to the conditions set out in Appendix 1. A condition is recommended to secure the painting of the fencing along the western boundary in a dark green colour to enable the proposal to assimilate with the adjacent Green Belt.

2 The Site and Locality

- 2.1 Pinkwell Primary School occupies an approximately 4.3 hectare irregularly shaped plot located on the southern site of Pinkwell Lane in Hayes. The main school building, a part single-storey and part two-storey building, and the playground occupy the northern part of the site and the playing fields occupy the southern part of the site. Three temporary classroom blocks are located towards the centre of the site.
- 2.2 The school site is bounded to the north by Pinkwell Lane, beyond which is Pinkwell Park and residential properties; to the east by Harlington Community School; and to the south and west by open land falling within the Green Belt. Residential properties fronting Pinkwell Lane abut part of the western boundary.

Figure 1: Location Plan (application site edged red)



Figure 2: Street View Image of the Application Property



3 Proposal

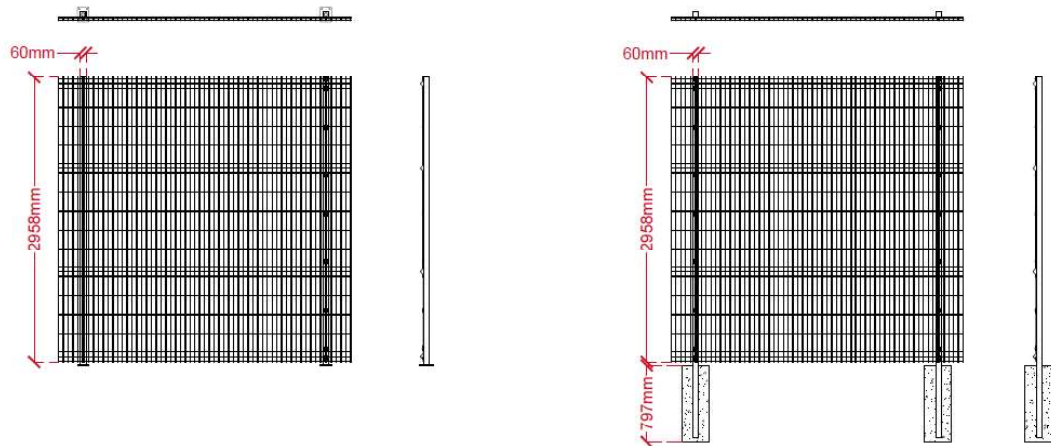
- 3.1 The proposed scheme comprises the replacement of the existing timber fence to a section of the school with 3.0m high V mesh fencing. The proposed fencing would replace a section of timber fencing along the western boundary measuring 55metres, which is shown in the plan below, edged in green. The existing fencing to be retained is also shown in the plan below, edged in red.

Figure 3: Proposed Plan (please note – larger version of plan can be found in the Committee Plan Pack)



EXISTING AND PROPOSED FENCING PLAN

Figure 4: Proposed Elevations (please note – larger version of plan can be found in the Committee Plan Pack)



PROPOSED FENCE (BOLT-ON OPTION)

SCALE : 1:50

PROPOSED FENCE (DIG-IN OPTION)

SCALE : 1:50

4 Relevant Planning History

4.1 A list of the relevant planning history related to the property can be found in Appendix 2.

5 Planning Policy

5.1 A list of planning policies relevant to the consideration of the application can be found in Appendix 3.

6 Consultations and Representations

6.1 36 neighbours were consulted by letter dated 5th August 2024. The consultation period ended on 27th August 2024. No comments were received.

6.2 Representations received in response to public consultation are summarised in Table 1 (below). Consultee responses received are summarised in Table 2 (below). Full copies of the responses have also separately been made available to Members.

Table 1: Summary of Representations Received

Representations	Summary of Issues Raised	Planning Officer Response
None	N/A	N/A

Table 2: Summary of Consultee Responses

Consultee and Summary of Comments	Planning Officer Response
Access Officer No accessibility concerns are raised.	Noted.
Highway Authority There are no highway objections to this proposal	Noted.

7 Planning Assessment

Impact on the character and appearance of the area

- 7.1 Policy D3 of the London Plan (2021) states that: Development proposals should: D1) enhance local context by delivering buildings and spaces that positively respond to local distinctiveness through their layout, orientation, scale, appearance and shape, with due regard to existing and emerging street hierarchy, building types, forms and proportions.
- 7.2 Policy BE1 of the Hillingdon Local Plan: Part One Strategic Policies (2012) seeks a quality of design in all new development that enhances and contributes to the area in terms of form, scale and materials; is appropriate to the identity and context of the townscape; and would improve the quality of the public realm and respect local character.
- 7.3 Policy DME1 6 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) states - New development adjacent to the Green Belt, Metropolitan Open Land, Green Chains, Sites of Importance for Nature Conservation, Nature Reserves, countryside, green spaces or the Blue Ribbon Network should incorporate proposals to assimilate development into the surrounding area by the use of extensive peripheral landscaping to site boundaries.
- 7.4 Policy DMHB 11 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) states that new development will be required to be designed to the highest standards and incorporate principles of good design.
- 7.5 As shown in Figure 4 of this report, there are two installation options for the fencing, one being 'bolt-on' and the other being 'dig-in'. Regardless of which option is installed, the appearance of the fencing would not differ. Overall, the proposal would be read in the context of the school and would not attract undue attention due to its unobtrusive and functional design.
- 7.6 In the interests of mitigating any visual impact on the openness of the adjacent Green Belt a condition is recommended that the section of fencing along the western boundary is painted in a dark green colour. This would help reduce any

visibility of the fencing from the surroundings and provide a colour scheme that would help the proposal assimilate with the Green belt site context.

- 7.7 The proposed scheme is considered to accord with Policy BE1 of the Local Plan: Part One (2012), Policies DME1 6 and DMHB 11 of the Local Plan: Part Two - Development Management Policies (2020) and Policy D3 of the London Plan (2021).

Impact on the amenity of the area

- 7.8 Policy D3 of the London Plan (2021) states Part D7) that development proposals should deliver appropriate outlook, privacy and amenity.
- 7.9 Policy DMHB 11 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) seeks to ensure that development proposals do not adversely impact on the amenity, daylight and sunlight of adjacent properties and open space.
- 7.10 Due to the nature of the proposal, there are no envisaged impacts on the amenity of the area. The proposed fencing would be erected on the western boundary abutting a field and away from neighbouring dwellings on Pinkwell Lane.
- 7.11 The proposed scheme is considered to accord with Policy DMHB 11 of the Local Plan: Part Two - Development Management Policies (2020) and Policy D3 of the London Plan (2021).

Highway safety and parking

- 7.12 Policy T4 Part F of the London Plan (2021) states - Development proposals should not increase road danger.
- 7.13 Policy T6 Part D of the London Plan (2021) states - The maximum car parking standards set out in Policy T6 .1 Residential parking to Policy T6 .5 Non-residential disabled persons parking should be applied to development proposals and used to set local standards within Development Plans.
- 7.14 Policies DMT 1 and DMT 2 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) require the Council to consider whether the traffic generated by proposed developments is acceptable in terms of the local highway and junction capacity, traffic flows and conditions of general highway or pedestrian safety.
- 7.15 Policy DMT 6 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) seeks to ensure that all development is in accordance with the car parking standards set out in Appendix C, Table 1 unless it can be demonstrated that a deviation from the standard would not result in a deleterious impact on the surrounding road network.

Hillingdon Planning Committee – 2nd October 2024

PART 1 – Members, Public & Press

- 7.16 The Highway Authority do not raise an objection to the application. It is therefore considered that the proposal would not exacerbate the demand for street parking or prejudice highway safety, in accordance with Policies DMT 1, DMT 2 and DMT 6 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) and Policies T4 and T6 of the London Plan (2021).

8 Other Matters

Human Rights

- 8.1 The development has been assessed against the provisions of the Human Rights Act, and in particular Article 1 of the First Protocol and Article 8 of the Act itself. This Act gives further effect to the rights included in the European Convention on Human Rights. In arriving at this recommendation, due regard has been given to the applicant's reasonable development rights and expectations which have been balanced and weighed against the wider community interests, as expressed through third party interests / the Development Plan and Central Government Guidance.

Equality

- 8.2 Due consideration has been given to Section 149 of the Equality Act with regard to the Public Sector Equality Duty in the assessment of this planning application. No adverse equality impacts are considered to arise from the proposal.

9 Conclusion / Planning Balance

- 9.1 For the reasons set out in this Committee Report, it is considered that the proposal would comply with the objectives of national, regional and local planning policies and guidance. It is therefore recommended that the application be approved, subject to the imposition of the conditions set out in Appendix 1.

10 Background Papers

- 10.1 Relevant published policies and documents taken into account in respect of this application are set out in the report. Documents associated with the application (except exempt or confidential information) are available on the [Council's website here](#), by entering the planning application number at the top of this report and using the search facility. Planning applications are also available to inspect electronically at the Civic Centre, High Street, Uxbridge, UB8 1UW upon appointment, by contacting Planning Services at planning@hillingdon.gov.uk.

APPENDICES

Planning Application

11242/APP/2024/1302

Appendix 1: Recommended Conditions and Informatives

Conditions

1. COM3 **Time Limit**

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

REASON

To comply with Section 91 of the Town and Country Planning Act 1990.

2. COM4 **Accordance with Approved Plans**

The development hereby permitted shall not be carried out except in complete accordance with the details shown on the submitted plans, numbers 01 - Rev P02, 02 - Rev P02, 03 - Rev P02, 04 - Rev P02 and shall thereafter be retained/maintained for as long as the development remains in existence.

REASON

To ensure the development complies with the provisions Hillingdon Local Plan Parts 1 (November 2012) and 2 (January 2020) and the London Plan (2021).

3. COM7 **Materials (Submission)**

Notwithstanding the approved plans, the fencing hereby permitted to be erected along the western boundary shall be painted in a dark green colour. Thereafter the development shall be constructed in accordance with the approved details and be retained as such.

REASON

To ensure that the development presents a satisfactory appearance in accordance with Policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

Informatives

1. I52 **Compulsory Informative (1)**

The decision to GRANT planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

2. I59 **Councils Local Plan : Part 1 - Strategic Policies**

On this decision notice policies from the Councils Local Plan: Part 1 - Strategic Policies appear first, then relevant Local Plan Part 2 (2020), then London Plan Policies (2021). Hillingdon's Full Council adopted the Hillingdon Local Plan: Part 1 - Strategic Policies on 8 November 2012 and the Hillingdon Local Plan Part 2 on 16 January 2020.

Appendix 2: Relevant Planning History

11242/APP/1999/2473 Pinkwell Infants School, Pinkwell Lane Hayes
ERECTION OF AN EXTENSION TO PROVIDE AN ADDITIONAL CLASSROOM

Decision: 14-02-2000 Approved

11242/APP/2000/1147 Pinkwell Primary School Pinkwell Lane Hayes
ERECTION OF EXTENSIONS TO FORM TWO ADDITIONAL CLASSROOMS

Decision: 17-08-2000 Approve
Deemed Hill.

11242/APP/2000/2058 Pinkwell Primary School Pinkwell Lane Hayes
ERECTION OF A SINGLE STOREY EXTENSION TO PROVIDE TWO ADDITIONAL CLASSROOMS

Decision: 06-10-2000 Approve
Deemed Hill.

11242/APP/2003/64 Pinkwell Primary School Pinkwell Lane Hayes
ERECTION OF SINGLE STOREY EXTENSION TO PROVIDE A NEW COMMUNICATION CENTRE

Decision: 24-02-2003 Approved

11242/APP/2003/948 Pinkwell Primary School Pinkwell Lane Hayes
ERECTION OF A SINGLE STOREY BUILDING TO PROVIDE A NEW COMMUNICATION CENTRE

Decision: 10-06-2003 Approved

11242/APP/2007/1130 Pinkwell Primary School Pinkwell Lane Hayes
ERECTION OF A DETACHED MODULAR BUILDING COMPRISING 2 CLASSROOMS AND ANCILLARY FACILITIES.

Decision: 31-07-2007 Approved

11242/APP/2007/2552 Pinkwell Primary School Pinkwell Lane Hayes
ERECTION OF 2 DETACHED MODULAR BUILDINGS COMPRISING SINGLE CLASSROOM AND REARRANGMENT OF EXISTING CAR PARKING FACILITIES.

Decision: 03-10-2007 Approved

11242/APP/2008/3117 Pinkwell Primary School Pinkwell Lane Hayes

ERECTION OF TWO STOREY SIDE/REAR EXTENSION, NEW WINDOWS TO EXISTING FLANK WALL AND EXTENSION TO EXISTING PLAYGROUND.

Decision: 07-01-2009 Approved

11242/APP/2009/1175 Pinkwell Primary School Pinkwell Lane Hayes

Conversion and extension of existing school hall for use as new adult and children's centre.

Decision: 10-08-2009 Approved

11242/APP/2009/2114 Pinkwell Primary School Pinkwell Lane Hayes

Two storey rear extension for use as hall.

Decision: 11-02-2010 Approved

11242/APP/2009/60 Pinkwell Primary School Pinkwell Lane Hayes

Change of use from caretaker's residence to educational teaching rooms including conversion of existing integral garage to teaching room, and alterations to door at side (Retrospective application).

Decision: 18-03-2009 Approved

11242/APP/2010/2054 Pinkwell Primary School Pinkwell Lane Hayes

Construction of a single storey building accommodating 2 combined classrooms (total floor area 159 sq. metres) and toilet facilities.

Decision: 12-10-2010 Approved

11242/APP/2011/834 Pinkwell Primary School Pinkwell Lane Hayes

Installation of temporary mobile double classroom for a period of 3 years.

Decision: 01-06-2011 Approved

11242/APP/2012/146 Pinkwell Primary School Pinkwell Lane Hayes

Erection of a new two classroom extension with associated storage and toilet facilities.

Decision: 19-03-2012 Approved

11242/APP/2012/2877 Pinkwell Primary School Pinkwell Lane Hayes

Expansion of school to include stand alone classroom block and extended nursery, dining room, staff room, reception, library and classrooms with associated car parking and

landscaping.

Decision: 19-12-2012 Withdrawn

11242/APP/2012/3116 Pinkwell Primary School Pinkwell Lane Hayes

Expansion of school to include standalone classroom block and extended nursery, staff room, reception, library and classrooms with associated car parking and landscaping.

Decision: 05-03-2013 Approved

11242/APP/2013/1115 Pinkwell Primary School Pinkwell Lane Hayes

Non-material amendment to planning permission ref: 11242/APP/2012/3116 dated 08/03/13 (expansion of school to include standalone classroom block and extended nursery, staff room, reception, library and classrooms with associated car parking and landscaping) to provide a small extension to the dining hall to improve circulation space.

Decision: 07-05-2013 Approved

11242/APP/2013/2024 Pinkwell Primary School Pinkwell Lane Hayes

Details in compliance with condition 4 (materials) and 8 (fire emergency plan) of permission 11242/APP/2012/3116 (Expansion of school to include stand alone classroom block and extended nursery, staff room, reception, library and classrooms with associated car parking and landscaping).

Decision: 10-09-2013 Approved

11242/APP/2013/2109 Pinkwell Primary School Pinkwell Lane Hayes

Details pursuant to conditions 6 (traffic management plan) and 16 (tree protection) of planning permission ref: 11242/APP/2012/3116 dated 08/03/13; Expansion of school to include standalone classroom block and extended nursery, staff room, reception, library and classrooms with associated car parking and landscaping.

Decision: 06-12-2013 Approved

11242/APP/2013/2218 Pinkwell Primary School Pinkwell Lane Hayes

Details pursuant to condition 12 (electric vehicle charging points) of planning permission ref: 11242/APP/2012/3116 dated 08/03/13; Expansion of school to include standalone classroom block and extended nursery, staff room, reception, library and classrooms with associated car parking and landscaping.

Decision: 27-09-2013 Approved

11242/APP/2013/2409 Pinkwell Primary School Pinkwell Lane Hayes

Details pursuant to condition 7 (lighting) of planning permission ref: 11242/APP/2012/3116

dated 08/03/13; Expansion of school to include stand alone classroom block and extended nursery, staffroom, reception, library and classrooms with associated car parking and landscaping.

Decision: 09-10-2013 Approved

11242/APP/2013/3207 Pinkwell Primary School Pinkwell Lane Hayes

Details in compliance with condition 5 (Sustainable Water Management) of planning permission ref: 11242/APP/2012/3116 dated 08/03/2013; Expansion of school to include standalone classroom block and extended nursery, staff room, reception, library and classrooms with associated car parking and landscaping.

Decision: 17-04-2014 Approved

11242/APP/2014/1691 Pinkwell Primary School Pinkwell Lane Hayes

Non-material amendment to planning permission ref: 11242/APP/2012/3116 dated 08/03/13 (Expansion of school to include standalone classroom block and extended nursery, staff room, reception, library and classrooms with associated car parking and landscaping) to add air handling unit and ductwork to staff room roof; add attenuator units and guard rails to KS2 block; include attenuators, omit louvre, and make minor changes to facade on Year 6 block; and to reduce the area of brown roof on the year 6 block.

Decision: 02-06-2014 Approved

11242/APP/2014/2423 Pinkwell Primary School Pinkwell Lane Hayes

Details pursuant to condition 9 (car park management strategy) of planning permission ref: 11242/APP/2012/3116 dated 08/03/13; Expansion of school to include stand alone classroom block and extended nursery, staff room, reception, library and classrooms with associated car parking and landscaping.

Decision: 15-07-2014 Approved

11242/APP/2014/2637 Pinkwell Primary School Pinkwell Lane Hayes

Non-material amendment to planning permission ref: 11242/APP/2012/3116 dated 08/03/13 (expansion of school to include stand alone classroom block and extended nursery, staff room, reception, library and classrooms with associated car parking and landscaping) to make changes to the external finishes of the nursery extension.

Decision: 11-08-2014 Approved

11242/APP/2014/410 Pinkwell Primary School Pinkwell Lane Hayes

Non-material amendment to planning permission ref: 11242/APP/2012/3116 dated 08/03/13 to add a small (300mm) parapet between the existing ICT room roof and the new library roof to ensure water-tightness (Expansion of school to include stand alone classroom block and extended nursery, staffroom, reception, library and classrooms with associated car

parking and landscaping)

Decision: 26-02-2014 Approved

11242/APP/2014/413 Pinkwell Primary School Pinkwell Lane Hayes

Details pursuant to condition 13 (Travel Plan and cycle stores) of planning permission ref: 11242/APP/2012/3116 dated 08/03/13; Expansion of school to include standalone classroom block and extended nursery, staff room, reception, library and classrooms with associated car parking and landscaping.

Decision: 15-07-2014 Approved

11242/APP/2014/561 Pinkwell Primary School Pinkwell Lane Hayes

Details in part compliance with condition 13 (Travel Plan & cycle storage) of planning permission ref: 11242/APP/2012/3116 dated 08/03/13; Expansion of school to include stand alone classroom block and extended nursery, staff room, reception, library and classrooms with associated car parking and landscaping.

Decision: 15-07-2014 No Further
Action(P)

11242/F/90/2049 Pinkwell Primary School Pinkwell Lane Hayes

Installation of two omni aerials and equipment cabin (Application for determination under Section 64 of the Act)

Decision: 12-12-1990 General
Perm.Devt.

11242/G/92/0513 Pinkwell Junior School Pinkwell Lane Hayes

Erection of a radio base station including metal stub tower, two microwave dishes and prefabricated equipment cabin

Decision: 27-08-1992 Withdrawn

11242/H/92/1291 Pinkwell Junior School Pinkwell Lane Hayes

Installation of microwave dish tower (Application for determination under Section 64 of the Act)

Decision: 10-08-1992 General
Perm.Devt.

11242/M/99/0090 Pinkwell Primary School Pinkwell Lane Hayes

Erection of a staffroom

Decision: 09-03-1999 Approve

Deemed Hill.

Appendix 3: List of Relevant Planning Policies

The following Local Plan Policies are considered relevant to the application:-

Part 1 Policies:

PT1.BE1 (2012) Built Environment

PT1.CI1 (2012) Community Infrastructure Provision

Part 2 Policies:

DMCI 1 Retention of Existing Community Sport and Education Facilities

DMCI 2 New Community Infrastructure

DMEI 12 Development of Land Affected by Contamination

DMEI 14 Air Quality

DMEI 9 Management of Flood Risk

DMHB 11 Design of New Development

DMHB 15 Planning for Safer Places

DMT 2 Highways Impacts

DMT 5 Pedestrians and Cyclists

DMT 6 Vehicle Parking

DMEI 4 Development on the Green Belt or Metropolitan Open Land

DMEI 6 Development in Green Edge Locations

Report of the Head of Development Management and Building Control Committee Report Part 2 – Application Report

Case Officer: Christos Chrysanthou	4501/APP/2024/1618
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Date Application Valid:	18-06-24	Statutory / Agreed Determination Deadline:	04-10-24
Application Type:	Full	Ward:	Colham & Cowley

Applicant: **London Borough of Hillingdon**

Site Address: **Central Depot, 128 Harlington Road, Uxbridge, UB8 3EU**

Proposal: **Installation of an acoustic wall around Service Yard.**

Summary of Recommendation: **GRANT planning permission subject to conditions**

Reason Reported to Committee: **Required under Part 6 of the Planning Scheme of Delegation (the Council is the Applicant)**



Summary of Recommendation:

GRANT planning permission subject to the conditions set out in Appendix 1.

1 Executive Summary

- 1.1 The development would provide a 4 metre high acoustic wall around the eastern section of the service yard at the Harlington Road depot.
- 1.2 The development would benefit nearby residents with by way of a visual screen and expected noise attenuation improving neighbouring residential amenity.
- 1.3 It is considered that the development would be read in the context of the existing depot and would not adversely impact on the character and appearance of the area. No changes are being sought to the onsite car parking layout and there would be no impact on highway safety.
- 1.4 The development is therefore considered to accord with the Development Plan, and it is recommended that planning permission be granted.

2 The Site and Locality

- 2.1 The application site is located on the west side of Harlington Road. The site is currently used as a service yard and is part of the wider Harlington Road Depot.
- 2.2 The depot is used to support many of the Council's services such as refuse collection, road maintenance and other activities.
- 2.3 The area surrounding the application site is predominantly residential. To the north, south and east of the depot are two storey dwellings and flatted developments. To the west and northwest of the depot is Coney Green and Bishopshalt School.
- 2.4 The application site is located on contaminated land and is within a critical drainage area. It is also located just outside of the Hillingdon Village Conservation Area and the Green Belt (which are to the west of the site).

Figure 1: Location Plan (application site edged red)

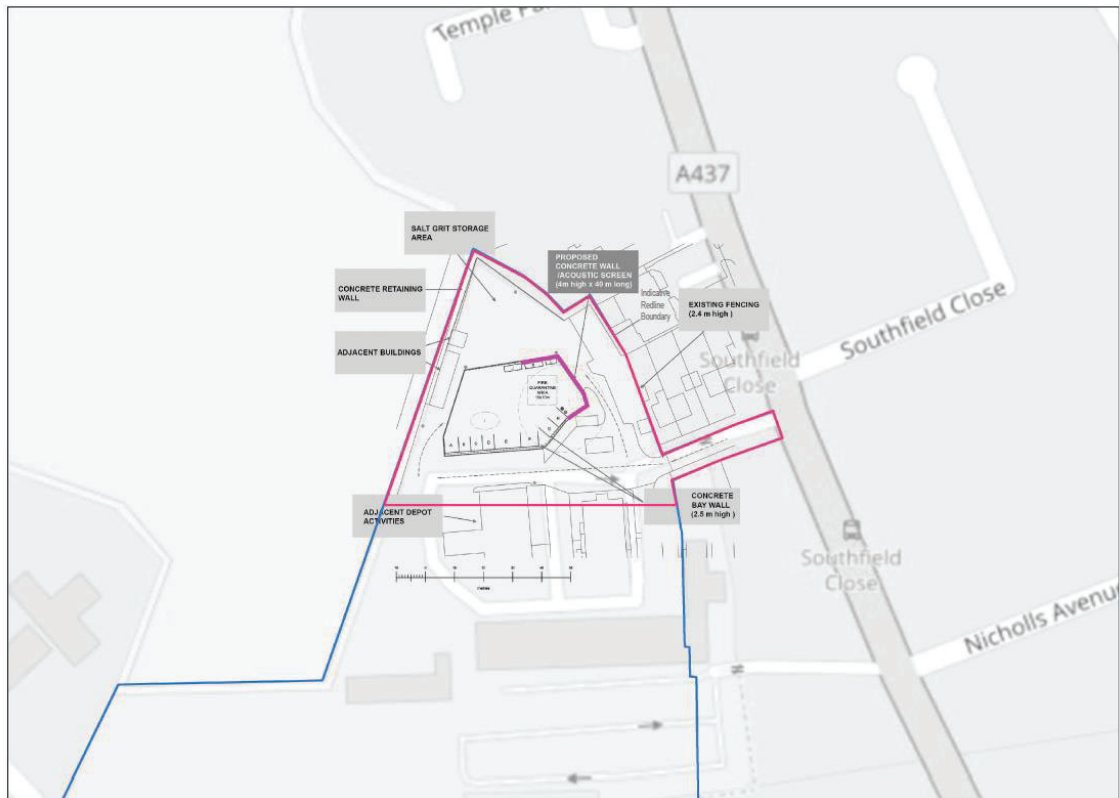


Figure 2: Street View Image of the Application Property



3 Proposal

3.1 The application proposes the installation of an acoustic wall around the eastern section of the service yard within the depot.

3.2 The proposed acoustic wall would be formed of 1metre wide prestressed concrete wall modules that are 4metres in height and constructed on 1.5metre deep

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foundations. The top half of the walls have a thickness of 15cm. The lower half of the walls increases from 15cm to 1.2metres at the base.

Figure 3: Proposed Plan (please note – larger version of plan can be found in the Committee Plan Pack)

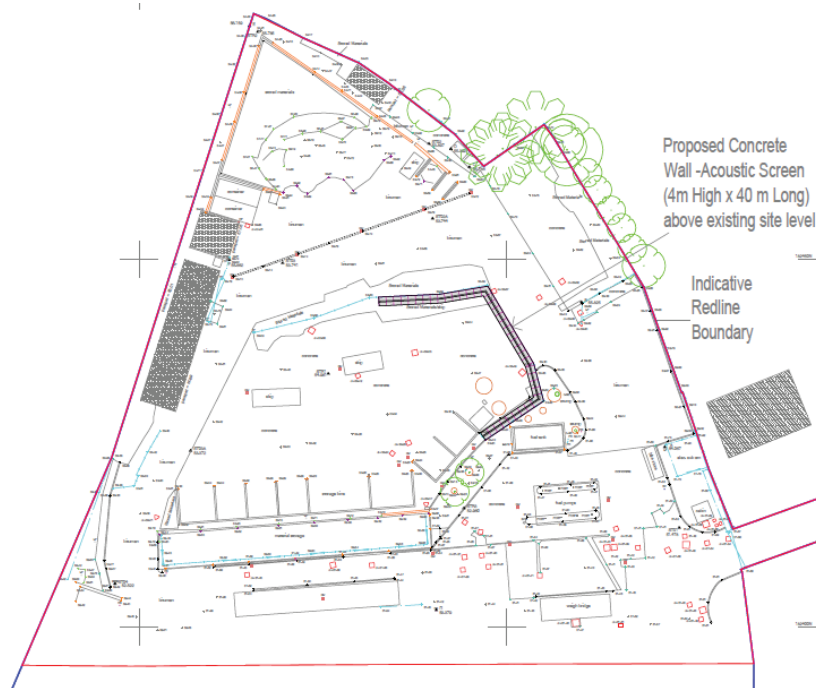
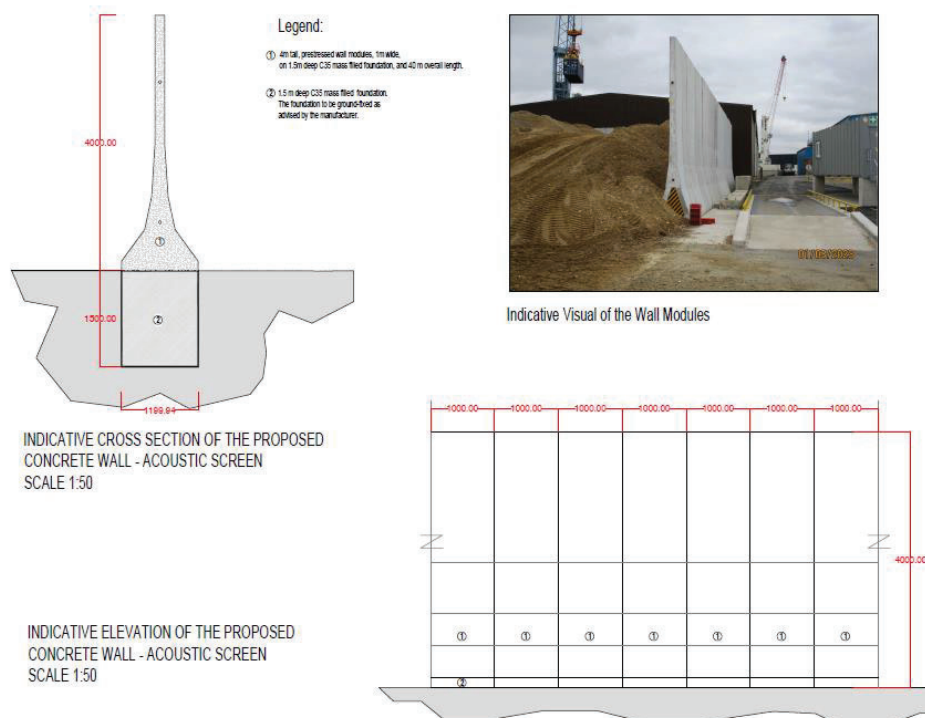


Figure 4: Proposed Elevations (please note – larger version of plan can be found in the Committee Plan Pack)



4 Relevant Planning History

4.1 A list of the relevant planning history related to the property can be found in Appendix 2.

5 Planning Policy

5.1 A list of planning policies relevant to the consideration of the application can be found in Appendix 3.

6 Consultations and Representations

6.1 63 neighbouring properties were originally consulted by letters dated 24-06-24. The consultation period expired on 15-07-24.

6.2 No comments were received.

6.3 Representations received in response to public consultation are summarised in Table 1 (below). Consultee responses received are summarised in Table 2 (below). Full copies of the responses have also separately been made available to Members.

Table 1: Summary of Representations Received

Representations	Summary of Issues Raised	Planning Officer Response
None received.	Not applicable.	No comment.

Table 2: Summary of Consultee Responses

Consultee and Summary of Comments	Planning Officer Response
<p>Highway Officer:</p> <p>The application proposes to erect a 4m x 40m acoustic wall within the London Borough of Hillingdon Central Depot which would not encroach upon visibility and would be unlikely to have a severe impact on the local highway network or highway safety which would be acceptable.</p> <p>The Highway Authority therefore has no objections to this application which would be unlikely to cause highways and traffic impact.</p>	Noted.

Contamination Officer: Having considered the nature of the proposed development and its location, please be advised that we have no objection in relation to the application on land contamination. However, land contamination informatives are recommend for the planning application if approved.	Noted and informatives added.
Noise Specialist: No comment.	Noted.

7 Planning Assessment

Impact on the character & appearance of the area

- 7.1 Paragraph 131 of the NPPF (2023) seeks the creation of high quality, beautiful and sustainable buildings. Parts b) and c) of paragraph 135 of the NPPF (2023) states that planning policies and decisions should ensure that developments are visually attractive as a result of good architecture and are sympathetic to local character and history, including the surrounding built environment.
- 7.2 Policies D3 and D4 of the London Plan (2021) require development proposals to be of a high quality and to enhance the local context by delivering buildings and spaces that positively respond to local distinctiveness.
- 7.3 Policy BE1 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012) states - All new developments should achieve a high quality of design in all new buildings, alterations, extensions and the public realm which enhances the local distinctiveness of the area, contributes to community cohesion and a sense of place.
- 7.4 Policy DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that all development will be required to be designed to the highest standards and incorporate principles of good design. It should take into account aspects including the scale of the development considering the height, mass and bulk of adjacent structures; building plot sizes and established street patterns; building lines and streetscape rhythm and landscaping.
- 7.5 Policy DMHB 12 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) re-emphasises the need for new development to be well integrated with the surrounding area and provides design criteria as to how this would be achieved.
- 7.6 The site is located in a set back position off Harlington Road and the proposed wall would be of limited visibility and read in the context of the existing depot. As such, it is considered that the proposed development would not impact on the character, appearance or visual amenity of the area, in accordance with Policy BE1 of the Hillingdon Local Plan: Part One- Strategic Policies (2012), Policies

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DMHB 11 and DMHB 12 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), Policies D3 and D4 of the London Plan (2021) and the NPPF (2023).

Impact on neighbours

- 7.7 Policy D3 of the London Plan (2021) states Part D7) that development proposals should deliver appropriate outlook, privacy and amenity.
- 7.8 Policy DMHB 11 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) seeks to ensure that development proposals do not adversely impact on the amenity, daylight and sunlight of adjacent properties and open space. The supporting text for this policy states that the Council will expect new development proposals to carefully consider layout and massing in order to ensure development does not result in an increased sense of enclosure and loss of outlook.
- 7.9 There are nearby residential properties to the east of the application site on Harlington Road. The proposed wall would be sited away from these neighbouring properties with separation distances of approximately 22 metres to the nearest neighbouring property No.126 Harlington Road and 27 metres to Thorney Court.
- 7.10 Taking these factors into account, it is considered that the proposed development would not result in a loss of light, outlook or sense of dominance for the neighbouring occupiers.
- 7.11 It is therefore considered that the proposed amendments would not adversely impact on the living conditions of neighbouring occupiers, in accordance with Policy DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), Policy D3 of the London Plan (2021) and the NPPF (2023).

Noise

- 7.12 Policy D14 of the London Plan (2021) states that in order to reduce, manage and mitigate noise to improve health and quality of life, residential and non-aviation development proposals should manage noise by avoiding significant adverse noise impacts on health and quality of life.
- 7.13 The Depot benefits from numerous physical barriers to minimise the transmission of noise, including solid concrete walls around the gritting storage area and building situated adjacent to the waste management area. The service yard is an operational area used for the processing and storing of loose wastes within bays and benefits from a 2.5 metres high solid concrete retaining wall along the southern section.
- 7.14 The proposed concrete wall will provide an additional 40 metres wide & 4 metre high barrier around the eastern section of the service yard which is as close to the sources of noise as practicable.

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- 7.15 The development would therefore improve sound insulation of the service yard and in turn reduce noise levels from neighbouring residential receptors. The barrier would also be providing a visual screen which would further benefit the residential amenity of neighbouring residents.
- 7.16 Due to the modular form of construction, it is considered that there would unlikely be any undue noise, nuisance and disturbance during the construction phase.
- 7.17 As detailed in Table 2 of this report, the Council's Noise specialist has been consulted and does not raise an objection or recommend any conditions.

Traffic impact, Car/cycle parking, pedestrian safety

- 7.18 Policy T6 of the London Plan (2021) states that new residential development should not exceed the maximum parking standards set out in Table 10.3.
- 7.19 Policy DMT 1 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that development proposals will be required to meet the transport needs of the development and address its transport impacts in a sustainable manner.
- 7.20 Policy DMT 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that proposals must ensure that safe and efficient vehicular access to the highway network is provided.
- 7.21 Policy DMT 6 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) requires that proposals comply with the Council's parking standards in order to facilitate sustainable development and address issues relating to congestion and amenity.
- 7.22 As detailed in Table 2 of this report, the Highway Authority have been consulted and do not raise an objection. No conditions are recommended.
- 7.23 Having regard to the Highway Authority's comments, the development would accord with Policy T6 of the London Plan (2021) and Policies DMT 1, DMT 2 and DMT 6 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

Flooding or Drainage Issues

- 7.24 Policy SI 12 of the London Plan (2021) states that development proposals should ensure that flood risk is minimised and mitigated, and that residual risk is addressed.
- 7.25 Policy SI 13 of the London Plan (2021) states that development proposals should aim to achieve greenfield run-off rates and ensure that surface water run-off is managed as close to its source as possible.

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- 7.26 Policy DMEI 9 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that proposals that fail to make appropriate provision for flood risk mitigation, or which would increase the risk or consequences of flooding, will be refused.
- 7.27 Policy DMEI 10 states that development within areas identified at risk from surface water flooding which fail to make adequate provision for the control and reduction of surface water runoff rates will be refused.
- 7.28 The site is in Flood Zone 1 and within a Critical Drainage Area (CDA) identified in the Surface Water Management Plan (SWMP) for Hillingdon. A CDA is the catchment area from which surface water drains and contributes to drainage problems.
- 7.29 Due to the nature of the proposal a full SuDS scheme would in this instance not be required. In the event of this application being recommended for approval, an informative would be attached with further guidance on how the applicant can reduce the surface water from the site entering the sewers.
- 7.30 Subject to the above informative, it is considered that the development would not significantly increase the risk of surface water flooding at the site or elsewhere in compliance with Policies DMEI 9 and DMEI 10 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) and Policies SI 12 and SI 13 of the London Plan (2021).

Land Contamination

- 7.31 Policy DMEI 12 of the Local Plan: Part Two (2020) requires proposals for development on potentially contaminated sites to be accompanied by at least an initial study of the likely contaminants. Conditions will be imposed where planning permission is given for development on land affected by contamination to ensure all the necessary remedial works are implemented, prior to commencement of development.
- 7.32 Due to the limited scale of the proposal, it is considered that the development would be unlikely to give rise to any land contamination issues.
- 7.33 As detailed in Table 2 of this report, the Council's land contamination officer has been consulted and does not raise an objection subject to informatives.
- 7.34 Subject to the informatives recommended by the Council's land contamination officer, that would be attached to any approval, the proposal would accord with DMEI 12 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

8 Other Matters

Human Rights

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- 8.1 The development has been assessed against the provisions of the Human Rights Act, and in particular Article 1 of the First Protocol and Article 8 of the Act itself. This Act gives further effect to the rights included in the European Convention on Human Rights. In arriving at this recommendation, due regard has been given to the applicant's reasonable development rights and expectations which have been balanced and weighed against the wider community interests, as expressed through third party interests / the Development Plan and Central Government Guidance.

Equality

- 8.2 Due consideration has been given to Section 149 of the Equality Act with regard to the Public Sector Equality Duty in the assessment of this planning application. No adverse equality impacts are considered to arise from the proposal.

9 Conclusion / Planning Balance

- 9.1 For the reasons set out in this Committee Report, it is considered that the proposal would comply with the objectives of national, regional and local planning policies and guidance. It is therefore recommended that the application be approved, subject to the imposition of the conditions set out in Appendix 1.

10 Background Papers

- 10.1 Relevant published policies and documents taken into account in respect of this application are set out in the report. Documents associated with the application (except exempt or confidential information) are available on the [Council's website here](#), by entering the planning application number at the top of this report and using the search facility. Planning applications are also available to inspect electronically at the Civic Centre, High Street, Uxbridge, UB8 1UW upon appointment, by contacting Planning Services at planning@hillingdon.gov.uk.

APPENDICES

Planning Application

4501/APP/2024/1618

1. COM3 Time Limit

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

REASON

To comply with Section 91 of the Town and Country Planning Act 1990.

2. COM4 Accordance with Approved Plans

The development hereby permitted shall not be carried out except in complete accordance with the details shown on the submitted plans, numbers 6718-ACC-00-ZZ-DR-A-1001 P1, 6718-ACC-00-ZZ-DR-A-1003 P1, 6718-ACC-00-ZZ-DR-A-1004 P1 and shall thereafter be retained/maintained for as long as the development remains in existence.

REASON

To ensure the development complies with the provisions Hillingdon Local Plan Parts 1 (November 2012) and 2 (January 2020) and the London Plan (2021).

Informatives

1. I52 Compulsory Informative (1)

The decision to GRANT planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

2. I59 Councils Local Plan : Part 1 - Strategic Policies

On this decision notice policies from the Councils Local Plan: Part 1 - Strategic Policies appear first, then relevant Local Plan Part 2 (2020), then London Plan Policies (2021). Hillingdon's Full Council adopted the Hillingdon Local Plan: Part 1 - Strategic Policies on 8 November 2012 and the Hillingdon Local Plan Part 2 on 16 January 2020.

3.

The site lies in a Critical Drainage Area (CDA) identified in the Surface Water Management Plan (SWMP) for Hillingdon. Therefore the water from the site entering the sewers should be minimised:

- Water run off from any hard paving associated with the development should be directed to a soakaway, or tank or made permeable or be collected and directed to a permeable area, or

it would need an additional permission.

- A water butt should be incorporated.

- No drainage to support the development should be connected to any existing surface water network, other than as an overflow.

4.

Construction Techniques - It is recommended that the ground penetrating structure/s / extension / outbuilding / porch / building / electrical supply cables and panels is/are designed and constructed to prevent/minimise the possible entry of any migrating landfill gas/ground gas. Please contact your building surveyor and/or architect if you require advice concerning suitable construction techniques.

5.

There is a possibility there may be some contaminating substances in the ground at the site. Our contaminated land record shows the site to be on a former contaminated land use identified as yards (various) as well as infilled/possibly infilled site. We would advise persons working on site to take basic precautions in relation to any contamination they may find.

Appendix 2: Relevant Planning History

4501/AA/82/0999	Lbh Central Depot 128 Harlington Road Hillingdon Retention of permission (base unknown)(P)
Decision: 16-12-1982	Approve Deemed Hill.
4501/AB/82/1120	Lbh Central Depot 128 Harlington Road Hillingdon Commercial dev. - 306sq.m. (Full)(P)
Decision: 01-09-1982	Approve Deemed Hill.
4501/AC/82/1154	Lbh Central Depot 128 Harlington Road Hillingdon Change of use
Decision: 08-09-1982	Approve Deemed Hill.
4501/AD/83/0305	Lbh Central Depot 128 Harlington Road Hillingdon Erection of canteen extension and resiting of double garage (Section 53 determination).
Decision: 28-04-1983	Approve Deemed Hill.
4501/AE/83/0953	Lbh Central Depot 128 Harlington Road Hillingdon Extension/Alterations to Commercial premises (P) of 30 sq.m.
Decision: 08-09-1983	Approve Deemed Hill.
4501/AG/85/1386	Lbh Central Depot 128 Harlington Road Hillingdon Extension/Alterations to Industrial premises (P) of 150 sq.m.
Decision: 30-09-1985	Approve Deemed Hill.
4501/AH/85/1478	Lbh Central Depot 128 Harlington Road Hillingdon Office development - 200sq.m. (Full)(P)
Decision: 01-11-1985	Approve

Delegated -

Deemed Hill.

4501/AN/89/0649 Lbh Central Depot 128 Harlington Road Hillingdon
Retention of a temporary building for use as office purposes for a 15 week period.

Decision: 07-06-1989 Approve Limited
Time

4501/APP/2000/919 Harlington Road Depot Harlington Road Hillingdon
ERECTION OF TWO STOREY TERRAPIN OFFICES (INVOLVING DEMOLITION OF
EXISTING BLOCK)

Decision: 30-10-2001 No Further
Action(P)

4501/APP/2009/535 Harlington Road Depot Harlington Road Hillingdon
Erection of 139 residential units, consisting of a mix of houses and apartments with
associated car parking and amenity space (involving demolition of existing buildings)
(Outline application).

Decision: 08-11-2013 Withdrawn

4501/APP/2021/3310 Lbh Central Depot 128 Harlington Road Hillingdon
Demolition of an existing lorry diesel pump station and installation of a new diesel pump
station and canopy with photovoltaic cell and SUDS/rain gardens to periphery and all
associated road works

Decision: 24-12-2021 Approved

4501/AZ/95/1209 Harlington Road Depot Harlington Road Hillingdon
Erection of a two storey office building

Decision: 20-09-1995 Approve
Deemed Hill.

4501/BB/95/1931 Harlington Road Depot Harlington Road Hillingdon
Installation of two lighting columns

Decision: 10-07-1996 No Further
Action(P)

4501/BC/98/3033 Lbh Central Depot 128 Harlington Road Hillingdon
Erection of an 8 metre high flagpole

Decision: 21-05-1998 Approve
Deemed Hill.

4501/N/77/1342 Lbh Central Depot 128 Harlington Road Hillingdon
Office development - 111sq.m. (Full)(P)

Decision: 02-11-1977 Approve
Deemed Hill.

4501/P/77/1785 Lbh Central Depot 128 Harlington Road Hillingdon
Extension/Alterations to Commercial premises (P) of 18 sq.m.

Decision: 09-02-1979 Approve
Deemed Hill.

4501/R/78/2137 Lbh Central Depot 128 Harlington Road Hillingdon
Alterations to elevation (P)

Decision: 15-10-1979 Approve
Deemed Hill.

4501/S/79/1046 Lbh Central Depot 128 Harlington Road Hillingdon
Change of use

Decision: 27-12-1979 Approve
Deemed Hill.

4501/X/81/0848 Lbh Central Depot 128 Harlington Road Hillingdon
Alterations to elevation (P)

Decision: 12-11-1981 Approve
Deemed Hill.

4501/Y/81/0849 Lbh Central Depot 128 Harlington Road Hillingdon
Alterations to elevation (P)

Decision: 10-11-1981 Approve
Deemed Hill.

Appendix 3: List of Relevant Planning Policies

The following Local Plan Policies are considered relevant to the application:-

Part 1 Policies:

- PT1.BE1 (2012) Built Environment
- PT1.EM6 (2012) Flood Risk Management

Part 2 Policies:

- DMCI 2 New Community Infrastructure
- DMEI 11 Protection of Ground Water Resources
- DMEI 12 Development of Land Affected by Contamination
- DMEI 14 Air Quality
- DMEI 2 Reducing Carbon Emissions
- DMEI 9 Management of Flood Risk
- DMHB 11 Design of New Development
- DMT 2 Highways Impacts

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Planning Committee Report Part 3:

Planning Policy Appendix - Frequently Cited Policies



Frequently Cited Planning Policies

Abbreviations

LP – London Plan (2021)

LP1 – Hillingdon Local Plan Part 1: Strategic Policies (2012)

LP2 – Hillingdon Local Plan Part 2: Development Management Policies (2020)

Topic	Policy	Page No.
Householder Policies	LP2 DMHD 1: Alterations and Extensions to Residential Dwellings	4
	LP2 DMHD 2: Outbuildings	6
	LP2 DMHD 3: Basement Development	6
Standard of Accommodation	LP D6: Housing quality and standards	7
	LP2 DMHB 16: Housing Standards	8
	LP2 DMHB 18: Private Outdoor Amenity Space	8
Housing	LP H2: Small sites	9
	LP H4: Delivering Affordable Housing?	9
	LP H10: Housing size mix	10
	LP1 H1: Housing Growth	10
	LP2 DMH 1: Safeguarding Existing Housing	11
	LP2 DMH 2: Housing Mix	11
	LP2 DMH 4: Residential Conversions and Redevelopment	11
	LP2 DMH 5: Houses in Multiple Occupation	11
	LP2 DMH 6: Garden and Backland Development	12
	LP2 DMH 7: Provision of Affordable Housing	12
Design (Including Heritage, Trees / Landscaping and Accessibility)	LP D3: Optimising site capacity through the design-led approach	13
	LP D5: Inclusive design	15
	LP D7: Accessible housing	15
	LP D8: Public realm	15
	LP D12: Fire safety	17
	LP HC1: Heritage conservation and growth	18
	LP G7: Trees and woodlands	19
	LP1 BE1: Built Environment	19
	LP2 DMHB 1: Heritage Assets	21

	LP2 DMHB 2: Listed Buildings	22
	LP2 DMHB 3: Locally Listed Buildings	22
	LP2 DMHB 4: Conservation Areas	23
	LP2 DMHB 5: Areas of Special Local Character	23
	LP2 DMHB 11: Design of New Development	23
	LP2 DMHB 12: Streets and Public Realm	24
	LP2 DMHB 14: Trees and Landscaping	25
	LP2 DMHB 15: Planning for Safer Places	25
Environmental	LP D13: Agent of change	25
	LP D14: Noise	26
	LP SI2: Minimising greenhouse gas emissions	27
	LP SI12: Flood risk management	27
	LP SI13: Sustainable drainage	28
	LP1 EM6: Flood Risk Management	29
	LP1 EM8: Land, Water, Air and Noise	29
	LP2 DMEI 2: Reducing Carbon Emissions	31
	LP2 DMEI 9: Management of Flood Risk	31
	LP2 DMEI 10: Water Management, Efficiency and Quality	31
	LP2 DMEI 12: Development of Land Affected by Contamination	33
	LP2 DMEI 14: Air Quality	33
Highways and Parking	LP T4: Assessing and mitigating transport impacts	33
	LP T5: Cycling	34
	LP T6: Car parking	35
	LP T6.1: Residential parking	36
	LP2 DMT 1: Managing Transport Impacts	37
	LP2 DMT 2: Highways Impacts	38
	LP2 DMT 5: Pedestrians and Cyclists	38
	LP2 DMT 6: Vehicle Parking	39

LP2 DMHD 1: Alterations and Extensions to Residential Dwellings

A) Planning applications relating to alterations and extensions of dwellings will be required to ensure that:

- i) there is no adverse cumulative impact of the proposal on the character, appearance or quality of the existing street or wider area;
- ii) a satisfactory relationship with adjacent dwellings is achieved;
- iii) new extensions appear subordinate to the main dwelling in their floor area, width, depth and height;
- iv) new extensions respect the design of the original house and be of matching materials;
- v) there is no unacceptable loss of outlook to neighbouring occupiers;
- vi) adequate garden space is retained;
- vii) adequate off-street parking is retained, as set out in Table 1: Parking Standards in Appendix C;
- viii) trees, hedges and other landscaping features are retained; and
- ix) all extensions in Conservation Areas and Areas of Special Local Character, and to Listed and Locally Listed Buildings, are designed in keeping with the original house, in terms of layout, scale, proportions, roof form, window pattern, detailed design and materials.

B) Rear Extensions

- i) single storey rear extensions on terraced or semi-detached houses with a plot width of 5 metres or less should not exceed 3.3 metres in depth or 3.6 metres where the plot width is 5 metres or more;
- ii) single storey rear extensions to detached houses with a plot width of 5 metres or more should not exceed 4.0 metres in depth;
- iii) flat roofed single storey extensions should not exceed 3.0 metres in height and any pitched or sloping roofs should not exceed 3.4 metres in height, measured from ground level;
- iv) in Conservation Areas and Areas of Special Local Character, flat roofed single storey extensions will be expected to be finished with a parapet;
- v) balconies or access to flat roofs which result in loss of privacy to nearby dwellings or gardens will not be permitted;
- vi) two storey extensions should not extend into an area provided by a 45-degree line of sight drawn from the centre of the nearest ground or first floor habitable room window of an adjacent property and should not contain windows or other openings that overlook other houses at a distance of less than 21 metres;
- vii) flat roofed two storey extensions will not be acceptable unless the design is in keeping with the particular character of the existing house;
- viii) pitched roofs on extensions should be of a similar pitch and materials to that of the original roof and subordinate to it in design. Large crown roofs on detached houses will not be supported; and
- ix) full width two storey rear extensions are not considered acceptable in designated areas or as extensions to Listed Buildings or Locally Listed Buildings.

C) Side Extensions

- i) side extensions should not exceed half the width of the original property;
- ii) extensions to corner plots should ensure that the openness of the area is maintained and the return building line is not exceeded;

- iii) garages should reflect the size guidelines set out in Appendix C Parking standards;
- iv) two storey side extensions should be set in a minimum of 1 metre from the side boundary or in the case of properties in the Copse Wood and Gatehill Estates, at least 1.5 metres, but more if on a wider than average plot, in order to maintain adequate visual separation and views between houses;
- v) two storey side extensions to detached and semi-detached properties should be set back a minimum of 1 metre behind the main front elevation;
- vi) where hip to gable roof extensions exist, a two storey side extension will not be supported; and
- vii) in Conservation Areas, single storey side extensions may be required to be set back.

D) Front Extensions

- i) alterations and extensions to the front of a house must be minor and not alter the overall appearance of the house or dominate the character of the street. Front extensions extending across the entire frontage will be refused;
- ii) porches should be subordinate in scale and individually designed to respect the character and features of the original building; pastiche features will not be supported; and
- iii) notwithstanding the above, at least 25% of the front garden must be retained.

E) Roof Extensions

- i) roof extensions should be located on the rear elevation only, be subservient to the scale of the existing roof and should not exceed more than two thirds the average width of the original roof. They should be located below the ridge tiles of the existing roof and retain a substantial element of the original roof slope above the eaves line;
- ii) the Council will not support poorly designed or over-large roof extensions including proposals to convert an existing hipped roof to a gable;
- iii) raising of a main roof above the existing ridgeline of a house will generally not be supported;
- iv) all roof extensions should employ appropriate external materials and architectural details to match the existing dwelling; and
- v) in Conservation Areas, Areas of Special Local Character and on Listed and Locally Listed Buildings, roof extensions should take the form of traditional 'dormer' windows, on the rear elevation, to harmonise with the existing building. The highest point of the dormer should be kept well within the back roof slope, away from the ridge, eaves or valleys, whilst each window should match the proportions, size and glazing pattern of the first floor windows.

F) Front Gardens and Parking

- i) new or replacement driveways should use permeable (porous) surfacing. Surfaces of more than five square metres will need planning permission for laying traditional, impermeable driveways; and
- ii) the design, materials and height of any front boundary must be in keeping with the character of the area to ensure harmonisation with the existing street scene.

LP2 DMHD 2: Outbuildings

The Council will require residential outbuildings to meet the following criteria:

- i) the building must be constructed to a high standard of design without compromising the amenity of neighbouring occupiers;
- ii) the developed footprint of the proposed building must be proportionate to the footprint of the dwelling house and to the residential curtilage in which it stands and have regard to existing trees;
- iii) the use shall be for a purpose incidental to the enjoyment of the dwelling house and not capable for use as independent residential accommodation; and
- iv) primary living accommodation such as a bedroom, bathroom, or kitchen will not be permitted.

LP2 DMHD 3: Basement Development

A) When determining proposals for basement and other underground development, the Council require an assessment of the scheme's impact on drainage, flooding, groundwater conditions and structural stability. The Council will only permit basement and other underground development that does not cause harm to the built and natural environment and local amenity and does not result in flooding or ground instability. Developers will be required to demonstrate by methodologies appropriate to the site that their proposals:

- i) avoid adversely affecting drainage and run-off or causing other damage to the water environment;
- ii) avoid cumulative impacts upon structural stability or the water environment in the local area;

B) Schemes should ensure that they:

- i) do not harm the amenity of neighbours;
- ii) do not lead to the loss of trees of townscape or amenity value;
- iii) do provide satisfactory landscaping, including adequate soil depth;
- iv) do not harm the appearance or setting of the property or the established character of the surrounding area, for example through the introduction of front lightwells; and
- v) do protect important archaeological remains.

C) The Council will not permit basement schemes which include habitable rooms and other sensitive uses in areas prone to flooding.

D) The Council will not permit basement schemes in Listed Buildings and will not permit them in Conservation Area locations where their introduction would harm the special architectural or historic character of the area.

LP D6: Housing Quality and Standards

- A) Housing development should be of high quality design and provide adequately-sized rooms (see Table 3.1) with comfortable and functional layouts which are fit for purpose and meet the needs of Londoners without differentiating between tenures.
- B) Qualitative aspects of a development are key to ensuring successful sustainable housing. Table 3.2 sets out key qualitative aspects which should be addressed in the design of housing developments.
- C) Housing development should maximise the provision of dual aspect dwellings and normally avoid the provision of single aspect dwellings. A single aspect dwelling should only be provided where it is considered a more appropriate design solution to meet the requirements of Part D in Policy D3 Optimising site capacity through the design-led approach than a dual aspect dwelling, and it can be demonstrated that it will have adequate passive ventilation, daylight and privacy, and avoid overheating.
- D) The design of development should provide sufficient daylight and sunlight to new and surrounding housing that is appropriate for its context, whilst avoiding overheating, minimising overshadowing and maximising the usability of outside amenity space.
- E) Housing should be designed with adequate and easily accessible storage space that supports the separate collection of dry recyclables (for at least card, paper, mixed plastics, metals, glass) and food waste as well as residual waste
- F) Housing developments are required to meet the minimum standards below which apply to all tenures and all residential accommodation that is self-contained.

Private internal space

1. Dwellings must provide at least the gross internal floor area and built-in storage area set out in Table 3.1.
2. A dwelling with two or more bedspaces must have at least one double (or twin) bedroom that is at least 2.75m wide. Every other additional double (or twin) bedroom must be at least 2.55m wide.
3. A one bedspace single bedroom must have a floor area of at least 7.5 sq.m. and be at least 2.15m wide.
4. A two bedspace double (or twin) bedroom must have a floor area of at least 11.5 sq.m.
5. Any area with a headroom of less than 1.5m is not counted within the Gross Internal Area unless used solely for storage (If the area under the stairs is to be used for storage, assume a general floor area of 1 sq.m. within the Gross Internal Area).
6. Any other area that is used solely for storage and has a headroom of 0.9-1.5m (such as under eaves) can only be counted up to 50 per cent of its floor area, and any area lower than 0.9m is not counted at all.
7. A built-in wardrobe counts towards the Gross Internal Area and bedroom floor area requirements, but should not reduce the effective width of the room below the minimum widths set out above. Any built-in area in excess of 0.72 sq.m. in a double

bedroom and 0.36 sq.m. in a single bedroom counts towards the built-in storage requirement.

8. The minimum floor to ceiling height must be 2.5m for at least 75 per cent of the Gross Internal Area of each dwelling.

Private outside space

9. Where there are no higher local standards in the borough Development Plan Documents, a minimum of 5 sq.m. of private outdoor space should be provided for 1-2 person dwellings and an extra 1 sq.m. should be provided for each additional occupant, and it must achieve a minimum depth and width of 1.5m. This does not count towards the minimum Gross Internal Area space standards required in Table 3.1

G) The Mayor will produce guidance on the implementation of this policy for all housing tenures.

LP2 DMHB 16: Housing Standards

All housing development should have an adequate provision of internal space in order to provide an appropriate living environment. To achieve this all residential development or conversions should:

- i) meet or exceed the most up to date internal space standards, as set out in Table 5.1; and
- ii) in the case of major developments, provide at least 10% of new housing to be accessible or easily adaptable for wheelchair users.

LP2 DMHB 18: Private Outdoor Amenity Space

A) All new residential development and conversions will be required to provide good quality and useable private outdoor amenity space. Amenity space should be provided in accordance with the standards set out in Table 5.3.

B) Balconies should have a depth of not less than 1.5 metres and a width of not less than 2 metres.

C) Any ground floor and/or basement floor unit that is non-street facing should have a defensible space of not less than 3 metres in depth in front of any window to a bedroom or habitable room. However, for new developments in Conservation Areas, Areas of Special Local Character or for developments, which include Listed Buildings, the provision of private open space will be required to enhance the streetscene and the character of the buildings on the site.

D) The design, materials and height of any front boundary must be in keeping with the character of the area to ensure harmonisation with the existing street scene.

LP H2: Small sites

A) Boroughs should pro-actively support well-designed new homes on small sites (below 0.25 hectares in size) through both planning decisions and plan-making in order to:

1. significantly increase the contribution of small sites to meeting London's housing needs
2. diversify the sources, locations, type and mix of housing supply
3. support small and medium-sized housebuilders
4. support those wishing to bring forward custom, self-build and community-led housing
5. achieve the minimum targets for small sites set out in Table 4.2 as a component of the overall housing targets set out in Table 4.1.

B Boroughs should:

1. recognise in their Development Plans that local character evolves over time and will need to change in appropriate locations to accommodate additional housing on small sites
2. where appropriate, prepare site-specific briefs, masterplans and housing design codes for small sites
3. identify and allocate appropriate small sites for residential development
4. list these small sites on their brownfield registers
5. grant permission in principle on specific sites or prepare local development orders.

LP H4: Delivering Affordable Housing

A) The strategic target is for 50 per cent of all new homes delivered across London to be genuinely affordable. Specific measures to achieve this aim include:

1. requiring major developments which trigger affordable housing requirements to provide affordable housing through the threshold approach (Policy H5 Threshold approach to applications)
2. using grant to increase affordable housing delivery beyond the level that would otherwise be provided
3. all affordable housing providers with agreements with the Mayor delivering at least 50 per cent affordable housing across their development programme, and 60 per cent in the case of strategic partners
4. public sector land delivering at least 50 per cent affordable housing on each site and public sector landowners with agreements with the Mayor delivering at least 50 per cent affordable housing across their portfolio
5. industrial land appropriate for residential use in accordance with Policy E7 Industrial intensification, co-location and substitution, delivering at least 50 per cent affordable housing where the scheme would result in a net loss of industrial capacity.

B) Affordable housing should be provided on site. Affordable housing must only be provided off-site or as a cash in lieu contribution in exceptional circumstances.

LP H10: Housing size mix

A) Schemes should generally consist of a range of unit sizes. To determine the appropriate mix of unit sizes in relation to the number of bedrooms for a scheme, applicants and decision-makers should have regard to:

1. robust local evidence of need where available or, where this is not available, the range of housing need and demand identified by the 2017 London Strategic Housing Market Assessment
2. the requirement to deliver mixed and inclusive neighbourhoods
3. the need to deliver a range of unit types at different price points across London
4. the mix of uses in the scheme
5. the range of tenures in the scheme
6. the nature and location of the site, with a higher proportion of one and two bed units generally more appropriate in locations which are closer to a town centre or station or with higher public transport access and connectivity
7. the aim to optimise housing potential on sites
8. the ability of new development to reduce pressure on conversion, sub-division and amalgamation of existing stock
9. the need for additional family housing and the role of one and two bed units in freeing up existing family housing.

B For low-cost rent, boroughs should provide guidance on the size of units required (by number of bedrooms) to ensure affordable housing meets identified needs. This guidance should take account of:

1. evidence of local housing needs, including the local housing register and the numbers and types of overcrowded and under-occupying households
2. other criteria set out in Part A, including the strategic and local requirement for affordable family accommodation
3. the impact of welfare reform
4. the cost of delivering larger units and the availability of grant.

LP1 H1: Housing Growth

The Council will meet and exceed its minimum strategic dwelling requirement, where this can be achieved, in accordance with other Local Plan policies.

The borough's current target is to provide an additional 4,250 dwellings, annualised as 425 dwellings per year, for the ten year period between 2011 and 2021.

Rolled forward to 2026, this target equates to a minimum provision of 6,375 dwellings over the period of the Hillingdon Local Plan: Part 1- Strategic Policies. Sites that will contribute to the achievement of this target will be identified in the Hillingdon Local Plan: Part 2- Site Specific Allocations Local Development Document (LDD).

LP2 DMH 1: Safeguarding Existing Housing

A) The net loss of existing self-contained³ housing, including affordable housing, will be resisted unless the housing is replaced with at least equivalent residential floorspace.

B) The Council will grant planning permission for the subdivision of dwellings only if:

- i) car parking standards can be met within the curtilage of the site without being detrimental to the street scene;
- ii) all units are self contained with exclusive use of sanitary and kitchen facilities and provided with individual entrances and internal staircases to serve units above ground floor level;
- iii) adequate amenity space is provided for the benefit of residents; and
- iv) adequate living space standards are met.

LP2 DMH 2: Housing Mix

The Council will require the provision of a mix of housing units of different sizes in schemes of residential development to reflect the Council's latest information on housing need.

LP2 DMH 4: Residential Conversions and Redevelopment

Residential conversions and the redevelopment of dwellings into new blocks of flats will only be permitted where:

- i) it is on a residential street where the proposal will not result in more than 10% of properties being redeveloped into flats;
- ii) On residential streets longer than 1km the proposed redevelopment site should be taken as the midpoint of a 1km length of road for assessment purposes;
- iii) the internal floor area of the original building to be converted is at least 120 sqm; and
- iv) units are limited to one unit per floor for residential conversions.

LP2 DMH 5: Houses in Multiple Occupation (HMOs) and Student Accommodation

A) In all parts of the Borough

Proposals for the provision of large HMOs, residential hostels, student accommodation and secure accommodation will be required to demonstrate that:

- i) there is good accessibility to local amenities and public transport;
- ii) they accord with the Accessible Homes standards and provide satisfactory living conditions for the intended occupiers; and
- iii) there will be no adverse impact on the amenity of neighbouring properties or the character of the area.

B) In wards covered by an Article 4 Direction for HMOs

Planning applications for the change of use from dwelling house (Use Class C3) to HMO (Use Class C4 and Sui Generis) will only be permitted:

- i) where it is in a neighbourhood area where less than 20% of properties are or would be exempt from paying council tax (or in the case of Conservation Areas 10%) because they are entirely occupied by full time students, recorded on the Council's database as a licensed HMO, benefit from C4/Sui Generis HMO planning consent and are known to the Council to be HMOs;
- ii) in Conservation Areas where less than 10% of properties are exempt from paying council tax because they are entirely occupied by full time students, recorded on the Council's database as a licensed HMO, benefit from C4/Sui Generis HMO planning consent and are known to the Council to be HMOs and the change of use does not form a consecutive HMO use in a street frontage;
- iii) where less than 15% of properties within 100 metres of a street length either side of an application property are exempt from paying council tax because they are entirely occupied by full time students, recorded on the Council's database as a licensed HMO, benefit from C4/Sui Generis HMO planning consent and are known to the Council to be HMOs; and iv) where the accommodation complies with all other planning standards relating to car parking, waste storage, retention of amenity space and garages and will not have a detrimental impact upon the residential amenity of adjoining properties.

LP2 DMH 6: Garden and Backland Development

There is a presumption against the loss of gardens due to the need to maintain local character, amenity space and biodiversity. In exceptional cases a limited scale of backland development may be acceptable, subject to the following criteria:

- i) neighbouring residential amenity and privacy of existing homes and gardens must be maintained and unacceptable light spillage avoided;
- ii) vehicular access or car parking should not have an adverse impact on neighbours in terms of noise or light. Access roads between dwellings and unnecessarily long access roads will not normally be acceptable;
- iii) development on backland sites must be more intimate in mass and scale and lower than frontage properties; and iv) features such as trees, shrubs and wildlife habitat must be retained or re-provided.

LP2 DMH 7: Provision of Affordable Housing

A) In accordance with national policy:

- i) developments with a capacity to provide 10 or more units will be required to maximise the delivery of on-site affordable housing;
- ii) subject to viability and if appropriate in all circumstances, a minimum of 35% of all new homes on sites of 10 or more units should be delivered as affordable housing, with the tenure split 70% Social/Affordable Rent and 30% Intermediate as set out in Policy H2: Affordable Housing of the Local Plan Part 1.

B) Affordable housing should be built to the same standards and should share the same level of amenity as private housing.

C) Proposals that do not provide sufficient affordable housing will be resisted.

D) To ensure that Policy H2: Affordable Housing of the Local Plan Part 1 is applied consistently and fairly on all proposed housing developments, the requirement for affordable housing will apply to:

i) sites that are artificially sub-divided or partially developed;

ii) phased developments where a housing development is part of a much larger development of 10 or more units (gross), affordable housing will be required as part of the overall scheme; and iii) additional units created through or subsequently amended planning applications, whereby the amount of affordable housing required will be calculated based on the new total number of units on the site. Affordable housing will be required where a development under the 10 unit threshold is amended to have 10 or more housing units in total (gross).

E) In exceptional circumstances, where on-site provision of affordable housing cannot be delivered and as a last resort, a financial contribution will be required to provide off-site affordable housing on other sites which may be more appropriate or beneficial in meeting the Borough's identified affordable housing needs.

LP D3: Optimising site capacity through the design-led approach

The design-led approach

A) All development must make the best use of land by following a design-led approach that optimises the capacity of sites, including site allocations. Optimising site capacity means ensuring that development is of the most appropriate form and land use for the site. The design-led approach requires consideration of design options to determine the most appropriate form of development that responds to a site's context and capacity for growth, and existing and planned supporting infrastructure capacity (as set out in Policy D2 Infrastructure requirements for sustainable densities), and that best delivers the requirements set out in Part D.

B) Higher density developments should generally be promoted in locations that are well connected to jobs, services, infrastructure and amenities by public transport, walking and cycling, in accordance with Policy D2 Infrastructure requirements for sustainable densities. Where these locations have existing areas of high density buildings, expansion of the areas should be positively considered by Boroughs where appropriate. This could also include expanding Opportunity Area boundaries where appropriate.

C) In other areas, incremental densification should be actively encouraged by Boroughs to achieve a change in densities in the most appropriate way. This should be interpreted in the context of Policy H2 Small sites.

D) Development proposals should:

Form and layout

1. enhance local context by delivering buildings and spaces that positively respond to local distinctiveness through their layout, orientation, scale, appearance and shape, with due regard to existing and emerging street hierarchy, building types, forms and proportions
2. encourage and facilitate active travel with convenient and inclusive pedestrian and cycling routes, crossing points, cycle parking, and legible entrances to buildings, that are aligned with peoples' movement patterns and desire lines in the area
3. be street-based with clearly defined public and private environments
4. facilitate efficient servicing and maintenance of buildings and the public realm, as well as deliveries, that minimise negative impacts on the environment, public realm and vulnerable road users

Experience

1. achieve safe, secure and inclusive environments
2. provide active frontages and positive reciprocal relationships between what happens inside the buildings and outside in the public realm to generate liveliness and interest
3. deliver appropriate outlook, privacy and amenity
4. provide conveniently located green and open spaces for social interaction, play, relaxation and physical activity
5. help prevent or mitigate the impacts of noise and poor air quality
6. achieve indoor and outdoor environments that are comfortable and inviting for people to use

Quality and character

1. respond to the existing character of a place by identifying the special and valued features and characteristics that are unique to the locality and respect, enhance and utilise the heritage assets and architectural features that contribute towards the local character
2. be of high quality, with architecture that pays attention to detail, and gives thorough consideration to the practicality of use, flexibility, safety and building lifespan through appropriate construction methods and the use of attractive, robust materials which weather and mature well
3. aim for high sustainability standards (with reference to the policies within London Plan Chapters 8 and 9) and take into account the principles of the circular economy
4. provide spaces and buildings that maximise opportunities for urban greening to create attractive resilient places that can also help the management of surface water.

E) Where development parameters for allocated sites have been set out in a Development Plan, development proposals that do not accord with the site capacity in a site allocation can be refused for this reason.

LP D5: Inclusive Design

A) Boroughs, in preparing their Development Plans, should support the creation of inclusive neighbourhoods by embedding inclusive design, and collaborating with local communities in the development of planning policies that affect them.

B) Development proposal should achieve the highest standards of accessible and inclusive design. They should:

1. be designed taking into account London's diverse population
2. provide high quality people focused spaces that are designed to facilitate social interaction and inclusion
3. be convenient and welcoming with no disabling barriers, providing independent access without additional undue effort, separation or special treatment
4. be able to be entered, used and exited safely, easily and with dignity for all
5. be designed to incorporate safe and dignified emergency evacuation for all building users. In all developments where lifts are installed, as a minimum at least one lift per core (or more subject to capacity assessments) should be a suitably sized fire evacuation lift suitable to be used to evacuate people who require level access from the building.

C) Design and Access Statements, submitted as part of development proposals, should include an inclusive design statement.

LP D7: Accessible Housing

A) To provide suitable housing and genuine choice for London's diverse population, including disabled people, older people and families with young children, residential development must ensure that:

1. at least 10 per cent of dwellings (which are created via works to which Part M volume 1 of the Building Regulations applies) meet Building Regulation requirement M4(3) 'wheelchair user dwellings'
2. all other dwellings (which are created via works to which Part M volume 1 of the Building Regulations applies) meet Building Regulation requirement M4(2) 'accessible and adaptable dwellings'.

LP D8: Public Realm

Development Plans and development proposals should:

A) encourage and explore opportunities to create new public realm where appropriate

B) ensure the public realm is well-designed, safe, accessible, inclusive, attractive, well-connected, related to the local and historic context, and easy to understand, service and maintain. Landscape treatment, planting, street furniture and surface materials should be of good quality, fit-for-purpose, durable and sustainable. Lighting, including for advertisements, should be carefully considered and well-

designed in order to minimise intrusive lighting infrastructure and reduce light pollution

C) maximise the contribution that the public realm makes to encourage active travel and ensure its design discourages travel by car and excessive on-street parking, which can obstruct people's safe enjoyment of the space. This includes design that reduces the impact of traffic noise and encourages appropriate vehicle speeds

D) be based on an understanding of how the public realm in an area functions and creates a sense of place during different times of the day and night, days of the week and times of the year. In particular, they should demonstrate an understanding of how people use the public realm, and the types, location and relationship between public spaces in an area, identifying where there are deficits for certain activities, or barriers to movement that create severance for pedestrians and cyclists

E) ensure both the movement function of the public realm and its function as a place are provided for and that the balance of space and time given to each reflects the individual characteristics of the area. The priority modes of travel for the area should be identified and catered for, as appropriate. Desire lines for people walking and cycling should be a particular focus, including the placement of street crossings, which should be regular, convenient and accessible

F) ensure there is a mutually supportive relationship between the space, surrounding buildings and their uses, so that the public realm enhances the amenity and function of buildings and the design of buildings contributes to a vibrant public realm

G) ensure buildings are of a design that activates and defines the public realm, and provides natural surveillance. Consideration should also be given to the local microclimate created by buildings, and the impact of service entrances and facades on the public realm

H) ensure appropriate management and maintenance arrangements are in place for the public realm, which maximise public access and minimise rules governing the space to those required for its safe management in accordance with the Public London Charter

I) incorporate green infrastructure such as street trees and other vegetation into the public realm to support rainwater management through sustainable drainage, reduce exposure to air pollution, moderate surface and air temperature and increase biodiversity

J) ensure that appropriate shade, shelter, seating and, where possible, areas of direct sunlight are provided, with other microclimatic considerations, including temperature and wind, taken into account in order to encourage people to spend time in a place

K) ensure that street clutter, including street furniture that is poorly located, unsightly, in poor condition or without a clear function is removed, to ensure that pedestrian amenity is improved. Consideration should be given to the use, design and location of street furniture so that it complements the use and function of the space. Applications which seek to introduce unnecessary street furniture should be refused

L) explore opportunities for innovative approaches to improving the public realm such as open street events and Play Streets

M) create an engaging public realm for people of all ages, with opportunities for social activities, formal and informal play and social interaction during the daytime, evening and at night. This should include identifying opportunities for the meanwhile use of sites in early phases of development to create temporary public realm

N) ensure that any on-street parking is designed so that it is not dominant or continuous, and that there is space for green infrastructure as well as cycle parking in the carriageway. Parking should not obstruct pedestrian lines

O) ensure the provision and future management of free drinking water at appropriate locations in the new or redeveloped public realm.

LP D12: Fire Safety

A) In the interests of fire safety and to ensure the safety of all building users, all development proposals must achieve the highest standards of fire safety and ensure that they:

1. identify suitably positioned unobstructed outside space:
 - a - for fire appliances to be positioned on
 - b - appropriate for use as an evacuation assembly point
2. are designed to incorporate appropriate features which reduce the risk to life and the risk of serious injury in the event of a fire; including appropriate fire alarm systems and passive and active fire safety measures
3. are constructed in an appropriate way to minimise the risk of fire spread
4. provide suitable and convenient means of escape, and associated evacuation strategy for all building users
5. develop a robust strategy for evacuation which can be periodically updated and published, and which all building users can have confidence in
6. provide suitable access and equipment for firefighting which is appropriate for the size and use of the development.

B) All major development proposals should be submitted with a Fire Statement, which is an independent fire strategy, produced by a third party, suitably qualified assessor.

The statement should detail how the development proposal will function in terms of:

1. the building's construction: methods, products and materials used, including manufacturers' details

2. the means of escape for all building users: suitably designed stair cores, escape for building users who are disabled or require level access, and associated evacuation strategy approach
3. features which reduce the risk to life: fire alarm systems, passive and active fire safety measures and associated management and maintenance plans
4. access for fire service personnel and equipment: how this will be achieved in an evacuation situation, water supplies, provision and positioning of equipment, firefighting lifts, stairs and lobbies, any fire suppression and smoke ventilation systems proposed, and the ongoing maintenance and monitoring of these
5. how provision will be made within the curtilage of the site to enable fire appliances to gain access to the building
6. ensuring that any potential future modifications to the building will take into account and not compromise the base build fire safety/protection measures.

LP HC1: Heritage Conservation and Growth

A) Boroughs should, in consultation with Historic England, local communities and other statutory and relevant organisations, develop evidence that demonstrates a clear understanding of London's historic environment. This evidence should be used for identifying, understanding, conserving, and enhancing the historic environment and heritage assets, and improving access to, and interpretation of, the heritage assets, landscapes and archaeology within their area.

B) Development Plans and strategies should demonstrate a clear understanding of the historic environment and the heritage values of sites or areas and their relationship with their surroundings. This knowledge should be used to inform the effective integration of London's heritage in regenerative change by:

1. setting out a clear vision that recognises and embeds the role of heritage in place-making
2. utilising the heritage significance of a site or area in the planning and design process
3. integrating the conservation and enhancement of heritage assets and their settings with innovative and creative contextual architectural responses that contribute to their significance and sense of place
4. delivering positive benefits that conserve and enhance the historic environment, as well as contributing to the economic viability, accessibility and environmental quality of a place, and to social wellbeing.

C) Development proposals affecting heritage assets, and their settings, should conserve their significance, by being sympathetic to the assets' significance and appreciation within their surroundings. The cumulative impacts of incremental change from development on heritage assets and their settings should also be actively managed. Development proposals should avoid harm and identify enhancement opportunities by integrating heritage considerations early on in the design process.

D) Development proposals should identify assets of archaeological significance and use this information to avoid harm or minimise it through design and appropriate mitigation. Where applicable, development should make provision for the protection of significant archaeological assets and landscapes. The protection of undesignated heritage assets of archaeological interest equivalent to a scheduled monument should be given equivalent weight to designated heritage assets.

E) Where heritage assets have been identified as being At Risk, boroughs should identify specific opportunities for them to contribute to regeneration and place-making, and they should set out strategies for their repair and re-use.

LP G7: Trees and Woodlands

A) London's urban forest and woodlands should be protected and maintained, and new trees and woodlands should be planted in appropriate locations in order to increase the extent of London's urban forest – the area of London under the canopy of trees.

B) In their Development Plans, boroughs should:

1. protect 'veteran' trees and ancient woodland where these are not already part of a protected site¹³⁹
2. identify opportunities for tree planting in strategic locations.

C) Development proposals should ensure that, wherever possible, existing trees of value are retained.¹⁴⁰ If planning permission is granted that necessitates the removal of trees there should be adequate replacement based on the existing value of the benefits of the trees removed, determined by, for example, i-tree or CAVAT or another appropriate valuation system. The planting of additional trees should generally be included in new developments – particularly large-canopied species which provide a wider range of benefits because of the larger surface area of their canopy.

LP 1 BE1: Built Environment

The Council will require all new development to improve and maintain the quality of the built environment in order to create successful and sustainable neighbourhoods, where people enjoy living and working and that serve the long-term needs of all residents. All new developments should:

1. Achieve a high quality of design in all new buildings, alterations, extensions and the public realm which enhances the local distinctiveness of the area, contributes to community cohesion and a sense of place;
2. Be designed to be appropriate to the identity and context of Hillingdon's buildings, townscapes, landscapes and views, and make a positive contribution to the local

area in terms of layout, form, scale and materials and seek to protect the amenity of surrounding land and buildings, particularly residential properties;

3. Be designed to include “Lifetime Homes” principles so that they can be readily adapted to meet the needs of those with disabilities and the elderly, 10% of these should be wheelchair accessible or easily adaptable to wheelchair accessibility encouraging places of work and leisure, streets, neighbourhoods, parks and open spaces to be designed to meet the needs of the community at all stages of people’s lives;

4. In the case of 10 dwellings or over, achieve a satisfactory assessment rating in terms of the latest Building for Life standards (as amended or replaced from time to time);

5. Improve areas of poorer environmental quality, including within the areas of relative disadvantage of Hayes, Yiewsley and West Drayton. All regeneration schemes should ensure that they are appropriate to their historic context, make use of heritage assets and reinforce their significance;

6. Incorporate a clear network of routes that are easy to understand, inclusive, safe, secure and connect positively with interchanges, public transport, community facilities and services;

7. Improve the quality of the public realm and provide for public and private spaces that are attractive, safe, functional, diverse, sustainable, accessible to all, respect the local character and landscape, integrate with the development, enhance and protect biodiversity through the inclusion of living walls, roofs and areas for wildlife, encourage physical activity and where appropriate introduce public art;

8. Create safe and secure environments that reduce crime and fear of crime, anti-social behaviour and risks from fire and arson having regard to Secure by Design standards and address resilience to terrorism in major development proposals;

9. Not result in the inappropriate development of gardens and green spaces that erode the character and biodiversity of suburban areas and increase the risk of flooding through the loss of permeable areas;

10. Maximise the opportunities for all new homes to contribute to tackling and adapting to climate change and reducing emissions of local air quality pollutants. The Council will require all new development to achieve reductions in carbon dioxide emission in line with the London Plan targets through energy efficient design and effective use of low and zero carbon technologies. Where the required reduction from on-site renewable energy is not feasible within major developments, contributions off-site will be sought. The Council will seek to merge a suite of sustainable design goals, such as the use of SUDS, water efficiency, lifetime homes, and energy efficiency into a requirement measured against the Code for Sustainable

Homes and BREEAM. These will be set out within the Hillingdon Local Plan: Part 2- Development Management Policies Local Development Document (LDD). All developments should be designed to make the most efficient use of natural resources whilst safeguarding historic assets, their settings and local amenity and include sustainable design and construction techniques to increase the re-use and recycling of construction, demolition and excavation waste and reduce the amount disposed to landfill;

11. In the case of tall buildings, not adversely affect their surroundings including the local character, cause harm to the significance of heritage assets or impact on important views. Appropriate locations for tall buildings will be defined on a Character Study and may include parts of Uxbridge and Hayes subject to considering the Obstacle Limitation Surfaces for Heathrow Airport. Outside of Uxbridge and Hayes town centres, tall buildings will not be supported. The height of all buildings should be based upon an understanding of the local character and be appropriate to the positive qualities of the surrounding townscape. Support will be given for proposals that are consistent with local strategies, guidelines, supplementary planning documents and Hillingdon Local Plan: Part 2- Development Management Policies.

LP2 DMHB 1: Heritage Assets

A) The Council will expect development proposals to avoid harm to the historic environment. Development that has an effect on heritage assets will only be supported where:

- i) it sustains and enhances the significance of the heritage asset and puts them into viable uses consistent with their conservation;
- ii) it will not lead to a loss of significance or harm to an asset, unless it can be demonstrated that it will provide public benefit that would outweigh the harm or loss, in accordance with the NPPF;
- iii) it makes a positive contribution to the local character and distinctiveness of the area;
- iv) any extensions or alterations are designed in sympathy, without detracting from or competing with the heritage asset;
- v) the proposal would relate appropriately in terms of siting, style, scale, massing, height, design and materials;
- vi) buildings and structures within the curtilage of a heritage asset, or in close proximity to it, do not compromise its setting; and
- vii) opportunities are taken to conserve or enhance the setting, so that the significance of the asset can be appreciated more readily.

B) Development proposals affecting designated heritage assets need to take account of the effects of climate change and renewable energy without impacting negatively on the heritage asset. The Council may require an alternative solution which will protect the asset yet meet the sustainability objectives of the Local Plan.

C) The Council will seek to secure the repair and reuse of Listed Buildings and monuments and improvements to Conservation Areas on the Heritage at Risk Register, through negotiations with owners, the provision of advice and guidance, the use of appropriate legal action, and through bids for external funding for improvement works.

LP2 DMHB 2: Listed Buildings

A) Applications for Listed Building Consent and planning permission to alter, extend, or change the use of a statutorily Listed Building will only be permitted if they are considered to retain its significance and value and are appropriate in terms of the fabric, historic integrity, spatial quality and layout of the building. Any additions or alterations to a Listed Building should be sympathetic in terms of scale, proportion, detailed design, materials and workmanship.

B) Applications should include a Heritage Statement that demonstrates a clear understanding of the importance of the building and the impact of the proposals on its significance.

C) The substantial harm to or total loss of significance of a statutory Listed Building will only be permitted in exceptional circumstances when the nature of the heritage asset prevents all reasonable use of the building, no viable use can be found through marketing, grant-funding or charitable or public ownership and the loss is outweighed by bringing the site back into use. In such circumstances, full archaeological recording of the building will be required.

D) Planning permission will not be granted for proposals which are considered detrimental to the setting of a Listed Building.

LP2 DMHB 3: Locally Listed Buildings

A) There is a general presumption in favour of the retention of buildings, structures and features included in the Local List. The Council will take into account the effect of a proposal on the building's significance and the scale of any harm of loss when considering planning applications, including those for major alterations and extensions. Proposals will be permitted where they retain the significance, appearance, character or setting of a Locally Listed Building.

B) Applications should include a Heritage Statement that demonstrates a clear understanding of the importance of the structure and the impact of the proposals on the significance of the Locally Listed Building.

C) Replacement will only be considered if it can be demonstrated that the community benefits of such a proposal significantly outweigh those of retaining the Locally Listed Building.

LP2 DMHB 4: Conservation Areas

New development, including alterations and extensions to existing buildings, within a Conservation Area or on its fringes, will be expected to preserve or enhance the character or appearance of the area. It should sustain and enhance its significance and make a positive contribution to local character and distinctiveness. In order to achieve this, the Council will:

- A) Require proposals for new development, including any signage or advertisement, to be of a high quality contextual design. Proposals should exploit opportunities to restore any lost features and/or introduce new ones that would enhance the character and appearance of the Conservation Area.
- B) Resist the loss of buildings, historic street patterns, important views, landscape and open spaces or other features that make a positive contribution to the character or appearance of the Conservation Area; any such loss will need to be supported with a robust justification.
- C) Proposals will be required to support the implementation of improvement actions set out in relevant Conservation Area Appraisals and Management Plans.

LP2 DMHB 5: Areas of Special Local Character

- A) Within Areas of Special Local Character, new development should reflect the character of the area and its original layout. Alterations should respect the established scale, building lines, height, design and materials of the area.
- B) Extensions to dwellings should be subservient to, and respect the architectural style of the original buildings and allow sufficient space for appropriate landscaping, particularly between, and in front of, buildings.
- C) The replacement of buildings which positively contribute to the character and local importance of Areas of Special Local Character will normally be resisted.

LP2 DMHB 11: Design of New Development

- A) All development, including extensions, alterations and new buildings will be required to be designed to the highest standards and, incorporate principles of good design including:
 - i) harmonising with the local context by taking into account the surrounding:
 - scale of development, considering the height, mass and bulk of adjacent structures;
 - building plot sizes and widths, plot coverage and established street patterns;
 - building lines and setbacks, rooflines, streetscape rhythm, for example, gaps between structures and other streetscape elements, such as degree of enclosure;
 - architectural composition and quality of detailing;
 - local topography, views both from and to the site; and

- impact on neighbouring open spaces and their environment.
- ii) ensuring the use of high quality building materials and finishes;
- iii) ensuring that the internal design and layout of development maximises sustainability and is adaptable to different activities;
- iv) protecting features of positive value within and adjacent to the site, including the safeguarding of heritage assets, designated and un-designated, and their settings; and
- v) landscaping and tree planting to protect and enhance amenity, biodiversity and green infrastructure.

B) Development proposals should not adversely impact on the amenity, daylight and sunlight of adjacent properties and open space.

C) Development will be required to ensure that the design safeguards the satisfactory re-development of any adjoining sites which have development potential. In the case of proposals for major development sites, the Council will expect developers to prepare master plans and design codes and to agree these with the Council before developing detailed designs.

D) Development proposals should make sufficient provision for well designed internal and external storage space for general, recycling and organic waste, with suitable access for collection. External bins should be located and screened to avoid nuisance and adverse visual impacts to occupiers and neighbours.

LP2 DMHB 12: Streets and Public Realm

A) Development should be well integrated with the surrounding area and accessible. It should:

- i) improve legibility and promote routes and wayfinding between the development and local amenities;
- ii) ensure public realm design takes account of the established townscape character and quality of the surrounding area;
- iii) include landscaping treatment that is suitable for the location, serves a purpose, contributes to local green infrastructure, the appearance of the area and ease of movement through the space;
- iv) provide safe and direct pedestrian and cycle movement through the space;
- v) incorporate appropriate and robust hard landscaping, using good quality materials, undertaken to a high standard;
- vi) where appropriate, include the installation of public art; and
- vii) deliver proposals which incorporate the principles of inclusive design. Proposals for gated developments will be resisted.

B) Public realm improvements will be sought from developments located close to transport interchanges and community facilities to ensure easy access between different transport modes and into local community facilities.

LP2 DMHB 14: Trees and Landscaping

A) All developments will be expected to retain or enhance existing landscaping, trees, biodiversity or other natural features of merit.

B) Development proposals will be required to provide a landscape scheme that includes hard and soft landscaping appropriate to the character of the area, which supports and enhances biodiversity and amenity particularly in areas deficient in green infrastructure.

C) Where space for ground level planting is limited, such as high rise buildings, the inclusion of living walls and roofs will be expected where feasible.

D) Planning applications for proposals that would affect existing trees will be required to provide an accurate tree survey showing the location, height, spread and species of trees. Where the tree survey identifies trees of merit, tree root protection areas and an arboricultural method statement will be required to show how the trees will be protected. Where trees are to be removed, proposals for replanting of new trees on-site must be provided or include contributions to offsite provision.

LP2 DMHB 15: Planning for Safer Places

The Council will require all new development to ensure safe and attractive public and private spaces by referring to the Council's latest guidance on Secured by Design principles. Where relevant, these should be included in the Design and Access Statement. Development will be required to comprise good design and create inclusive environments whilst improving safety and security by incorporating the following specific measures:

- i) providing entrances in visible, safe and accessible locations;
- ii) maximising natural surveillance;
- iii) ensuring adequate defensible space is provided;
- iv) providing clear delineations between public and private spaces; and
- v) providing appropriate lighting and CCTV.

LP D13: Agent of Change

A) The Agent of Change principle places the responsibility for mitigating impacts from existing noise and other nuisance-generating activities or uses on the proposed new noise-sensitive development. Boroughs should ensure that Development Plans and planning decisions reflect the Agent of Change principle and take account of existing noise and other nuisance-generating uses in a sensitive manner when new development is proposed nearby.

B) Development should be designed to ensure that established noise and other nuisance-generating uses remain viable and can continue or grow without unreasonable restrictions being placed on them.

C) New noise and other nuisance-generating development proposed close to residential and other noise-sensitive uses should put in place measures to mitigate and manage any noise impacts for neighbouring residents and businesses.

D) Development proposals should manage noise and other potential nuisances by:

1. ensuring good design mitigates and minimises existing and potential nuisances generated by existing uses and activities located in the area
2. exploring mitigation measures early in the design stage, with necessary and appropriate provisions including ongoing and future management of mitigation measures secured through planning obligations
3. separating new noise-sensitive development where possible from existing noise-generating businesses and uses through distance, screening, internal layout, sound-proofing, insulation and other acoustic design measures.

E) Boroughs should not normally permit development proposals that have not clearly demonstrated how noise and other nuisances will be mitigated and managed.

LP D14: Noise

A) In order to reduce, manage and mitigate noise to improve health and quality of life, residential and other non-aviation development proposals should manage noise by:

1. avoiding significant adverse noise impacts on health and quality of life
2. reflecting the Agent of Change principle as set out in Policy D13 Agent of Change
3. mitigating and minimising the existing and potential adverse impacts of noise on, from, within, as a result of, or in the vicinity of new development without placing unreasonable restrictions on existing noise-generating uses
4. improving and enhancing the acoustic environment and promoting appropriate
5. separating new noise-sensitive development from major noise sources (such as road, rail, air transport and some types of industrial use) through the use of distance, screening, layout, orientation, uses and materials – in preference to sole reliance on sound insulation
6. where it is not possible to achieve separation of noise-sensitive development and noise sources without undue impact on other sustainable development objectives, then any potential adverse effects should be controlled and mitigated through applying good acoustic design principles
7. promoting new technologies and improved practices to reduce noise at source, and on the transmission path from source to receiver.

B) Boroughs, and others with relevant responsibilities, should identify and nominate new Quiet Areas and protect existing Quiet Areas in line with the procedure in Defra's Noise Action Plan for Agglomerations.

LP SI 2: Minimising Greenhouse Gas Emissions

A) Major development should be net zero-carbon.¹⁵¹ This means reducing greenhouse gas emissions in operation and minimising both annual and peak energy demand in accordance with the following energy hierarchy:

1. be lean: use less energy and manage demand during operation
2. be clean: exploit local energy resources (such as secondary heat) and supply energy efficiently and cleanly
3. be green: maximise opportunities for renewable energy by producing, storing and using renewable energy on-site
4. be seen: monitor, verify and report on energy performance.

B) Major development proposals should include a detailed energy strategy to demonstrate how the zero-carbon target will be met within the framework of the energy hierarchy.

C) A minimum on-site reduction of at least 35 per cent beyond Building Regulations¹⁵² is required for major development. Residential development should achieve 10 per cent, and non-residential development should achieve 15 per cent through energy efficiency measures. Where it is clearly demonstrated that the zero-carbon target cannot be fully achieved on-site, any shortfall should be provided, in agreement with the borough, either:

1. through a cash in lieu contribution to the borough's carbon offset fund, or
2. off-site provided that an alternative proposal is identified and delivery is certain.

D) Boroughs must establish and administer a carbon offset fund. Offset fund payments must be ring-fenced to implement projects that deliver carbon reductions. The operation of offset funds should be monitored and reported on annually.

E) Major development proposals should calculate and minimise carbon emissions from any other part of the development, including plant or equipment, that are not covered by Building Regulations, i.e. unregulated emissions.

F) Development proposals referable to the Mayor should calculate whole life-cycle carbon emissions through a nationally recognised Whole Life-Cycle Carbon Assessment and demonstrate actions taken to reduce life-cycle carbon emissions.

LP SI 12: Flood Risk Management

A) Current and expected flood risk from all sources (as defined in paragraph 9.2.12) across London should be managed in a sustainable and cost-effective way in collaboration with the Environment Agency, the Lead Local Flood Authorities, developers and infrastructure providers.

B) Development Plans should use the Mayor's Regional Flood Risk Appraisal and their Strategic Flood Risk Assessment as well as Local Flood Risk Management

Strategies, where necessary, to identify areas where particular and cumulative flood risk issues exist and develop actions and policy approaches aimed at reducing these risks. Boroughs should cooperate and jointly address cross-boundary flood risk issues including with authorities outside London.

C) Development proposals should ensure that flood risk is minimised and mitigated, and that residual risk is addressed. This should include, where possible, making space for water and aiming for development to be set back from the banks of watercourses.

D) Developments Plans and development proposals should contribute to the delivery of the measures set out in Thames Estuary 2100 Plan. The Mayor will work with the Environment Agency and relevant local planning authorities, including authorities outside London, to safeguard an appropriate location for a new Thames Barrier.

E) Development proposals for utility services should be designed to remain operational under flood conditions and buildings should be designed for quick recovery following a flood.

F) Development proposals adjacent to flood defences will be required to protect the integrity of flood defences and allow access for future maintenance and upgrading. Unless exceptional circumstances are demonstrated for not doing so, development proposals should be set back from flood defences to allow for any foreseeable future maintenance and upgrades in a sustainable and cost-effective way.

G) Natural flood management methods should be employed in development proposals due to their multiple benefits including increasing flood storage and creating recreational areas and habitat.

LP SI 13: Sustainable Drainage

A) Lead Local Flood Authorities should identify – through their Local Flood Risk Management Strategies and Surface Water Management Plans – areas where there are particular surface water management issues and aim to reduce these risks. Increases in surface water run-off outside these areas also need to be identified and addressed.

B) Development proposals should aim to achieve greenfield run-off rates and ensure that surface water run-off is managed as close to its source as possible. There should also be a preference for green over grey features, in line with the following drainage hierarchy:

1. rainwater use as a resource (for example rainwater harvesting, blue roofs for irrigation)
2. rainwater infiltration to ground at or close to source
3. rainwater attenuation in green infrastructure features for gradual release (for example green roofs, rain gardens)

4. rainwater discharge direct to a watercourse (unless not appropriate)
5. controlled rainwater discharge to a surface water sewer or drain
6. controlled rainwater discharge to a combined sewer.

C) Development proposals for impermeable surfacing should normally be resisted unless they can be shown to be unavoidable, including on small surfaces such as front gardens and driveways.

D) Drainage should be designed and implemented in ways that promote multiple benefits including increased water use efficiency, improved water quality, and enhanced biodiversity, urban greening, amenity and recreation.

LP1 EM6: Flood Risk Management

The Council will require new development to be directed away from Flood Zones 2 and 3 in accordance with the principles of the National Planning Policy Framework (NPPF).

The subsequent Hillingdon Local Plan: Part 2 -Site Specific Allocations LDD will be subjected to the Sequential Test in accordance with the NPPF. Sites will only be allocated within Flood Zones 2 or 3 where there are overriding issues that outweigh flood risk. In these instances, policy criteria will be set requiring future applicants of these sites to demonstrate that flood risk can be suitably mitigated.

The Council will require all development across the borough to use sustainable urban drainage systems (SUDS) unless demonstrated that it is not viable. The Council will encourage SUDS to be linked to water efficiency methods. The Council may require developer contributions to guarantee the long term maintenance and performance of SUDS is to an appropriate standard.

LP1 EM8: Land, Water, Air and Noise

Water Quality

The Council will seek to safeguard and improve all water quality, both ground and surface. Principal Aquifers, and Source Protection Zones will be given priority along with the:

- River Colne
- Grand Union Canal
- River Pinn
- Yeading Brook
- Porter Land Brook
- River Crane
- Ruislip Lido

Air Quality

All development should not cause deterioration in the local air quality levels and should ensure the protection of both existing and new sensitive receptors.

All major development within the Air Quality Management Area (AQMA) should demonstrate air quality neutrality (no worsening of impacts) where appropriate; actively contribute to the promotion of sustainable transport measures such as vehicle charging points and the increased provision for vehicles with cleaner transport fuels; deliver increased planting through soft landscaping and living walls and roofs; and provide a management plan for ensuring air quality impacts can be kept to a minimum.

The Council seeks to reduce the levels of pollutants referred to in the Government's National Air Quality Strategy and will have regard to the Mayor's Air Quality Strategy. London Boroughs should also take account of the findings of the Air Quality Review and Assessments and Actions plans, in particular where Air Quality Management Areas have been designated.

The Council has a network of Air Quality Monitoring stations but recognises that this can be widened to improve understanding of air quality impacts. The Council may therefore require new major development in an AQMA to fund additional air quality monitoring stations to assist in managing air quality improvements.

Noise

The Council will investigate Hillingdon's target areas identified in the Defra Noise Action Plans, promote the maximum possible reduction in noise levels and will minimise the number of people potentially affected.

The Council will seek to identify and protect Quiet Areas in accordance with Government Policy on sustainable development and other Local Plan policies.

The Council will seek to ensure that noise sensitive development and noise generating development are only permitted if noise impacts can be adequately controlled and mitigated.

Land Contamination

The Council will expect proposals for development on contaminated land to provide mitigation strategies that reduce the impacts on surrounding land uses. Major development proposals will be expected to demonstrate a sustainable approach to remediation that includes techniques to reduce the need to landfill.

Water Resources

The Council will require that all new development demonstrates the incorporation of water efficiency measures within new development to reduce the rising demand on potable water. All new development must incorporate water recycling and collection facilities unless it can be demonstrated it is not appropriate. For residential

developments, the Council will require applicants to demonstrate that water consumption will not surpass 105 litres per person per day.

LP2 DMEI 2: Reducing Carbon Emissions

- A) All developments are required to make the fullest contribution to minimising carbon dioxide emissions in accordance with London Plan targets.
- B) All major development proposals must be accompanied by an energy assessment showing how these reductions will be achieved.
- C) Proposals that fail to take reasonable steps to achieve the required savings will be resisted. However, where it is clearly demonstrated that the targets for carbon emissions cannot be met onsite, the Council may approve the application and seek an off-site contribution to make up for the shortfall.

LP2 DMEI 9: Management of Flood Risk

- A) Development proposals in Flood Zones 2 and 3a will be required to demonstrate that there are no suitable sites available in areas of lower flood risk. Where no appropriate sites are available, development should be located on the areas of lowest flood risk within the site. Flood defences should provide protection for the lifetime of the development. Finished floor levels should reflect the Environment Agency's latest guidance on climate change.
- B) Development proposals in these areas will be required to submit an appropriate level Flood Risk Assessment (FRA) to demonstrate that the development is resilient to all sources of flooding.
- C) Development in Flood Zone 3b will be refused in principle unless identified as an appropriate development in Flood Risk Planning Policy Guidance. Development for appropriate uses in Flood Zone 3b will only be approved if accompanied by an appropriate FRA that demonstrates the development will be resistant and resilient to flooding and suitable warning and evacuation methods are in place.
- D) Developments may be required to make contributions (through legal agreements) to previously identified flood improvement works that will benefit the development site.
- E) Proposals that fail to make appropriate provision for flood risk mitigation, or which would increase the risk or consequences of flooding, will be refused.

LP2 DMEI 10: Water Management, Efficiency, and Quality

- A) Applications for all new build developments (not conversions, change of use, or refurbishment) are required to include a drainage assessment demonstrating that

appropriate sustainable drainage systems (SuDS) have been incorporated in accordance with the London Plan Hierarchy (Policy 5.13: Sustainable drainage).

B) All major new build developments, as well as minor developments in Critical Drainage Areas or an area identified at risk from surface water flooding must be designed to reduce surface water run-off rates to no higher than the pre-development greenfield run-off rate in a 1:100 year storm scenario, plus an appropriate allowance for climate change for the worst storm duration. The assessment is required regardless of the changes in impermeable areas and the fact that a site has an existing high run-off rate will not constitute justification.

C) Rain Gardens and non householder development should be designed to reduce surface water run-off rates to Greenfield run-off rates.

D) Schemes for the use of SuDS must be accompanied by adequate arrangements for the management and maintenance of the measures used, with appropriate contributions made to the Council where necessary.

E) Proposals that would fail to make adequate provision for the control and reduction of surface water run-off rates will be refused.

F) Developments should be drained by a SuDS system and must include appropriate methods to avoid pollution of the water environment. Preference should be given to utilising the drainage options in the SuDS hierarchy which remove the key pollutants that hinder improving water quality in Hillingdon. Major development should adopt a 'treatment train' approach where water flows through different SuDS to ensure resilience in the system. Water Efficiency

G) All new development proposals (including refurbishments and conversions) will be required to include water efficiency measures, including the collection and reuse of rain water and grey water.

H) All new residential development should demonstrate water usage rates of no more than 105 litres/person/day.

I) It is expected that major development proposals will provide an integrated approach to surface water run-off attenuation, water collection, recycling and reuse. Water and Wastewater Infrastructure

J) All new development proposals will be required to demonstrate that there is sufficient capacity in the water and wastewater infrastructure network to support the proposed development. Where there is a capacity constraint the local planning authority will require the developer to provide a detailed water and/or drainage strategy to inform what infrastructure is required, where, when and how it will be delivered.

LP2 DMEI 12: Development of Land Affected by Contamination

A) Proposals for development on potentially contaminated sites will be expected to be accompanied by at least an initial study of the likely contaminants. The Council will support planning permission for any development of land which is affected by contamination where it can be demonstrated that contamination issues have been adequately assessed and the site can be safely remediated so that the development can be made suitable for the proposed use.

B) Conditions will be imposed where planning permission is given for development on land affected by contamination to ensure all the necessary remedial works are implemented, prior to commencement of development.

C) Where initial studies reveal potentially harmful levels of contamination, either to human health or controlled waters and other environmental features, full intrusive ground investigations and remediation proposals will be expected prior to any approvals.

D) In some instances, where remedial works relate to an agreed set of measures such as the management of ongoing remedial systems, or remediation of adjoining or other affected land, a S106 planning obligation will be sought.

LP2 DMEI 14: Air Quality

A) Development proposals should demonstrate appropriate reductions in emissions to sustain compliance with and contribute towards meeting EU limit values and national air quality objectives for pollutants.

B) Development proposals should, as a minimum:

i) be at least “air quality neutral”;

ii) include sufficient mitigation to ensure there is no unacceptable risk from air pollution to sensitive receptors, both existing and new; and

iii) actively contribute towards the improvement of air quality, especially within the Air Quality Management Area.

TP T4: Assessing and Mitigating Transport Impacts

A) Development Plans and development proposals should reflect and be integrated with current and planned transport access, capacity and connectivity.

B) When required in accordance with national or local guidance, transport assessments/statements should be submitted with development proposals to ensure that impacts on the capacity of the transport network (including impacts on pedestrians and the cycle network), at the local, network-wide and strategic level, are fully assessed. Transport assessments should focus on embedding the Healthy Streets Approach within, and in the vicinity of, new development. Travel Plans,

Parking Design and Management Plans, Construction Logistics Plans and Delivery and Servicing Plans will be required having regard to Transport for London guidance.

C) Where appropriate, mitigation, either through direct provision of public transport, walking and cycling facilities and highways improvements or through financial contributions, will be required to address adverse transport impacts that are identified.

D) Where the ability to absorb increased travel demand through active travel modes has been exhausted, existing public transport capacity is insufficient to allow for the travel generated by proposed developments, and no firm plans and funding exist for an increase in capacity to cater for the increased demand, planning permission will be contingent on the provision of necessary public transport and active travel infrastructure.

E) The cumulative impacts of development on public transport and the road network capacity including walking and cycling, as well as associated effects on public health, should be taken into account and mitigated.

F) Development proposals should not increase road danger.

LP T5: Cycling

A) Development Plans and development proposals should help remove barriers to cycling and create a healthy environment in which people choose to cycle. This will be achieved through:

1. supporting the delivery of a London-wide network of cycle routes, with new routes and improved infrastructure
2. securing the provision of appropriate levels of cycle parking which should be fit for purpose, secure and well-located. Developments should provide cycle parking at least in accordance with the minimum standards set out in Table 10.2 and Figure 10.3, ensuring that a minimum of two short-stay and two long-stay cycle parking spaces are provided where the application of the minimum standards would result in a lower provision.

B) Cycle parking should be designed and laid out in accordance with the guidance contained in the London Cycling Design Standards. Development proposals should demonstrate how cycle parking facilities will cater for larger cycles, including adapted cycles for disabled people.

C) Development Plans requiring more generous provision of cycle parking based on local evidence will be supported.

D) Where it is not possible to provide suitable short-stay cycle parking off the public highway, the borough should work with stakeholders to identify an appropriate on-street location for the required provision. This may mean the reallocation of space

from other uses such as on-street car parking. Alternatively, in town centres, adding the required provision to general town centre cycle parking is also acceptable. In such cases, a commuted sum should be paid to the local authority to secure provision.

E) Where it is not possible to provide adequate cycle parking within residential developments, boroughs must work with developers to propose alternative solutions which meet the objectives of the standards. These may include options such as providing spaces in secure, conveniently-located, on-street parking facilities such as bicycle hangers.

F) Where the use class of a development is not fixed at the point of application, the highest potential applicable cycle parking standard should be applied.

LP T6: Car Parking

A) Car parking should be restricted in line with levels of existing and future public transport accessibility and connectivity.

B) Car-free development should be the starting point for all development proposals in places that are (or are planned to be) well-connected by public transport, with developments elsewhere designed to provide the minimum necessary parking ('car-lite'). Car-free development has no general parking but should still provide disabled persons parking in line with Part E of this policy.

C) An absence of local on-street parking controls should not be a barrier to new development, and boroughs should look to implement these controls wherever necessary to allow existing residents to maintain safe and efficient use of their streets.

D) The maximum car parking standards set out in Policy T6 .1 Residential parking to Policy T6 .5 Non-residential disabled persons parking should be applied to development proposals and used to set local standards within Development Plans.

E) Appropriate disabled persons parking for Blue Badge holders should be provided as set out in Policy T6 .1 Residential parking to Policy T6 .5 Non-residential disabled persons parking.

F) Where provided, each motorcycle parking space should count towards the maximum for car parking spaces at all use classes.

G) Where car parking is provided in new developments, provision should be made for infrastructure for electric or other Ultra-Low Emission vehicles in line with Policy T6 .1 Residential parking, Policy T6 .2 Office Parking, Policy T6 .3 Retail parking, and Policy T6 .4 Hotel and leisure uses parking.

All operational parking should make this provision, including offering rapid charging. New or re-provided petrol filling stations should provide rapid charging hubs and/or hydrogen refuelling facilities.

H) Where electric vehicle charging points are provided on-street, physical infrastructure should not negatively affect pedestrian amenity and should ideally be located off the footway. Where charging points are located on the footway, it must remain accessible to all those using it including disabled people.

I) Adequate provision should be made for efficient deliveries and servicing and emergency access.

J) A Parking Design and Management Plan should be submitted alongside all applications which include car parking provision, indicating how the car parking will be designed and managed, with reference to Transport for London guidance on parking management and parking design.

K) Boroughs that have adopted or wish to adopt more restrictive general or operational parking policies are supported, including borough-wide or other area-based car-free policies. Outer London boroughs wishing to adopt minimum residential parking standards through a Development Plan Document (within the maximum standards set out in Policy T6 .1 Residential parking) must only do so for parts of London that are PTAL 0-1. Inner London boroughs should not adopt minimum standards. Minimum standards are not appropriate for non-residential use classes in any part of London.

L) Where sites are redeveloped, parking provision should reflect the current approach and not be re-provided at previous levels where this exceeds the standards set out in this policy. Some flexibility may be applied where retail sites are redeveloped outside of town centres in areas which are not well served by public transport, particularly in outer London.

LP T6.1: Residential Parking

A) New residential development should not exceed the maximum parking standards set out in Table 10.3. These standards are a hierarchy with the more restrictive standard applying when a site falls into more than one category.

B) Parking spaces within communal car parking facilities (including basements) should be leased rather than sold.

C) All residential car parking spaces must provide infrastructure for electric or Ultra-Low Emission vehicles. At least 20 per cent of spaces should have active charging facilities, with passive provision for all remaining spaces.

D) Outside of the CAZ, and to cater for infrequent trips, car club spaces may be considered appropriate in lieu of private parking. Any car club spaces should have active charging facilities.

E) Large-scale purpose-built shared living, student accommodation and other sui generis residential uses should be car-free.

F) The provision of car parking should not be a reason for reducing the level of affordable housing in a proposed development.

G) Disabled persons parking should be provided for new residential developments. Residential development proposals delivering ten or more units must, as a minimum:

1. ensure that for three per cent of dwellings, at least one designated disabled persons parking bay per dwelling is available from the outset
2. demonstrate as part of the Parking Design and Management Plan, how an additional seven per cent of dwellings could be provided with one designated disabled persons parking space per dwelling in future upon request as soon as existing provision is insufficient. This should be secured at the planning stage.

H) All disabled persons parking bays associated with residential development must:

1. be for residents' use only (whether M4(2) or M4(3) dwellings)
2. not be allocated to specific dwellings, unless provided within the curtilage of the dwelling
3. be funded by the payment of a commuted sum by the applicant, if provided on-street (this includes a requirement to fund provision of electric vehicle charging infrastructure)
4. count towards the maximum parking provision for the development
5. be designed in accordance with the design guidance in BS8300vol.1
6. be located to minimise the distance between disabled persons parking bays and the dwelling or the relevant block entrance or lift core, and the route should be preferably level or where this is not possible, should be gently sloping (1:60-1:20) on a suitable firm ground surface.

LP2 DMT 1: Managing Transport Impacts

A) Development proposals will be required to meet the transport needs of the development and address its transport impacts in a sustainable manner. In order for developments to be acceptable they are required to:

- i) be accessible by public transport, walking and cycling either from the catchment area that it is likely to draw its employees, customers or visitors from and/or the services and facilities necessary to support the development;
- ii) maximise safe, convenient and inclusive accessibility to, and from within developments for pedestrians, cyclists and public transport users;
- iii) provide equal access for all people, including inclusive access for disabled people;
- iv) adequately address delivery, servicing and drop-off requirements; and

v) have no significant adverse transport or associated air quality and noise impacts on the local and wider environment, particularly on the strategic road network.

B) Development proposals will be required to undertake a satisfactory Transport Assessment and Travel Plan if they meet or exceed the appropriate thresholds. All major developments¹¹ that fall below these thresholds will be required to produce a satisfactory Transport Statement and Local Level Travel Plan. All these plans should demonstrate how any potential impacts will be mitigated and how such measures will be implemented.

LP2 DMT 2: Highways Impacts

Development proposals must ensure that:

- i) safe and efficient vehicular access to the highway network is provided to the Council's standards;
- ii) they do not contribute to the deterioration of air quality, noise or local amenity or safety of all road users and residents;
- iii) safe, secure and convenient access and facilities for cyclists and pedestrian are satisfactorily accommodated in the design of highway and traffic management schemes;
- iv) impacts on local amenity and congestion are minimised by routing through traffic by the most direct means to the strategic road network, avoiding local distributor and access roads; and
- v) there are suitable mitigation measures to address any traffic impacts in terms of capacity and functions of existing and committed roads, including along roads or through junctions which are at capacity.

LP2 Policy DMT 5: Pedestrians and Cyclists

A) Development proposals will be required to ensure that safe, direct and inclusive access for pedestrians and cyclists is provided on the site connecting it to the wider network, including:

- i) the retention and, where appropriate, enhancement of any existing pedestrian and cycle routes;
- ii) the provision of a high quality and safe public realm or interface with the public realm, which facilitates convenient and direct access to the site for pedestrian and cyclists;
- iii) the provision of well signposted, attractive pedestrian and cycle routes separated from vehicular traffic where possible; and
- iv) the provision of cycle parking and changing facilities in accordance with Appendix C, Table 1 or, in agreement with Council.

B) Development proposals located next to or along the Blue Ribbon Network will be required to enhance and facilitate inclusive, safe and secure pedestrian and cycle access to the network. Development proposals, by virtue of their design, will be

required to complement and enhance local amenity and include passive surveillance to the network.

LP2 DMT 6: Vehicle Parking

A) Development proposals must comply with the parking standards outlined in Appendix C Table 1 in order to facilitate sustainable development and address issues relating to congestion and amenity. The Council may agree to vary these requirements when:

- i) the variance would not lead to a deleterious impact on street parking provision, congestion or local amenity; and/or
- ii) a transport appraisal and travel plan has been approved and parking provision is in accordance with its recommendations.

B) All car parks provided for new development will be required to contain conveniently located reserved spaces for wheelchair users and those with restricted mobility in accordance with the Council's Accessible Hillingdon SPD.

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Hillingdon Planning Committee

Wednesday 2nd October 2024



HILLINGDON
LONDON

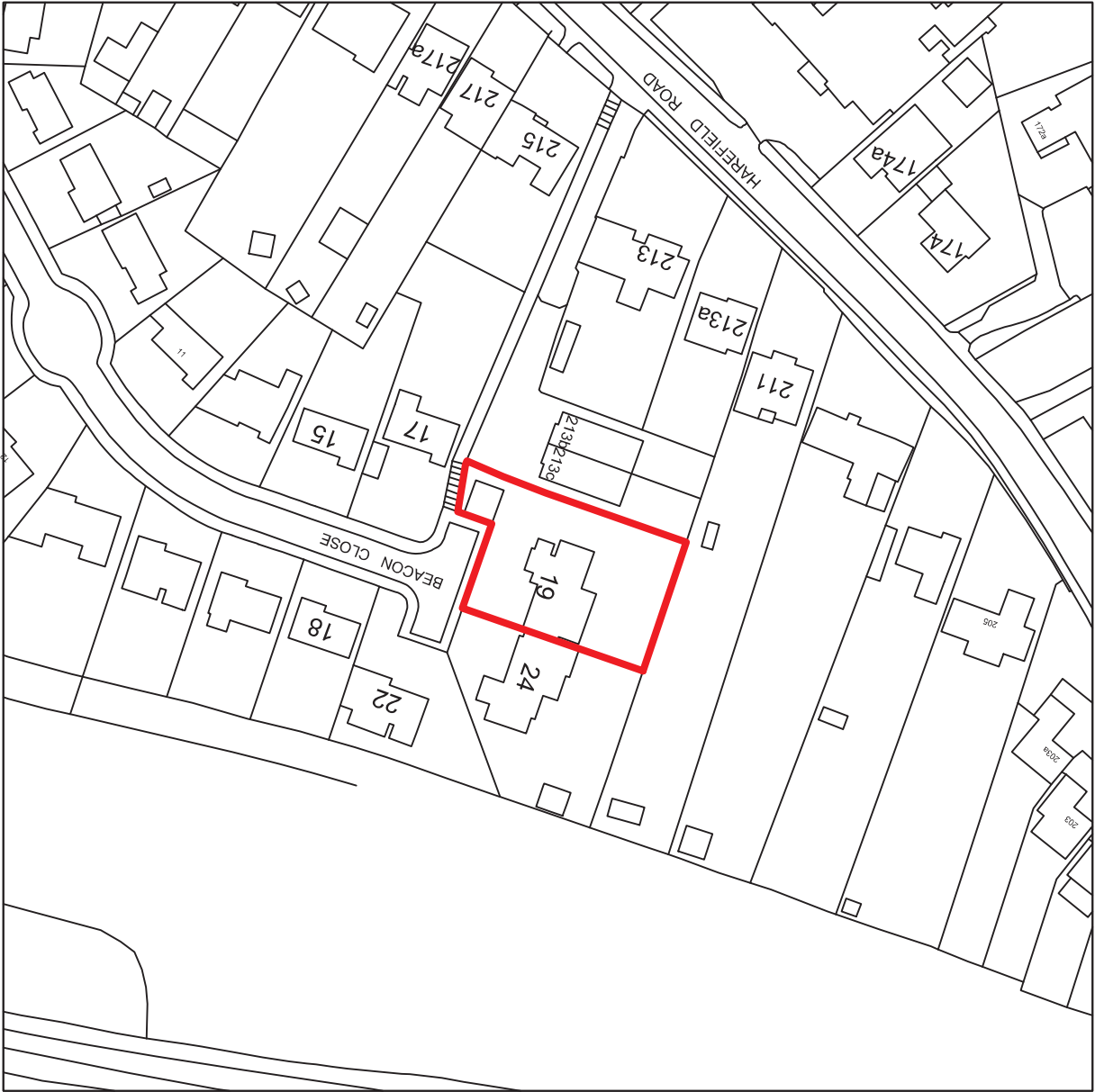
www.hillingdon.gov.uk

Report of the Head of Development Management and Building Control

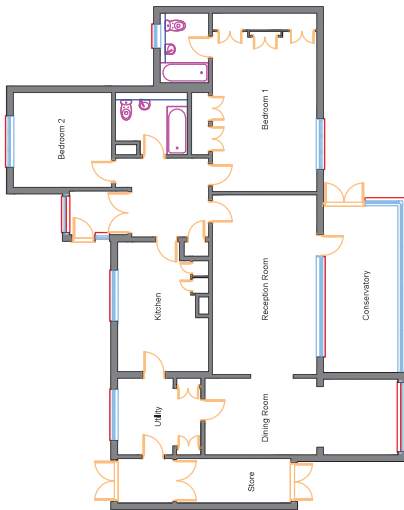
Address: 19 BEACON CLOSE UXBRIDGE

Development: Demolition of existing bungalow and erection of two pairs of semi-detached dwellings to provide 2 x 3-bedroom dwellings and 2 x 2-bedroom dwellings plus associated hard and soft landscaping.

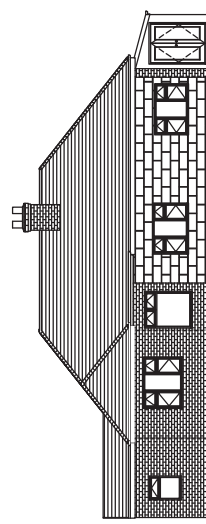
LBH Ref Nos: 17969/APP/2024/845



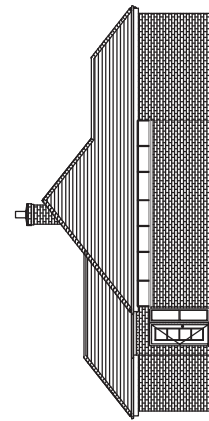
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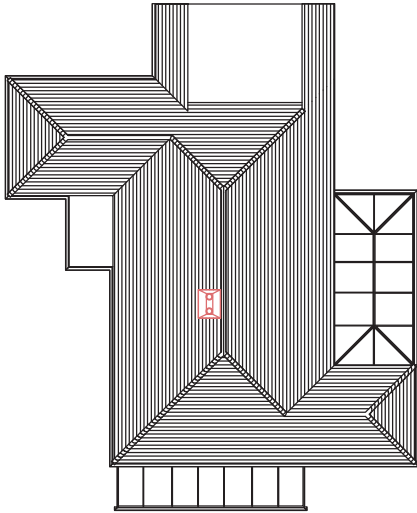
Existing Ground Floor Plan



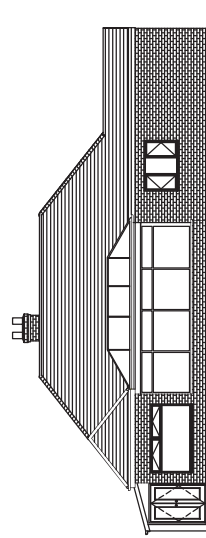
Existing Front Elevation



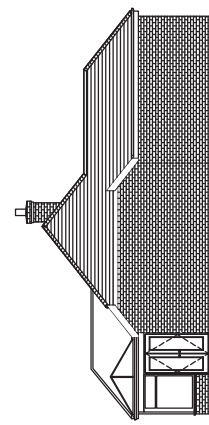
Existing Left Side Elevation



Existing Roof Floor Plan



Existing Rear Elevation



Existing Right Side Elevation

REV	DESCRIPTION	DRAWN	CHECKED	DATE
REVISIONS				

Drawn (check date)	CLIENT APPROVAL	PLANNING D
Checked (check date)	BUILDING REGS D	TENDER D CONSTRUCTION D AS-BUILT D

NOTE: Drawings must not be released, handed or copied without the written consent of Consilio Town Planning Ltd, (the originator).
 All errors, omissions, discrepancies should be reported to the originator immediately, by email or telephone. The originator shall not be liable for any errors, omissions, discrepancies or omissions, or for any consequences arising therefrom, whether or not such errors, omissions, discrepancies or omissions were given, or not given, in writing.
 The Principal Contractor shall be responsible for the coordination of the proposed work and shall ensure that the work is carried out in accordance with the relevant Building Regulations and the relevant Design and Management Regulations 2015. All works to comply with the relevant British Standards, Codes of Practice and the Building Regulations.

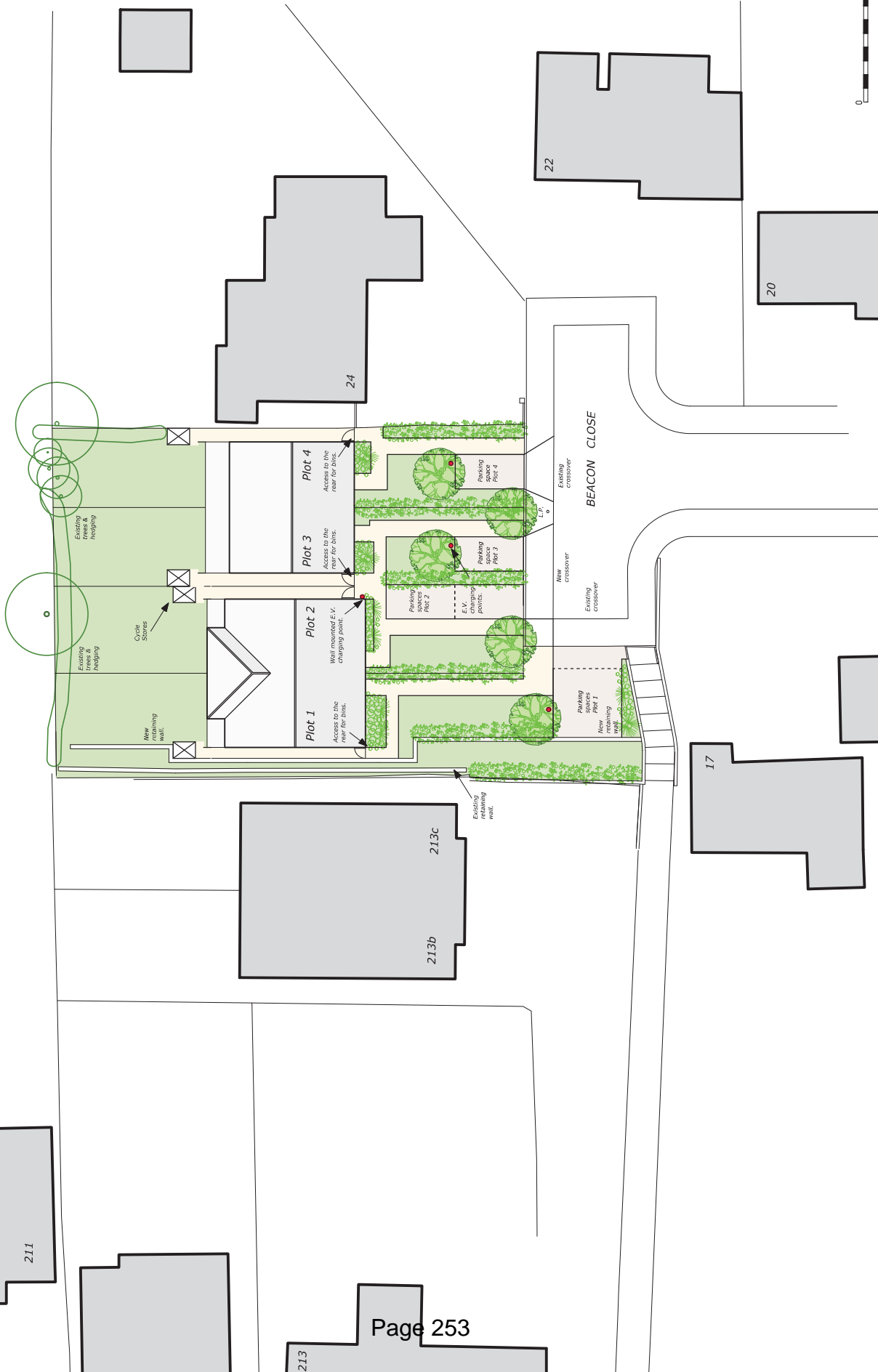


CONSILIO TOWN PLANNING LTD.
 BROOK HOUSE, 54A COWLEY MILL ROAD
 LUXBRIDGE, MIDDLESEX, UB8 3FK
 TELEPHONE 0184822238
 EMAIL INFO@CONSILIO.TOWNPLANNING.CO.UK
 WEB WWW.CONSILIO.TOWNPLANNING.CO.UK

CLIENT: _____
 PROJECT: _____
 TITLE: _____

EXISTING PLANS AND ELEVATIONS

SCALE 1:100 @ A1
 DATE MARCH 2022
 DRAWN: SG
 CHECKED: HM
 JOB NO. 09-22-001 DRAWING NO. 001 REV. 1



KEY



EXISTING TREES RETAINED.



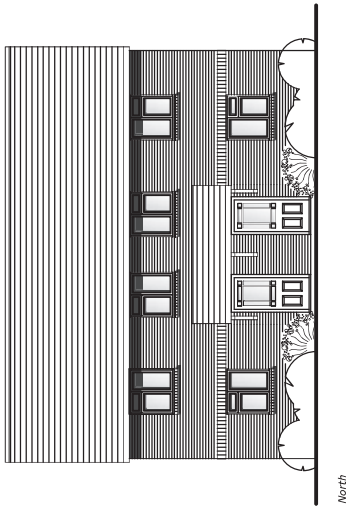
INDICATIVE PROPOSED TREE PLANTING.



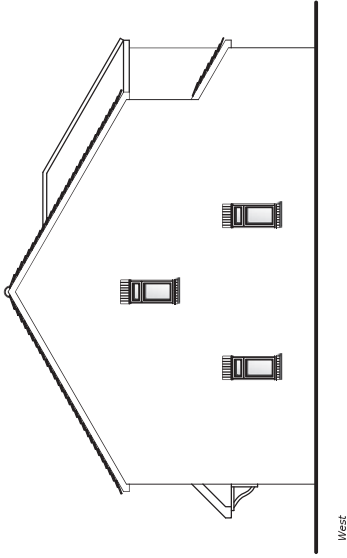
Project No	24 / 3551 / 1	Revision	A
Date	19 / 3 / 24	Scale	1:200 @ A2

W J Macleod
ARCHITECT
70 High Street, Northwood, Middlesex, HA6 1BL
Phone: 01892 69600

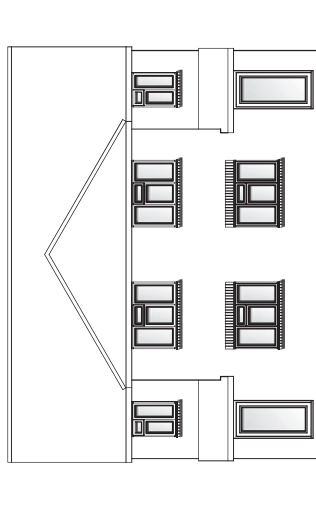
LAND AT 19 BEACON CLOSE, UXBRIDGE.



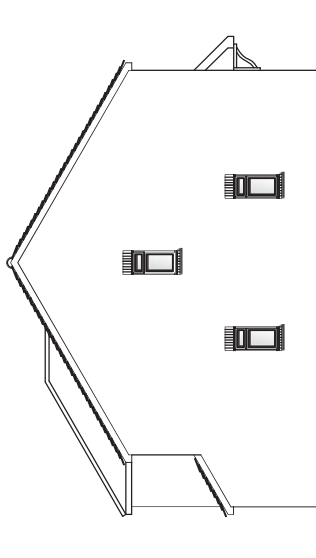
North



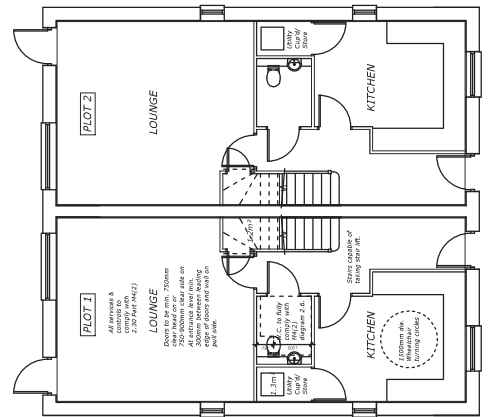
West



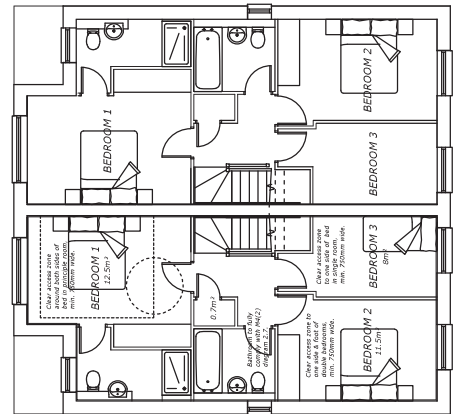
South



East



Ground



First

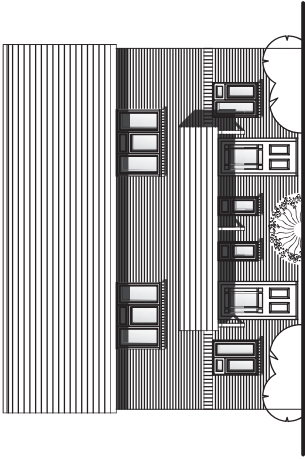
Floor Plans and Elevations Plot 1 & 2



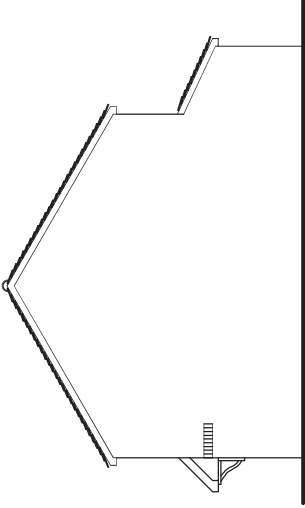
Planning Number	24/3551/2	Version	A
Date	19/3/24	Scale	1:100@A2
Drawn		Checked	

W J Macleod
 ARCHITECT
 7th Hill Street, Northwood Hill, Uxbridge, Middlesex, U.K.
 Phone: 01892 83000

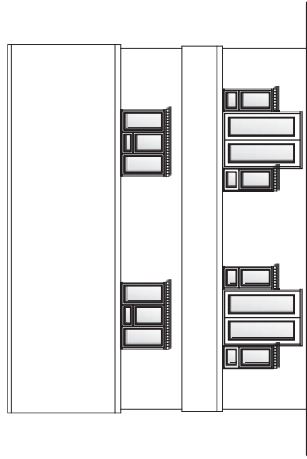
LAND AT 19 BEACON CLOSE, UXBRIDGE.



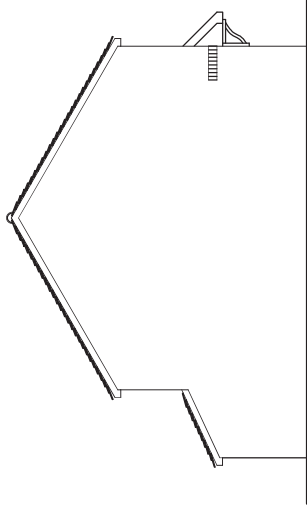
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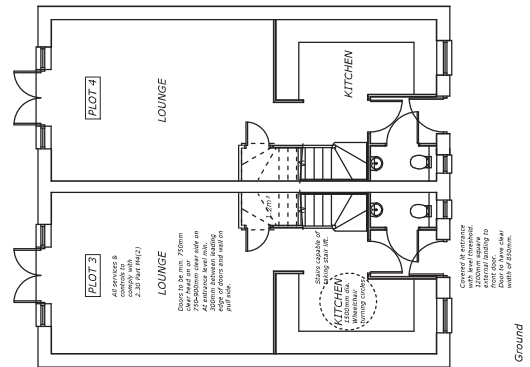
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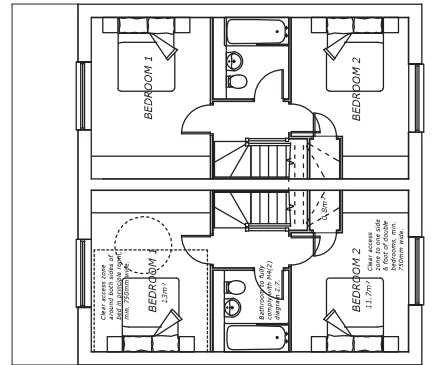
South



East



Ground



First

Floor Plans and Elevations Plot 3 & 4



W J Macleod	
ARCHITECT	
75th Hill Street, Edinburgh EH6 6AE, UK phone: 01232 84000	
Drawing Number	24/3551 / 3
Date	19 / 3 / 24
Scale	1:100 @ A2

LAND AT 19 BEACON CLOSE, UXBRIDGE.



Street Scene

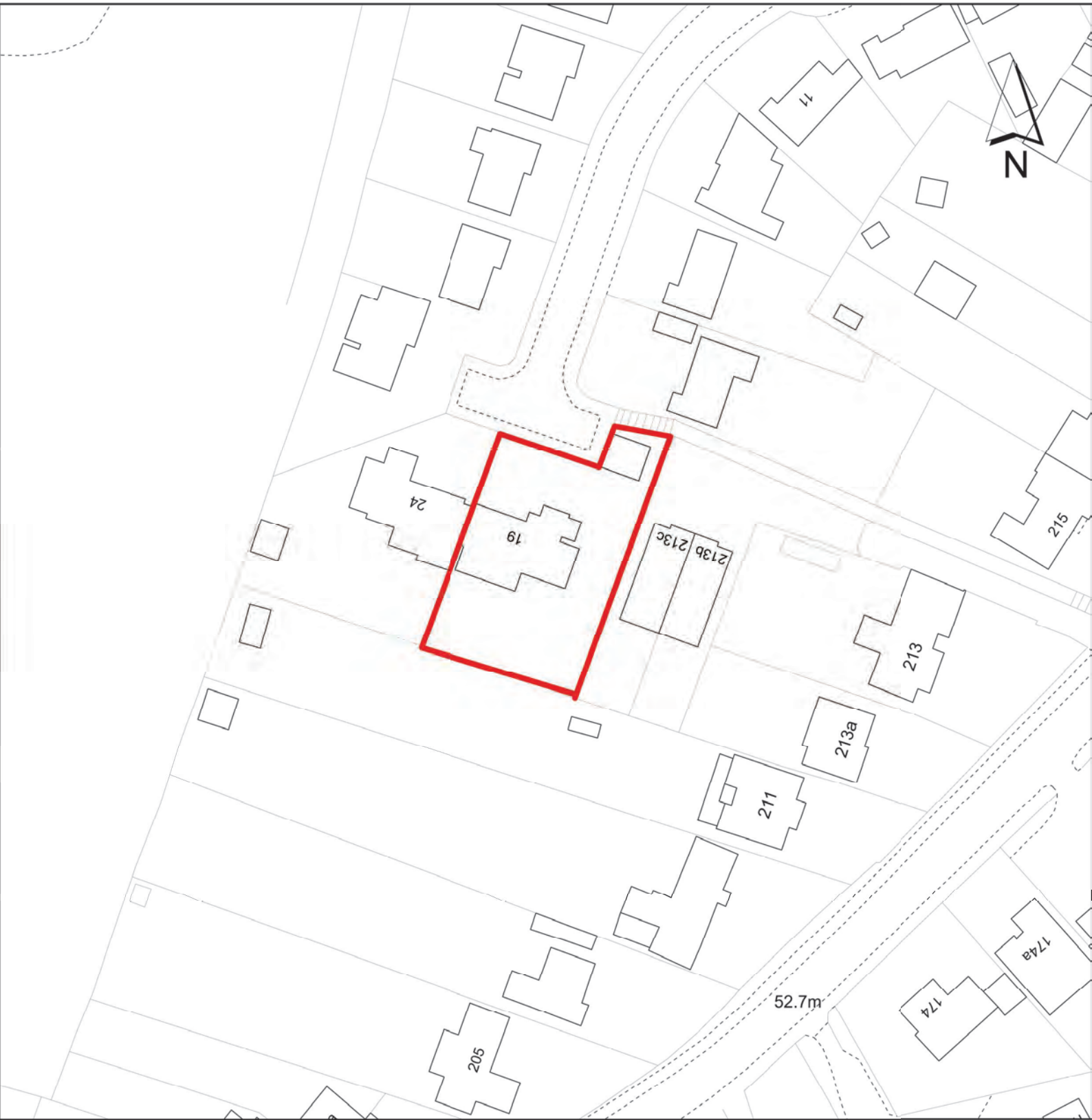




LAND AT 19 BEACON CLOSE, UXBRIDGE.

W J Macleod
ARCHITECT

7th Hill Street, Northwood, Middlesex HA8 1BL.
Phone: 01823 43000

Project No.	24 / 551 / 5
Date	17 / 5 / 24
Scale	1:100 @ A2

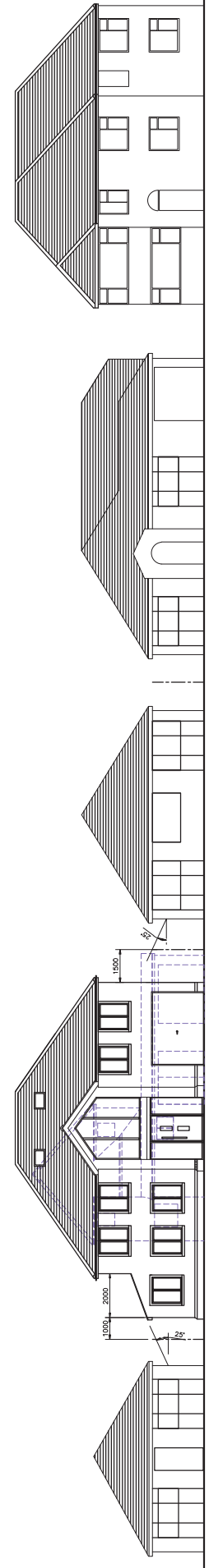
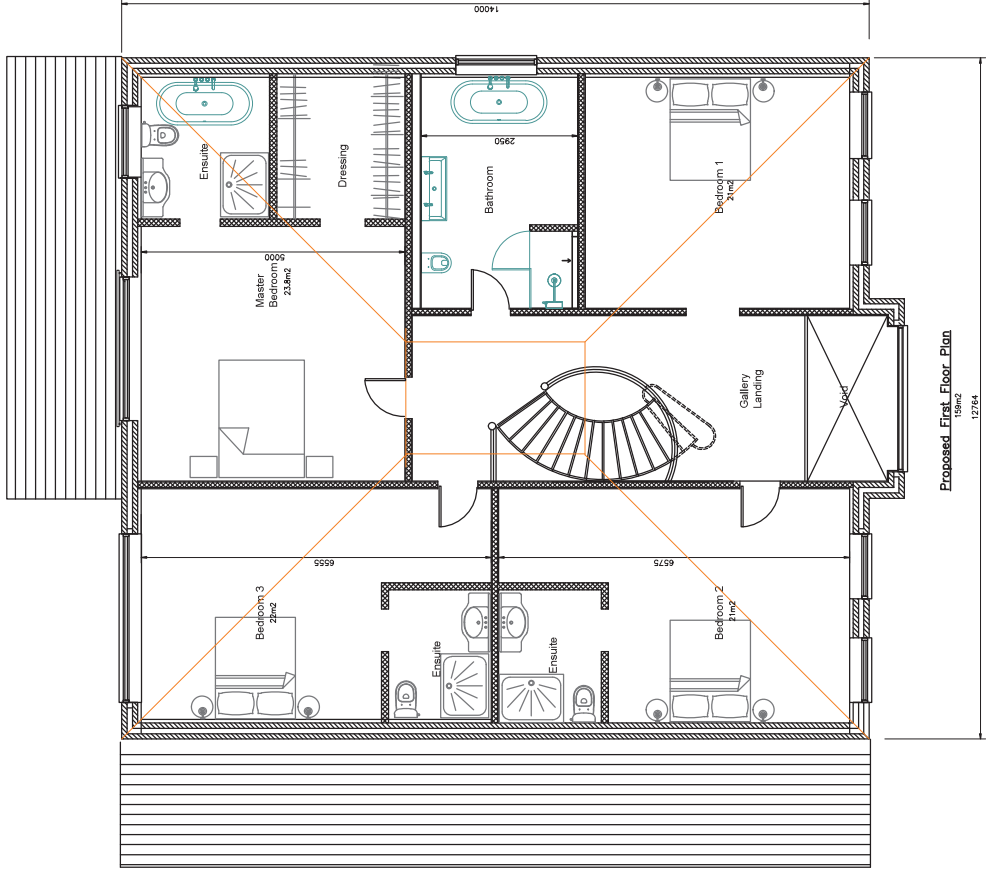
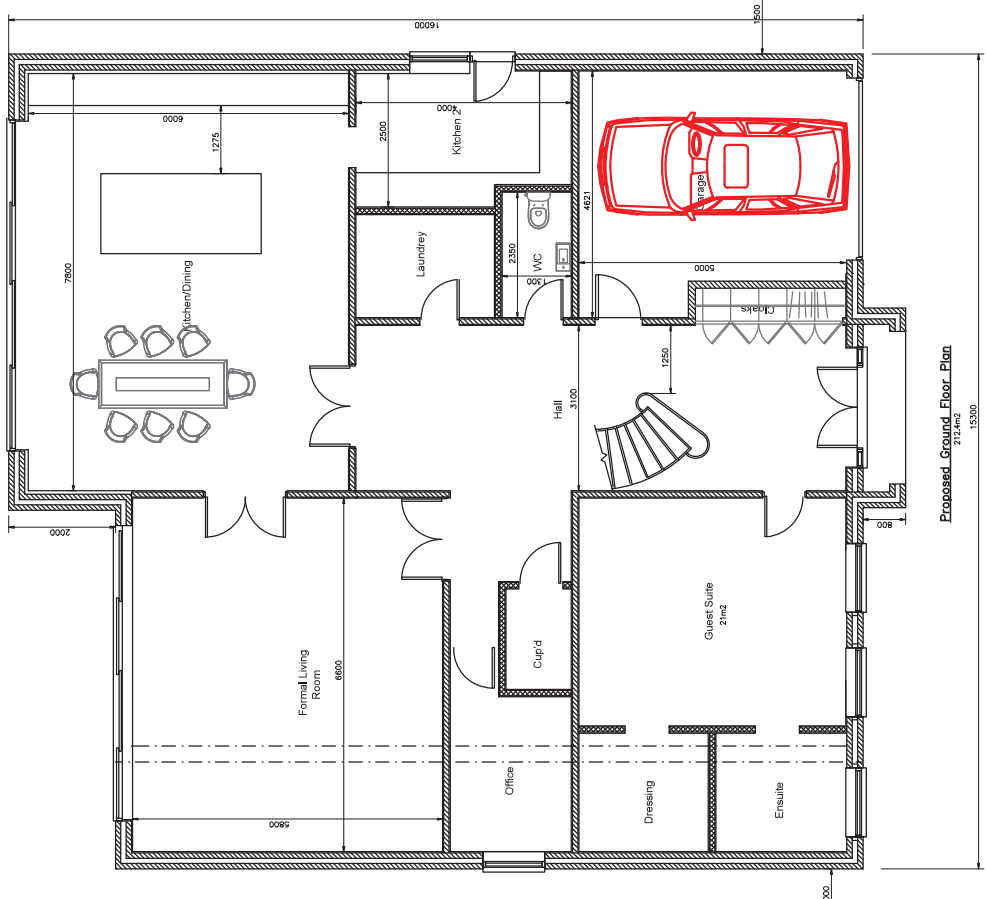


KEY :  Site Boundary	ADDRESS : 19 Beacon Close		LONDON BOROUGH OF HILLINGDON RESIDENTS SERVICES PLANNING SECTION
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	PLANNING COMMITTEE :	DATE : 05/07/2024	

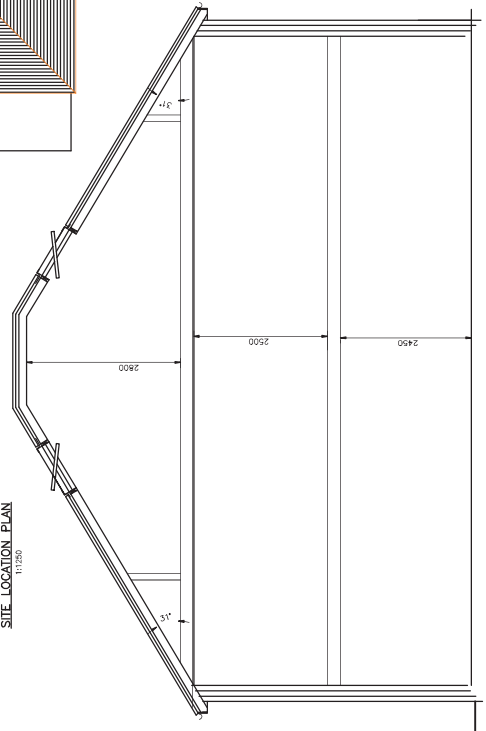
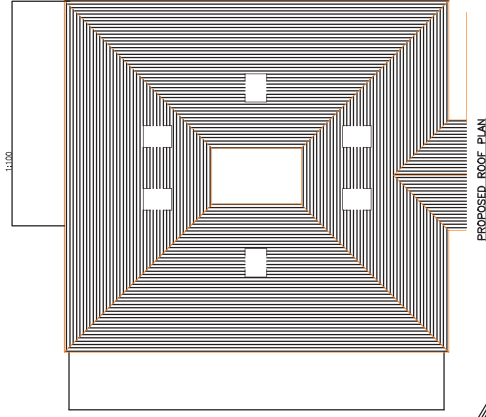
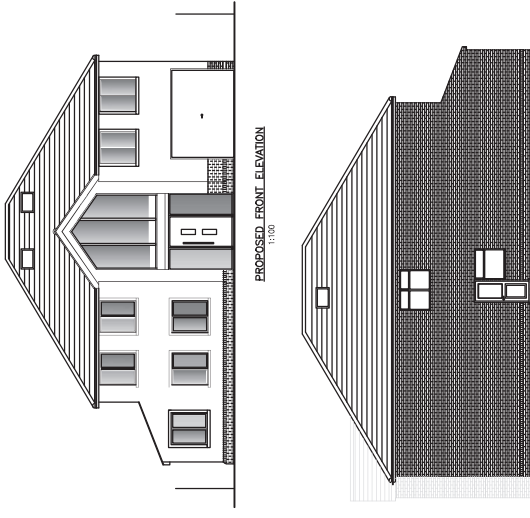
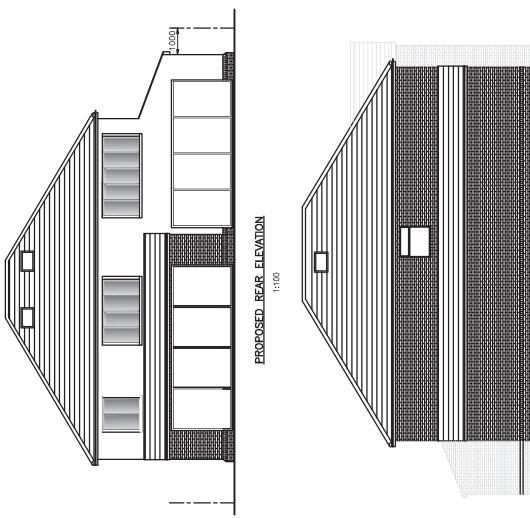
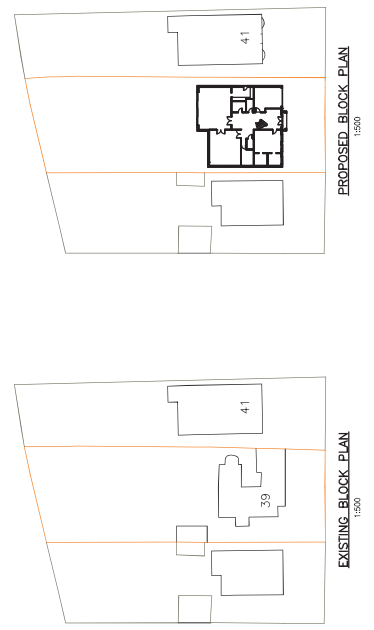
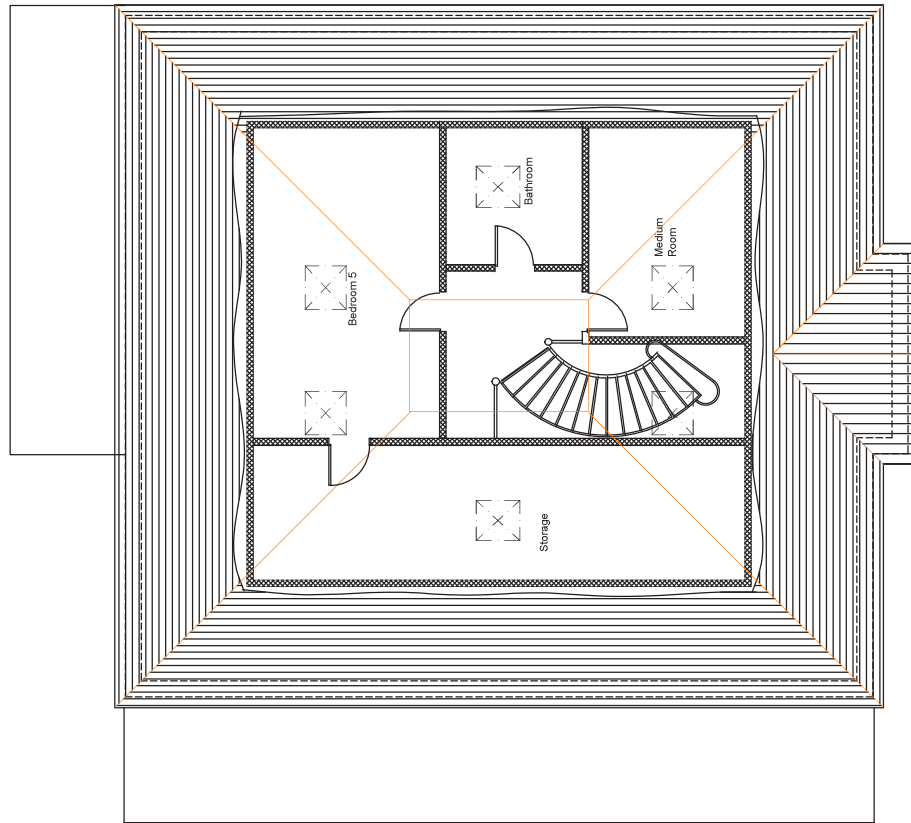
Report of the Head of Development Management and Building Control

Address: 39 PARKFIELD ROAD ICKENHAM
Development: Erection of a replacement dwelling.
LBH Ref Nos: 24825/APP/2023/81

SCALE 1:150



PROPOSED STREET SCENE 1:100		PROPOSED PLANS & STREET SCENE	
Project KDA Designs Ltd Architectural & Structural Services 39 PARKFIELD ROAD ICKENHAM UB10 8LW Tel: 0181 608 2222 Email: kwd@kdaesigns.co.uk www.kdaesigns.co.uk	Drawing PROPOSED REPLACEMENT DWELLING 39 PARKFIELD ROAD ICKENHAM UB10 8LW	Date JUN 2023	Rev 01 39PR/P100 B
B ISSUED FOR FINAL ASSESSMENT 20/9/23		A AMENDED TO SUIT PLANNERS REQUEST 16/9/23	
REVISIONS			



Rev	Description	Date
E	SUED ELEVATIONS AMENDED	26/7/24
D	AMENDED TO SUIT PLANNERS REQUESTS	09/10/23
C	ISSUED FOR FINAL ASSESSMENT	25/9/23
B	AMENDED TO SUIT PLANNERS REQUESTS	22/9/23
A	AMENDED TO SUIT PLANNERS REQUESTS	14/9/23

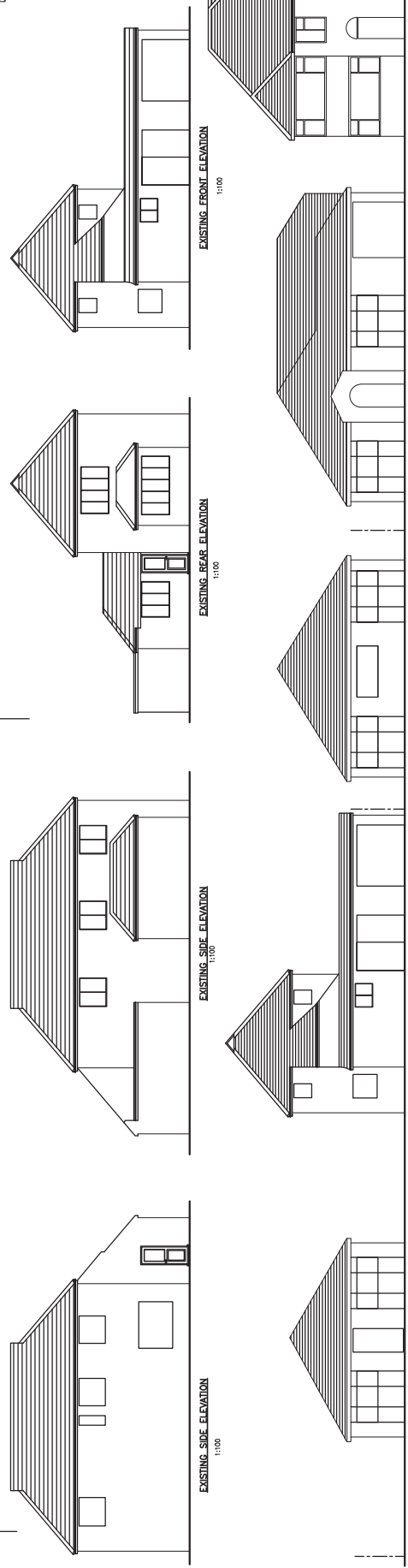
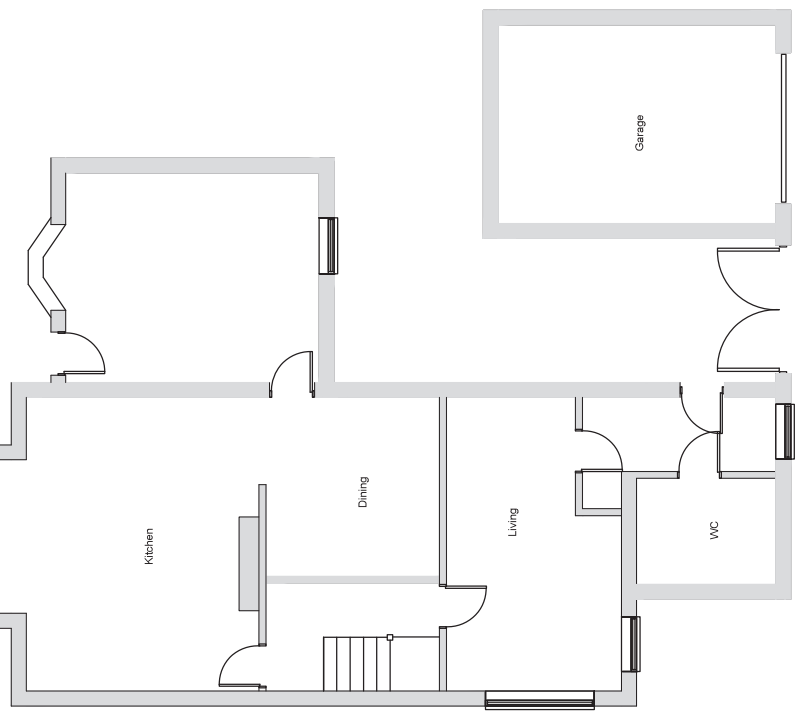
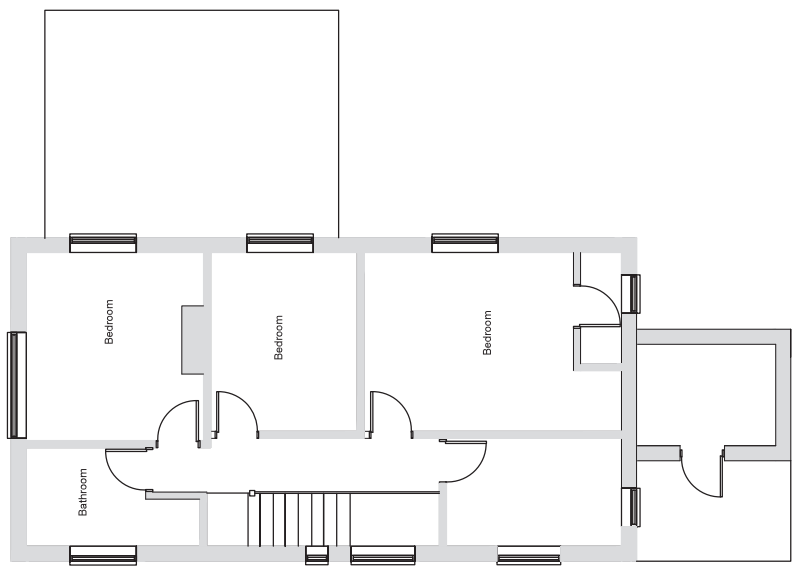
Project		Checked	
Drawn	KDA	Drawn	KDA
Scale	A1/A1:150	Date	JAN 2023
Stage	P	Draw No.	39PR/P200
		Rev	E

Project	
PROPOSED REPLACEMENT DWELLING	KDA Designs Ltd
35 ASHTONFIELD ROAD	Architectural & Structural Services
ICKENHAM	01753 811111
UB10 8LW	www.kdadesigns.co.uk
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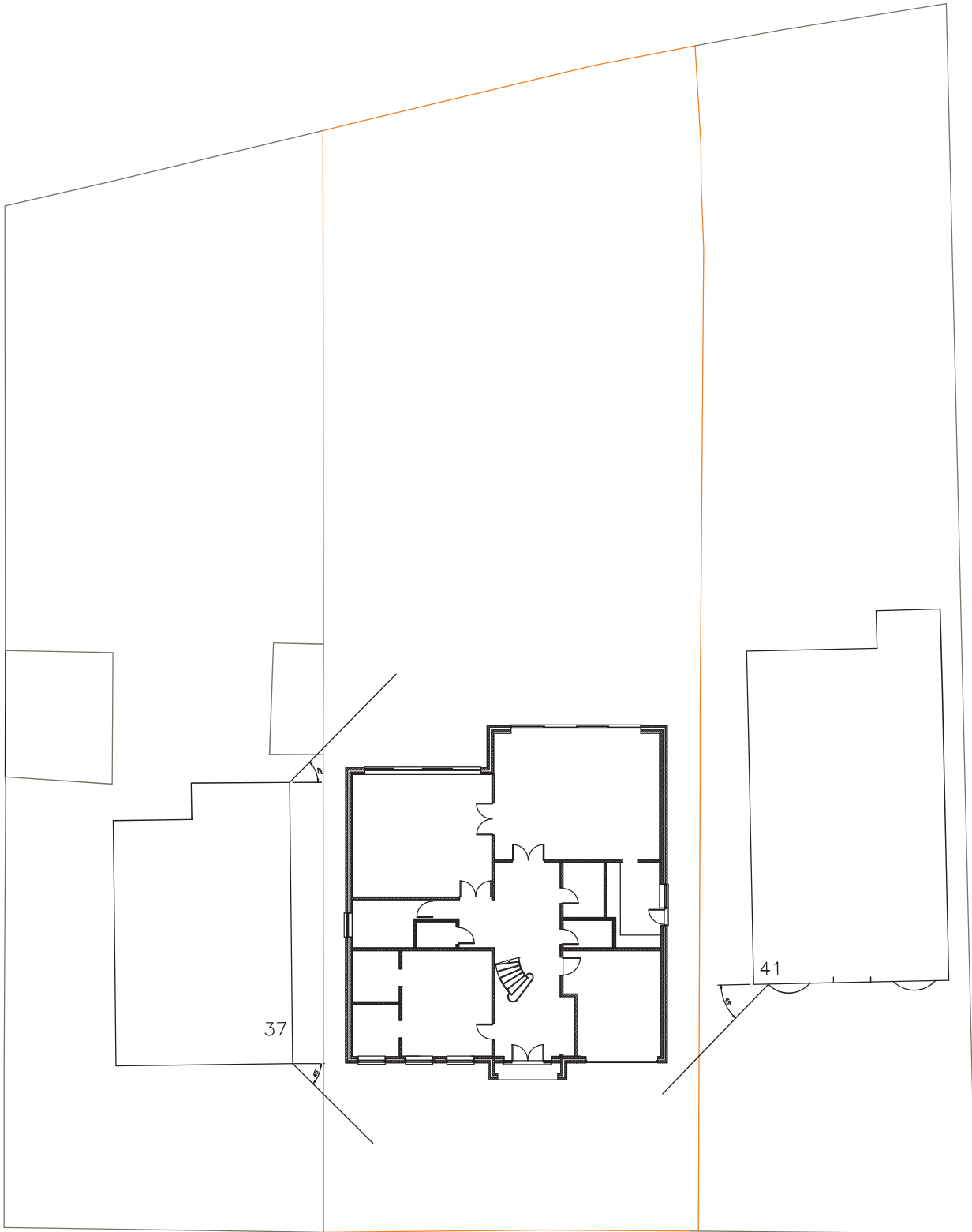
Drawing	
PROPOSED ELEVATIONS & SECTIONS	

SCALE 1:50

0 1 2 3 4 5 6 7 8 9 10



Project	PROPOSED REPLACEMENT DWELLING		Drawing	REVISIONS
	39 PARKFIELD ROAD			
Client	UBID ELW		Date	JAN 2023
	ICKENHAM			
Architectural & Structural Services	KDA Designs Ltd.	Kewington@kdaesigns.co.uk	Sheet No.	A101250 UNO
EXISTING PLANS & ELEVATIONS		B		



PROPOSED BLOCK PLAN
1:200

A	ISSUED FOR FINAL ASSESSMENT	16/5/23
Rev	Description	Date



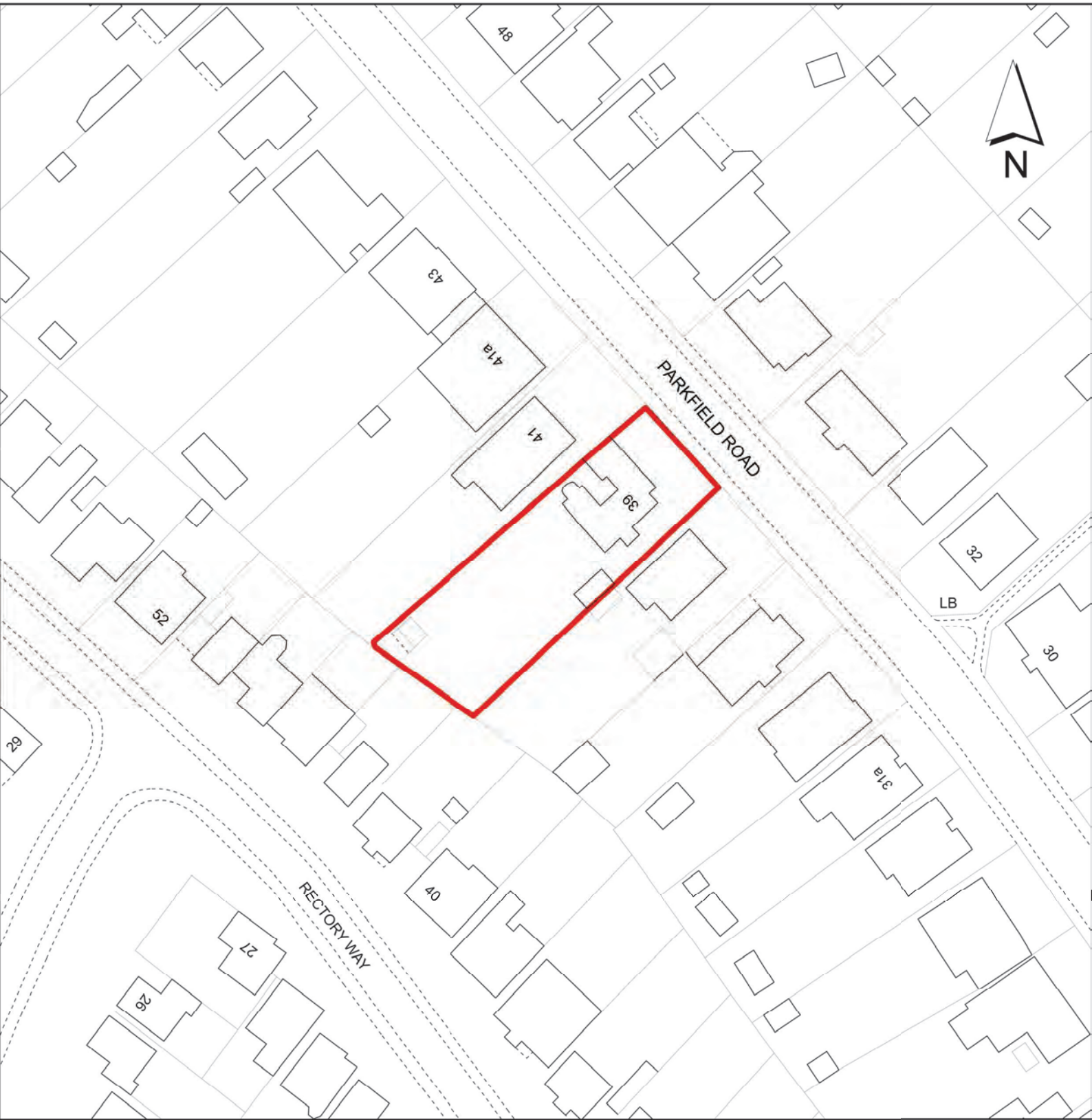
KDA Designs Ltd.
Architectural & Structural Services
kevin@kdadesigns.co.uk



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Project
**PROPOSED REPLACEMENT DWELLING
39 PARKFIELD ROAD
ICKENHAM
UB10 8LW**

Drawing
PROPOSED BLOCK PLAN

REVISIONS	
Scale	A3@1:50 UNO Date MAY 2023
Drg. No.	Rev
39PR/P400	A



<p>KEY :</p> <p> Site Boundary</p>	<p>ADDRESS :</p> <p>39 Parkfield Road, Ickenham</p>		<p>LONDON BOROUGH OF HILLINGDON</p> <p>RESIDENTS SERVICES PLANNING SECTION</p>
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	<p>PLANNING COMMITTEE :</p>	<p>DATE : 02/10/2024</p>	

Report of the Head of Development Management and Building Control

Address: 152 - 154 UXBRIDGE ROAD HAYES

Development: Erection of three storey mixed use retail and 9 residential apartments with ancillary parking, amendments to dropped kerbs, refuse and bicycle storage, following the demolition of existing buildings.

LBH Ref Nos: 4482/APP/2022/213

DS	Drawn	Checked	MAS	10.01.2022	MAS
IP	Drawn	Checked	MAS	05.03.2021	MAS
PS	Drawn	Checked	MAS	05.03.2021	MAS
Version	Description	Drawn	Checked		

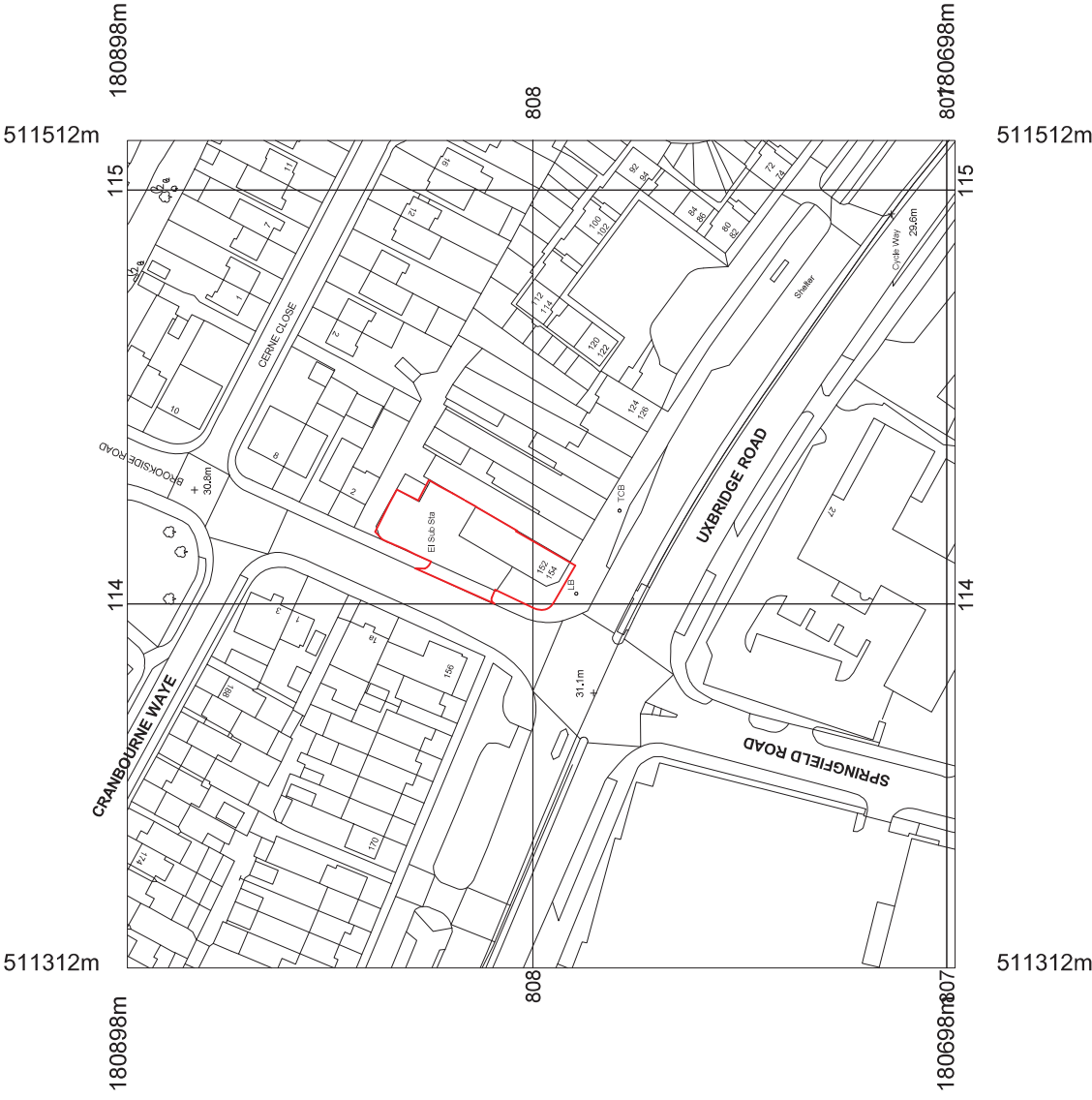
PROJECT NAME
 Kumarasamy Sivakumaran
 Visvanathan Yoganathan
 Ganesh Jayakumar
 JOB TITLE
 152-154 Uxbridge Road, Hayes

DRAWING TITLE
 Site Location Plan

SCALE
 As indicated @A3

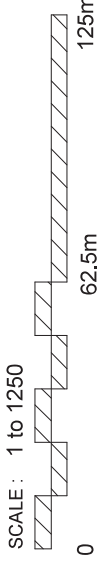
DATE
 5005

PROJECT NO
 101



Site Location Plan

1 : 1250



Note: This maybe a reduced sized print, check scales below



Red line denotes approximate application boundary
 "Ordnance Survey MasterMap Data purchased under 12 month license from
 Stanforbs (OS Partner license 10003540920 19/12/2022, Order license
 reference OI1436691, OI1436690

NOTES

All dimensions are to be checked on site by the contractor. In the event of any discrepancies refer immediately to Twickenham Surveys.

The accuracy and completeness of this survey is dependent on the accuracy of the information provided to us. We will have been matched to the client's original requirements. Twickenham Surveys accepts no responsibility or liability in their own or their contractors' liability to the extent of the original information provided to us.

The sketch of this survey was established for a brief requiring a 1:100 plan to be issued for the design of the building. The survey grid is not applied to Ordnance Survey National Grid.

Level values are indicated by a 'v' symbol. The survey did not include building underground services and equipment.

These points are representative of the site and are shown in blue. These points (where shown) should be treated with caution and expert identification is advised.

Level values shown on this site reflect reference to datum. Levels below and above are indicated as shown on drawing 201571.

Twickenham Surveys Ltd
 124 High Street
 Middlesex TW12 1JNS
 Tel: 020 8614 4480
 mail@twickenhamsurveys.co.uk
 www.twickenhamsurveys.co.uk



FIRST FLOOR FOOTPRINT

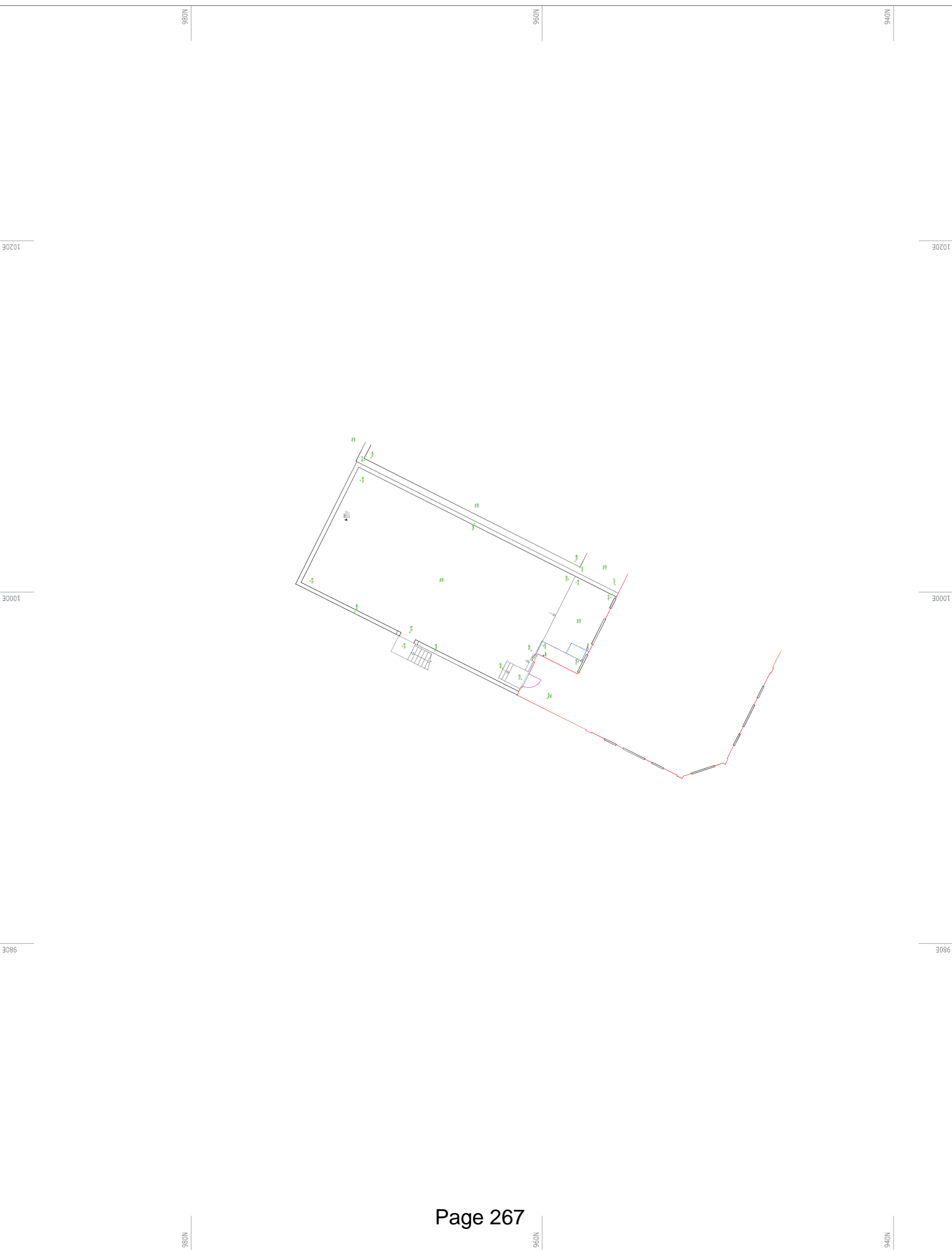
152-154 UXBRIDGE ROAD

HAYES

UB4

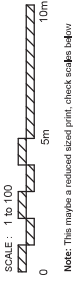
Drawing No. : 201571
 Scale : refer grid
 Drawn by : SC
 Checked by : JWH/D

TWICKENHAM SURVEYS
 LAND AND BUILDING SURVEYORS
 CENTRAL HOUSE
 124 HIGH STREET
 MIDDLESEX TW12 1JNS
 Tel: 020 8614 4480
 mail@twickenhamsurveys.co.uk
 www.twickenhamsurveys.co.uk



NOTE: Report all errors and omissions to the Architect. All dimensions to be checked on site before fabrication. Contractors must verify all dimensions on site before setting out, commencing work, or making any shop drawings.

- All dimensions must be checked on site
- All setting out to be checked on site
- The drawing must be read in conjunction with the relevant specification clauses
- This drawing must not be used for best transfer purposes



Note: This may be a reduced sized print, check scales below

- LEGEND**
- 2bed/Person Apartment
 - 3bed Apartment
 - Commercial Retail Unit
 - Communal Use Area

ACCOMMODATION SCHEDULE

Ground Level	Area	Area
R46 00.01 (2x49)	70m ² (753 Sq Ft)	15.4m ²
R46 00.02 (res)	222m ² (2,390 Sq Ft)	21m ²
First Level		
R46 01.01 (2x49)	70m ² (753 Sq Ft)	8.4m ²
R46 01.02 (2x49)	70m ² (753 Sq Ft)	8.4m ²
R46 01.03 (2x49)	70m ² (753 Sq Ft)	8.4m ²
R46 01.04 (2x49)	70m ² (753 Sq Ft)	8.4m ²
Second Level		
R46 02.01 (2x49)	70m ² (753 Sq Ft)	8.4m ²
R46 02.02 (2x49)	70m ² (753 Sq Ft)	8.4m ²
R46 02.03 (2x49)	70m ² (753 Sq Ft)	8.4m ²
R46 02.04 (2x49)	70m ² (753 Sq Ft)	8.4m ²
Third Level		
R46 03.01 (2x49)	70m ² (753 Sq Ft)	8.4m ²
R46 03.02 (2x49)	70m ² (753 Sq Ft)	8.4m ²
R46 03.03 (2x49)	70m ² (753 Sq Ft)	8.4m ²
R46 03.04 (2x49)	70m ² (753 Sq Ft)	8.4m ²
Total	2301m² (2,455 Sq Ft) - residential	230m² - other
Sub Total	2531m² (27,282 Sq Ft)	

Communal Amenity Gardens

Amenity Gardens

9 car parking spaces (1 per unit)

10 cycle spaces (1 space per unit)

Notes: Based upon 11 November 2015 survey. All dimensions are approximate. All dimensions are in meters. All dimensions are in meters. All dimensions are in meters.

Revision	Description	Date	Drawn	Checked
P1	Issue of preliminary drawings for approval	MAR 10 2017 20:24	MAS	
P2	Ref: Change Briefly dated 10/03/2017	MAR 10 2017 20:24	MAS	
P3	Issue of preliminary drawings for approval	MAR 10 2017 20:24	MAS	
P4	Issue of preliminary drawings for approval	MAR 10 2017 20:24	MAS	
P5	Issue of preliminary drawings for approval	MAR 10 2017 20:24	MAS	
P6	Issue of preliminary drawings for approval	MAR 10 2017 20:24	MAS	
P7	Issue of preliminary drawings for approval	MAR 10 2017 20:24	MAS	

Client Name: Kumarasamy Sivakumaran
Visvanathan Voganathan
Ganesh Jeyakumar

152-154 Uxbridge Road, Hayes

Proposed Ground Floor Plan

Scale: 1 : 100 @A1

Drawn: MAS
Checked: MAS
Date: 10/03/2017

Project No: 5005
Revision: P7

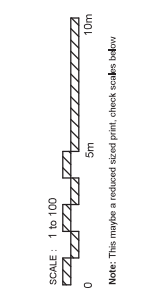
Submission: PLANNING SUBMISSION P7

The Old Cozen House
 81a Victoria Road
 Farnborough
 Hampshire,
 GU14 7PP

T: +44 (0)1252 643298
 M: +44 (0)777 208 9393
 E: mike@ms4architects.co.uk



NOTE: Report all errors and omissions to the Architect. All dimensions to be checked on site before fabrication.
 Contractors must verify all dimensions on site before setting out, commencing work, or making any shop drawings.
 - All dimensions must be checked on site
 - All setting out to be checked on site
 - This drawing must be read in conjunction with the relevant specification sheets.
 - This drawing must not be used for hand transfer purposes



Note: This map is a reduced sized print, check scale below

LEGEND

- 2-Bed/3-Bed Apartment
- 3-Bed Apartment
- Commenced Plot Unit
- Communal Facilities

ACCOMMODATION SCHEDULE

Standard Level	Area	Area
PH 01 (03 SqM)	70m ² (703 SqM)	15.5m ²
PH 02 (02 (res))	222m ² (2,280 SqM)	21m ²
Plot Level		
PH 01 (03 SqM)	70m ² (703 SqM)	9.4m ²
PH 02 (02 (res))	70m ² (703 SqM)	9.4m ²
PH 03 (03 SqM)	70m ² (703 SqM)	6.5m ²
PH 04 (04 (res))	81m ² (815 SqM)	5.4m ²
Street Level		
PH 01 (03 SqM)	70m ² (703 SqM)	5.4m ²
PH 02 (02 (res))	70m ² (703 SqM)	5.4m ²
PH 03 (03 SqM)	70m ² (703 SqM)	1.6m ²
PH 04 (04 (res))	81m ² (815 SqM)	4.9m ²
Total Level		
Communal Primary Gardens	77m ² (756 SqM) + 232m ² (2,319 SqM)	
Site Total	936m ² (10,083 Sq. Ft.)	

Total
 936m² (10,083 Sq. Ft.)
 9 car parking spaces (1 per unit)
 10 cycle spaces (1 space per unit)

Notes: Based upon Trencher Survey topographic survey information including (Orig Ref 2012/2) & Elevations (Orig Ref 2012/2).

Item No.	Description	MAS	Rev	Date
P8	Initial Submission	MAS	01.07.2024	MAS
P7	Design developed Council Design Ref. Case File No. 15102/2024	MAS	06.07.2023	MAS
P6	Design developed Council Design Ref. Case File No. 15102/2024	MAS	15.03.2022	MAS
P5	Planning Submission 01	MAS	12.12.2021	MAS
P4	Planning Submission 02	MAS	22.11.2021	MAS
P3	Planning Submission 03	MAS	11.02.2021	MAS
P2	Site plan for planning submission	MAS	18.02.2020	MAS
P1	Initial Submission	MAS	18.02.2020	MAS

Client Name:
 Kumarasamy Sivakumaran
 Viswanathan Yoganathan
 Ganesh Jeyakumar

Job Title:
 152-154 Uxbridge Road, Hayes

Proposed Second Floor Plan

Scale:
 1 : 100 @A1

Site No:
 5005

Drawing No:
 202

Status:
 PLANNING SUBMISSION P8

Author:
 MS4

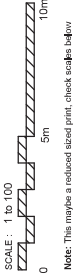
Contact:
 Avenue Business Centre
 Abbey Mill
 102 Farnborough Road
 Farnborough
 Hampshire
 GU14 7WA
 Tel: +44 (0)1253 422433
 Fax: +44 (0)1253 508383
 Email: info@ms4architects.co.uk



02_Second Level (Lower)
 1 : 100

NOTE: Report all errors and omissions to the Architect. All dimensions to be checked on site before fabrication. Contractors must verify all dimensions on site before setting out, commencing work, or making any shop drawings.

- All dimensions must be checked on site
- All setting out to be checked on site
- The drawing must be read in conjunction with the relevant specification clause.
- This drawing must not be used for permit transfer purposes



Note: This may be a reduced sized print, check scales below

LEGEND

- 2-Bed/3-Bed/4-Bed Apartment
- 3-Bed Apartment
- Commercial Retail Unit
- Commercial Use/Office

ACCOMMODATION SCHEDULE

Standard Level	Area (m ²)	Area (sq ft)
1st Floor (1000)	70m ² (753 sq ft)	18,5m ² (201 sq ft)
2nd Floor (1000)	22m ² (238 sq ft)	2m ² (21 sq ft)
FLOOR TOTAL	92m² (991 sq ft)	20m² (216 sq ft)
3rd Floor (1000)	70m ² (753 sq ft)	18,5m ² (201 sq ft)
4th Floor (1000)	70m ² (753 sq ft)	18,5m ² (201 sq ft)
5th Floor (1000)	70m ² (753 sq ft)	18,5m ² (201 sq ft)
6th Floor (1000)	70m ² (753 sq ft)	18,5m ² (201 sq ft)
7th Floor (1000)	70m ² (753 sq ft)	18,5m ² (201 sq ft)
8th Floor (1000)	70m ² (753 sq ft)	18,5m ² (201 sq ft)
9th Floor (1000)	70m ² (753 sq ft)	18,5m ² (201 sq ft)
10th Floor (1000)	70m ² (753 sq ft)	18,5m ² (201 sq ft)
Grand Total	700m² (7530 sq ft)	185m² (2010 sq ft)

Commercial Amenity Gardens

Site Area: 17,546 Sqm (188,000 Sq Ft)

Total: 94m² (1,014 Sq Ft)

Additional Details:

9 car parking spaces (1 per unit)

10 cycle spaces (1 space per unit)

MS4: Based upon Topographical Survey, Topographic Survey information including (Orig Ref 2017/21.5) & Elevations (Orig Ref 2017/21).

Revision	Description	Drawn	Checkd
P1	Issue for tender (10/07/2024)	MAS	10/07/2024
P2	Issue for tender (10/07/2024)	MAS	10/07/2024
P3	Issue for tender (10/07/2024)	MAS	10/07/2024
P4	Issue for tender (10/07/2024)	MAS	10/07/2024
P5	Issue for tender (10/07/2024)	MAS	10/07/2024
P6	Issue for tender (10/07/2024)	MAS	10/07/2024
P7	Issue for tender (10/07/2024)	MAS	10/07/2024

Kumarasamy Sivakumaran
Visvanathan Yoganathan
Ganesh Jeyakumar

152-154 Uxbridge Road, Hayes

Proposed Roof Floor Plan

Scale: 1 : 100 @A1

Drawn: MAS
 Date: 10/07/2024

Checked: MAS
 Date: 10/07/2024

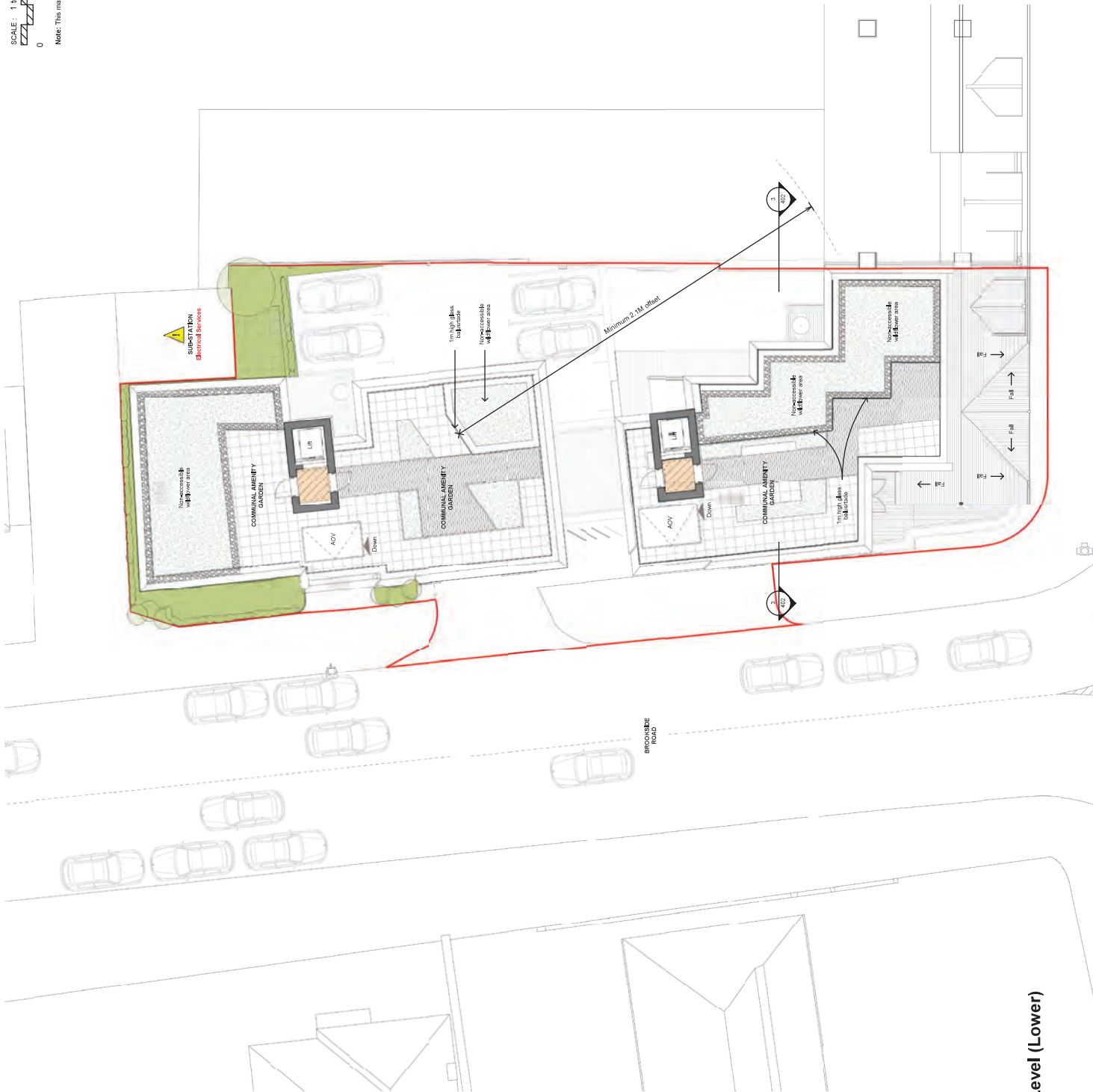
Project No: 5005

Submission: PLANNING SUBMISSION P7

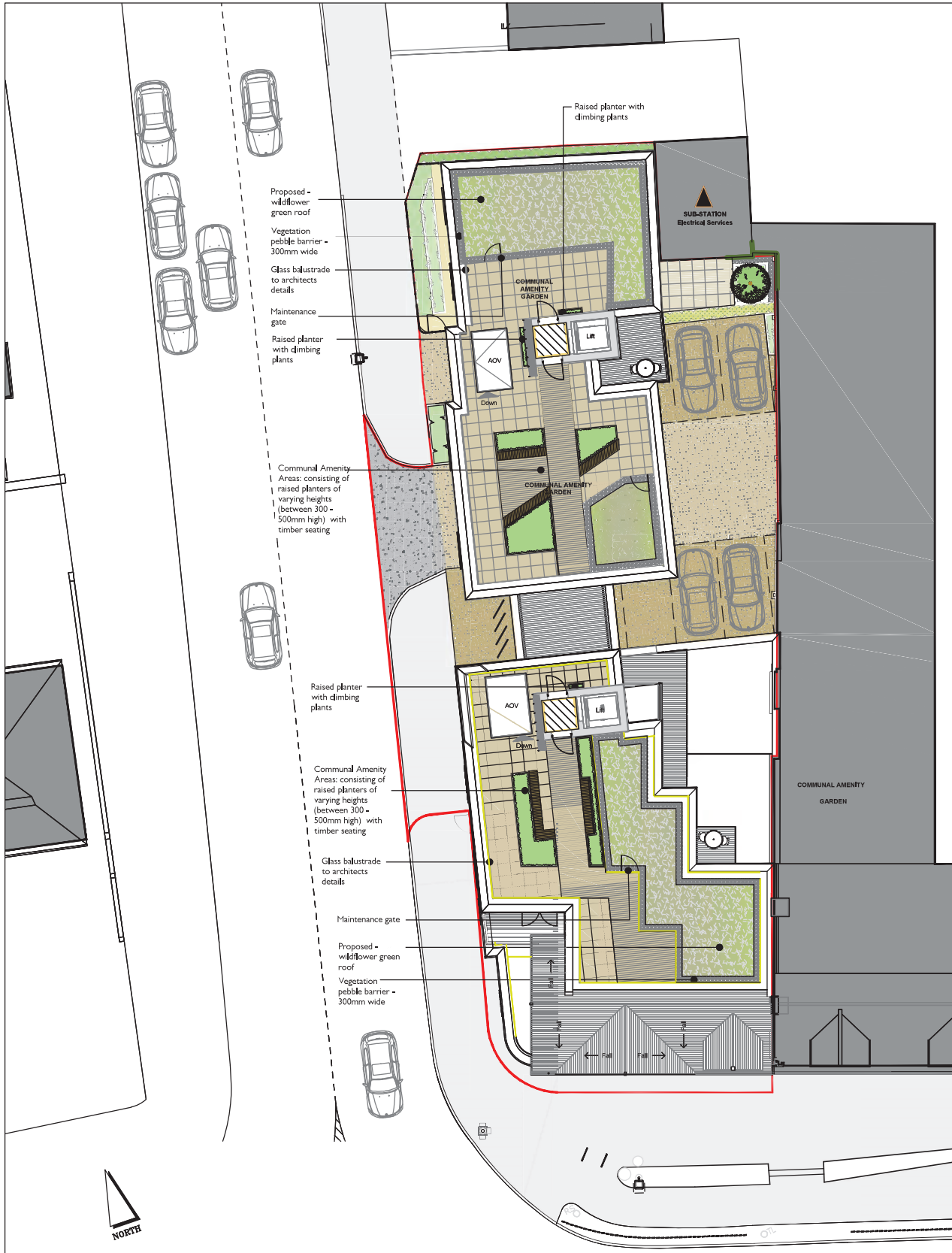


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03_Third Level (Lower)
 1 : 100



NOTES:
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 Measurements: All figures are dimensions and are to be taken from this drawing. All measurements must rest on the ground level unless otherwise stated. All dimensions shall be rounded to the nearest millimetre.
 Utilities: Highways (overhead cables) are the responsibility of the client or their contractor. It must be checked with the relevant utility companies and the Government Agency and to ensure that all approvals are in place. It is the responsibility of the contractor to locate all underground services prior to construction.
 Drawings: All drawings and details to be checked on site prior to the construction works commencing on site. All drawings shall be used in conjunction with the Landscape Specifications.
 Discrepancies: Any discrepancy on the drawings shall be referred to the CA immediately.

- LEGEND**
- SOFT LANDSCAPE WORKS**
- Proposed small feature tree / large shrub - mid-stem species (on ground floor)
 - Proposed hedgerow/ wall shrubs planting (on ground floor)
 - Proposed structure shrub planting (on ground floor)
 - Proposed ornamental shrub & groundcover planting (on ground floor)
 - Proposed clipped hedgerow planting (on ground floor)
 - Proposed clipped instant hedgerow planting (on ground floor)
 - Proposed Raised Planters: varying heights (300-500mm high) with mixed ornamental shrub & herbaceous planting
 - Proposed Wildflower Green Roof

- HARD LANDSCAPE WORKS**
- Macadam areas: suitable for pedestrian and vehicular use
 - Access drives: Porous macadam areas: suitable for pedestrian and vehicular use
 - Private Amenity Areas: (on ground floor) Paving: 600x600 Paving Panels or Toleron or similar square: Colour: random non-slip surface tiles installed in accordance with the manufacturer's instructions
 - Access / Turning Area: (on ground floor) Paving: 600x600 Paving Panels or Toleron or similar square: Colour: TBC, installed in accordance with the manufacturer's instructions
 - Car Parking Bays: (on ground floor) Paving: 600x600 Paving Panels or Toleron or similar square: Colour: TBC, installed in accordance with the manufacturer's instructions
 - Communal Amenity Areas: Paving external areas: suitable for pedestrian use: Colour: the tiles selected tiles installed in accordance with the manufacturer's instructions
 - Communal Amenity Areas: Timber decking: 40mm x 150mm supplied by Outdoor Deck Company or similar equivalent: Colour: A forward second fixing system installed in accordance with the manufacturer's instructions
 - Raised Planters: 1000x1000x100 GRP planters of varying sizes & heights 300/500mm high supplied by Europlanters or similar equivalent
 - Benches: timber 4000x150mm supplied by Europlanters or similar equivalent
 - Vegetation Pebble Barrier: 300mm wide
 - Site Boundary

NOTES
Hard landscape details: refer to Architects and Engineers details. For details on retaining structures, please refer to engineers / architects details.
Surface drainage: to be provided by engineers.
Surface materials: to be installed in accordance with the manufacturer's instructions. Laid to falls.
For details on retaining structures: refer to Engineer's details.
Raised GRP Planters:
 All raised planters: Water reservoir & filler pipe to be installed.
Plant Irrigation: Mona Plantava Tanks: sizes and numbers to be confirmed; supplied by Green+tech Ltd
 Lightweight Topsoil to all planter subject to confirmation of weight and drainage by engineer: go: Growrite Roof Garden Compost: Drainage: 100mm (40mm) - 100mm deep. All supplied by Green+tech Ltd or similar approved (www.green+tech.co.uk).

PLANNING

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 LANDSCAPE ARCHITECT

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Project
 152-154 UXBRIDGER ROAD, HAYES,
 MIDDLESEX, UB4 0JH

Client
 Kumarasamy Sivakumar, Visvanathan Yoganathan and
 Ganesh Jayaraman

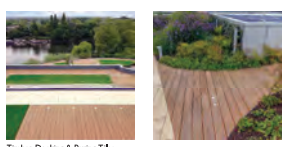
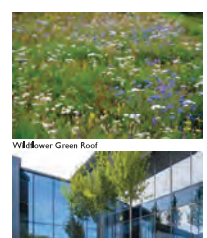
Drawing Title
Roof Garden Areas

Scale Date Drawn
 1:100 @ A1 December 2021 MS

Drawing No. Status Revision
 887-14-102 PLANNING C

- Preparation
- Based for Client/Revision
- Based for Planning Approval
- Based for Tender
- Based for Construction
- As B.C.

TYPICAL IMAGES FOR THE HARD & SOFT PALETTE FOR ROOF GARDEN



NOTE: Report all errors and omissions to the Architect. All dimensions to be checked on site before fabrication. Contractors must verify all dimensions on site before setting out, commencing work, or making any shop drawings. All dimensions must be checked on site. All setting out to be checked on site. This drawing must be used in conjunction with the relevant specifications. This drawing must not be used for third transfer purposes.

MATERIALS LEGEND

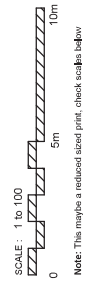
- 1 Facing brickwork • Red (to match existing)
- 2 Glazed Brick • Grey/Blue
- 3 Zinc cladding • Grey
- 4 Aluminium windows • White
- 5 Aluminium window doors • Grey
- 6 Concrete mezzanine floor roof tiles • Brown
- 7 Pressed aluminium coping • Grey
- 8 Clazed balustrade with stainless steel handrail
- 9 Metal balustrade • Black
- 10 PVCu gutters & coverings • Black
- 11 Orange flashing
- 12 Colours Steel • Polaris Screens
- 13 Cladding - to match neighbouring domers
- 14 Render • White
- 15 Metal cladding panels • White



Front Elevation
1 : 100



Side Elevation (Left)
1 : 100



Note: This maybe a reduced sized print, check scales before

Revision	Description	Date	Checked
P8	Final design and construction details	15/07/2024	MAS
P7	Design developed (Council Energy)	06/07/2023	MAS
P6	Final design and construction details	15/03/2022	MAS
P5	Final design and construction details	12/12/2021	MAS
P4	Final design and construction details	11/12/2021	MAS
P3	Final design and construction details	11/02/2021	MAS
P2	Final design and construction details	11/02/2021	MAS
P1	Final design and construction details	11/02/2021	MAS

CLIENT NAME
Kumarasamy Sivakumaran
Viswanathan Yoganathan
Ganesh Jeyakumar

JOB TITLE
152-154 Uxbridge Road, Hayes

DRAWING TITLE
Proposed Elevations - Sheet 1

SCALE
1 : 100 @A1

JOB NO
5005

STATUS
DRAWING FOR
PLANNING SUBMISSION P8

MS4
Avenue Business Centre
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282 Farborough Road
Farborough
Hampshire
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E: info@ms4architects.co.uk

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MATERIALS LEGEND

- 1 Facing brickwork - Red (to match existing)
- 2 Glazed Brick - Charvillat
- 3 Zinc cladding - Grey
- 4 Aluminium windows - White
- 5 Aluminium window doors - Grey
- 6 Concrete metalworking plan roof tiles - Brown
- 7 Pressed aluminium coping - Grey
- 8 Glazed balustrade with stainless steel handrail
- 9 Metal balustrade - Black
- 10 PVC/Glass & composite - Black
- 11 Orange floor
- 12 Cream Small Pattern Screens
- 13 Cladding - to match neighbouring corners
- 14 Render - White
- 15 Metal cladding panels - White



Rear Elevation
1 : 100

Revision	Description	Date	Checked
P8	Design approved Council Energy comments. Issue 1 10/07/2024	MAS	10/07/2024
P7	Design approved Council Energy comments. Issue 1 10/07/2024	MAS	10/07/2024
P6	Finalised Submittals 0.01	MAS	10/01/2022
P5	Finalised Submittals 0.01	MAS	12/12/2021
P4	Finalised Submittals 0.01	MAS	22/11/2021
P3	Finalised Submittals 0.01	MAS	11/02/2021
P2	Revised floor plan & WCC E-worksheets for client approval	MAS	10/02/2021
P1	Issue for client approval	MAS	10/02/2021

CLIENT NAME
**Kumarasamy Sivakumaran
Viswanathan Yoganathan
Ganesh Jeyakumar**

JOB TITLE
152-154 Uxbridge Road, Hayes

DRAWING TITLE
Proposed Elevations - Sheet 2

SCALE
1 : 100 @A1

JOB NO
5005

DRAWING NO
401

STATUS
PLANNING SUBMISSION P8

MS4
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Hampshire
GU14 7NA
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M: +44 (0)777 5061300
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Side Elevation (Right)
1 : 100

SCALE : 1 to 100
0 5m 10m
Note: This maybe a reduced sized print, check scales before

NOTE: Report all errors and omissions to the Architect. All dimensions to be checked on site before fabrication. Contractors must verify all dimensions on site before setting out, commencing work, or making any shop drawings. All dimensions must be checked on site. All setting out to be checked on site. This drawing must be used in conjunction with the relevant specification sheets. This drawing must not be used for final transfer purposes.

MATERIALS LEGEND

- 1 Facing brickwork - Red (to match existing)
- 2 Glazed Brick - Clay/Blue
- 3 Zinc cladding - Grey
- 4 Aluminium windows - White
- 5 Aluminium window doors - Grey
- 6 Concrete mezzanine floor roof tiles - Brown
- 7 Pressed aluminium coping - Grey
- 8 Coloured balustrade with stainless steel handrail
- 9 Metal balustrade - Black
- 10 PVCU gutters & downpipes - Black
- 11 Grapes flooring
- 12 Cream Steel - Polished Stainless
- 13 Cladding - to match neighbouring domains
- 14 Render - White
- 15 Mezz flooring panels - White



Internal Courtyard Elevation (South)
1 : 100



Internal Courtyard Elevation (North)
1 : 100

Revision	Description	Drawn	Date	Checked
P2	Final design	MAS	12.02.2024	MAS
P1	Final design	MAS	12.02.2024	MAS

Client Name
Kumarasamy Sivakumaran
Viswanathan Yogannathan
Ganesh Jeyakumar

Job Title
152-154 Uxbridge Road, Hayes

Proposed Elevations - Sheet 3

Scale
1 : 100 @A1

Job No
5005

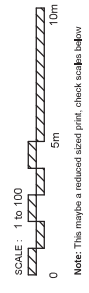
Drawing No
402

STATUS
PLANNING SUBMISSION P2

MS4
Avenue Business Centre
Abbey Mill
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Farnborough
Hampshire
GU14 7NA
Tel: +44 (0)1256 402 411
Fax: +44 (0)1256 402 412
info@ms4architects.co.uk



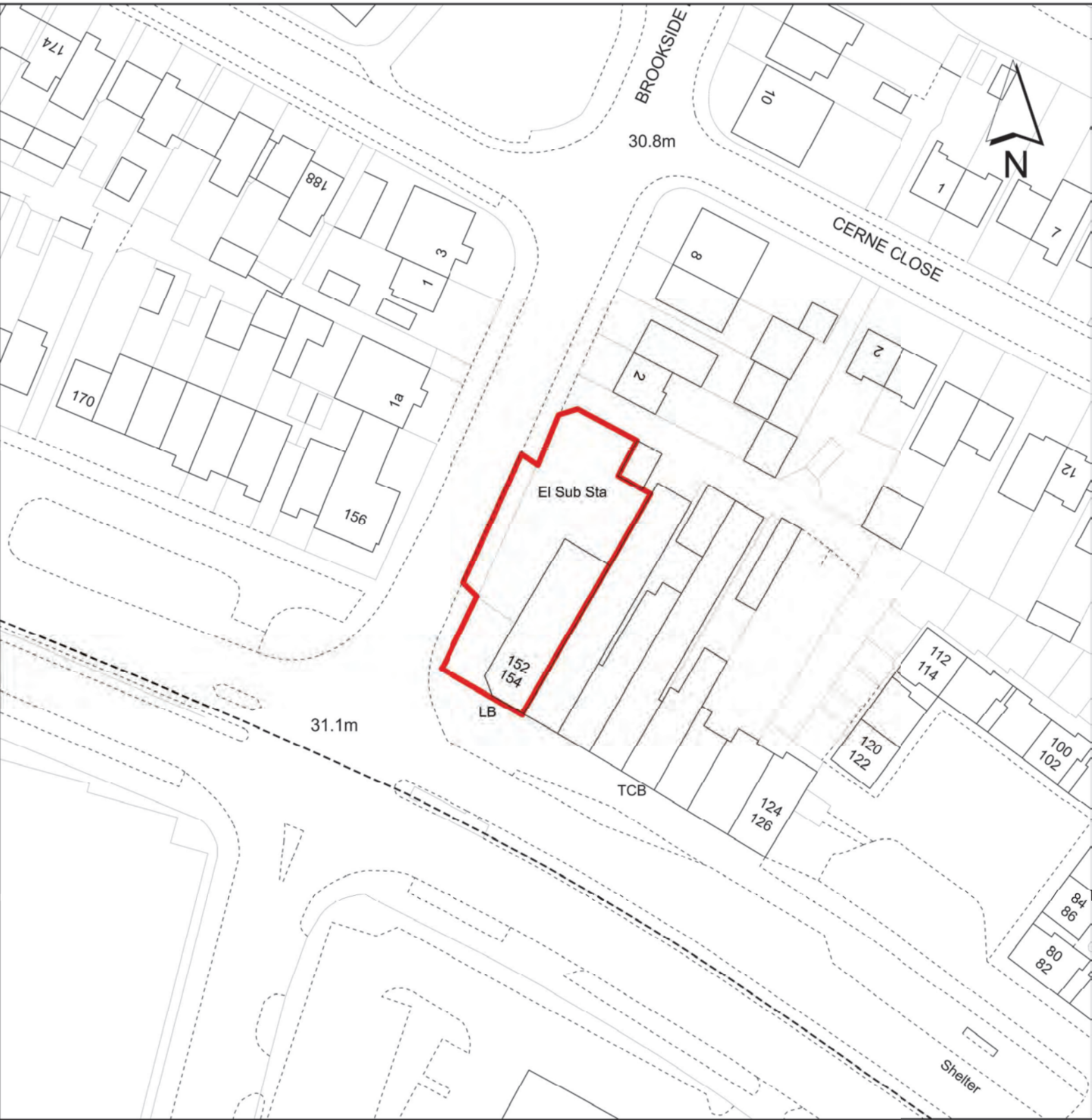
Perspective View - Southern Roof Garden





Note: This maybe a reduced sized print, check scales before



Section X - X
1 : 100

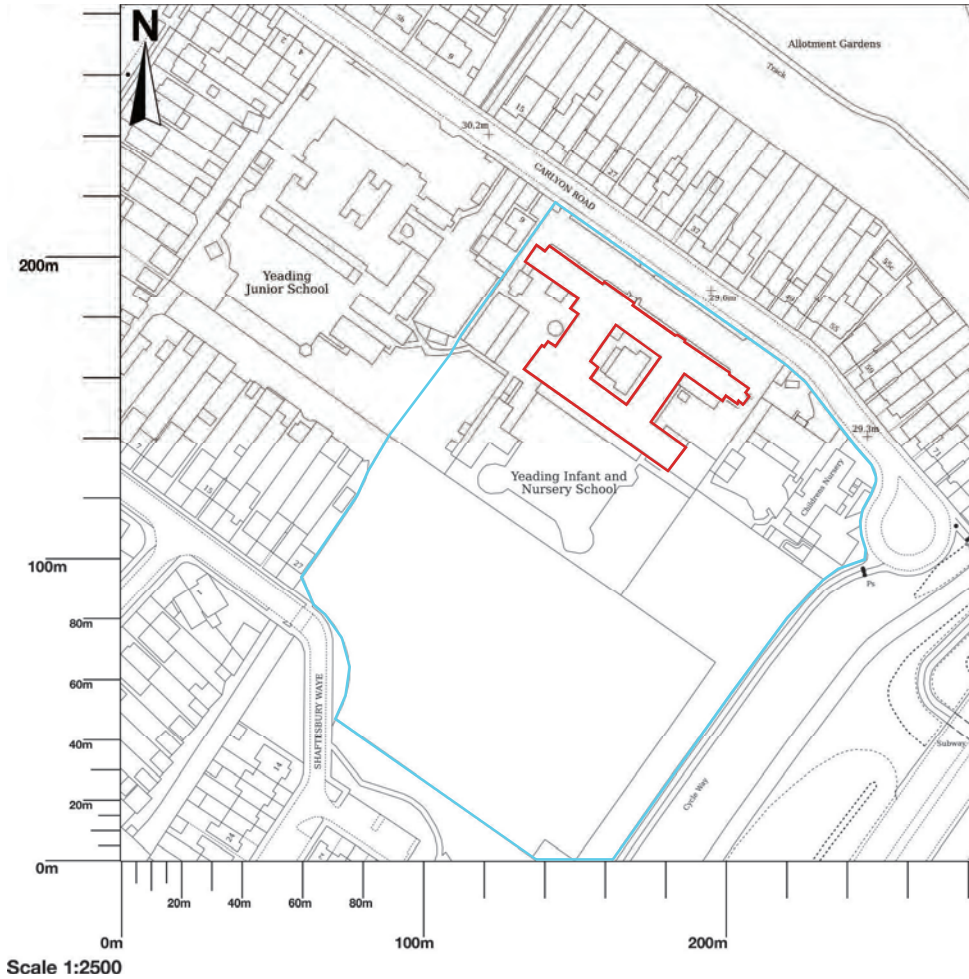


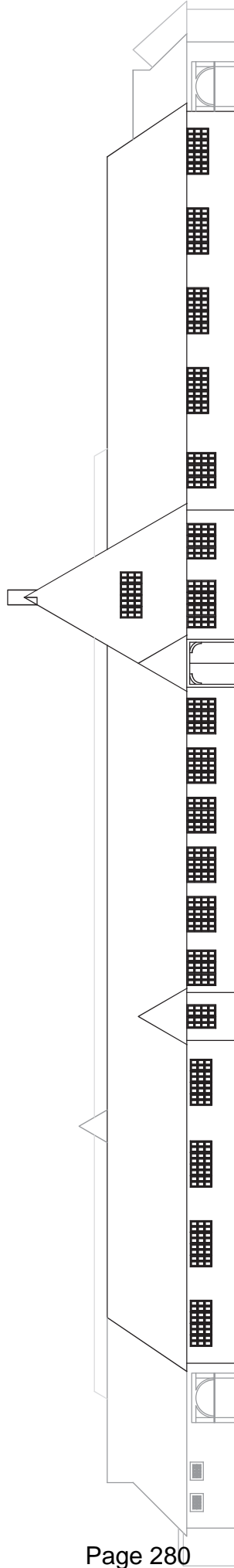
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<p>DISCLAIMER :</p> <p>For identification purposes only. This copy has been made by or with the authority of the Head of Committee Services pursuant to section 47 of the Copyright, Designs and Patents Act 1988 (the Act). Unless the Act provides a relevant exception to copyright.</p> <p>© Crown copyright and database rights 2024 Ordnance Survey AC0000810857</p>	<p>PLANNING APPLICATION REFERENCE :</p> <p>4482/APP/2022/213</p>	<p>SCALE :</p> <p>1:1,250</p>	<p>CIVIC CENTRE, UXBRIDGE, UB8 1UW</p> 
<p>PLANNING COMMITTEE :</p>		<p>DATE : 02/10/2024</p>	

Report of the Head of Development Management and Building Control

Address: YEADING INFANT SCHOOL CARLYON ROAD HAYES
Development: Installation of solar panels on the school roofs.
LBH Ref Nos: 17997/APP/2024/1610

Yeading Infant



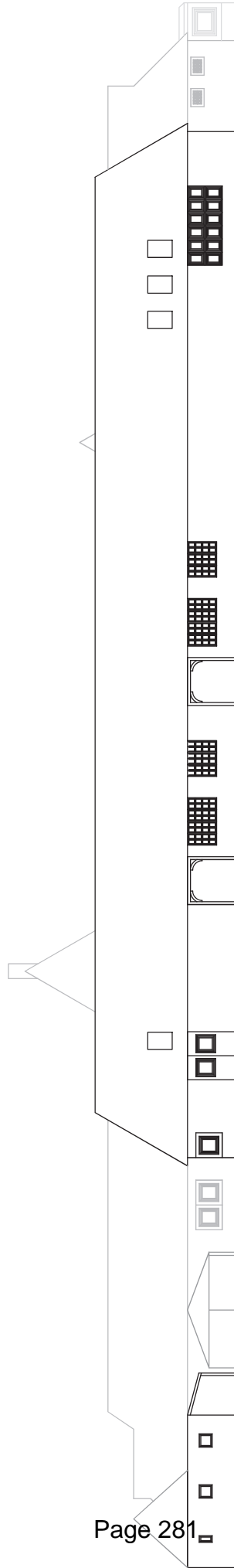


solar for schools

DWG: 2

Project Name
Yealings Infant and Nursery School - Northern Elevation Plan

Scale
1:250



solar for schools

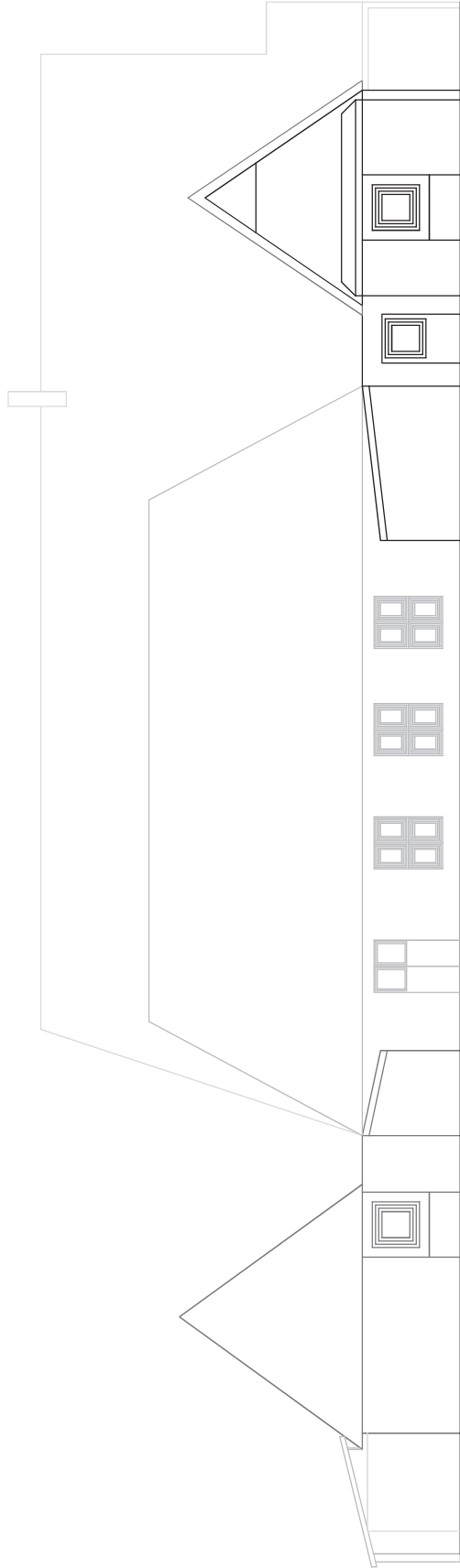
DWG: 2



Project Name
Yeading Infant and Nursery School - Southern Elevation Plan

Scale

1:250



solar for schools

DWG: 2

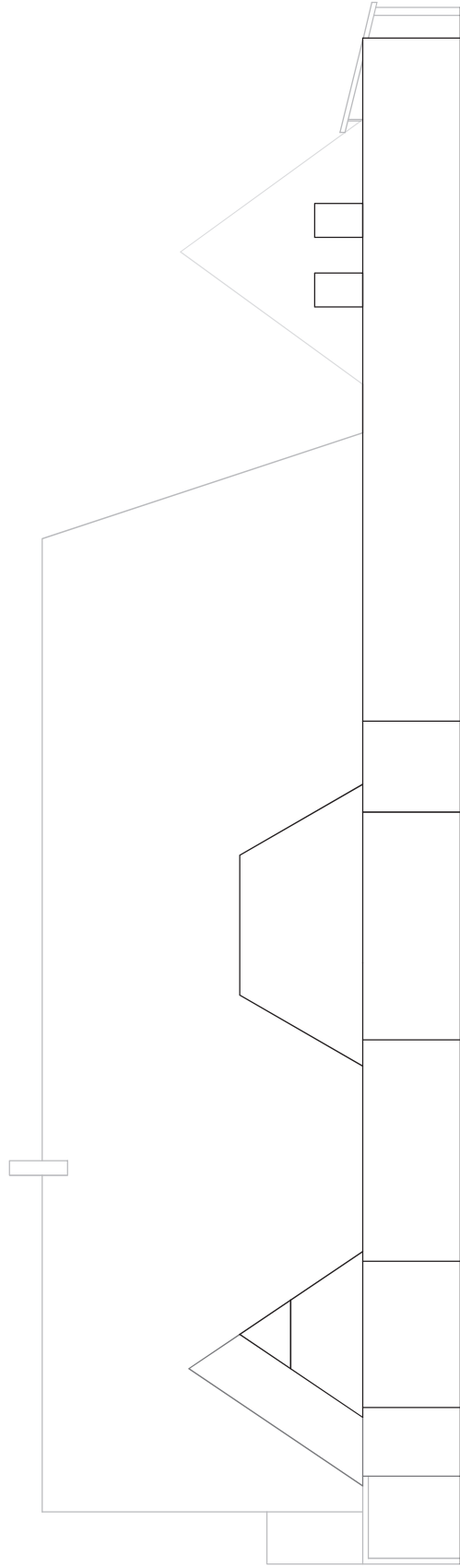


Project Name

Yeendings Infant and Nursery School - Eastern Elevation Plan

Scale

1:150



solar for schools

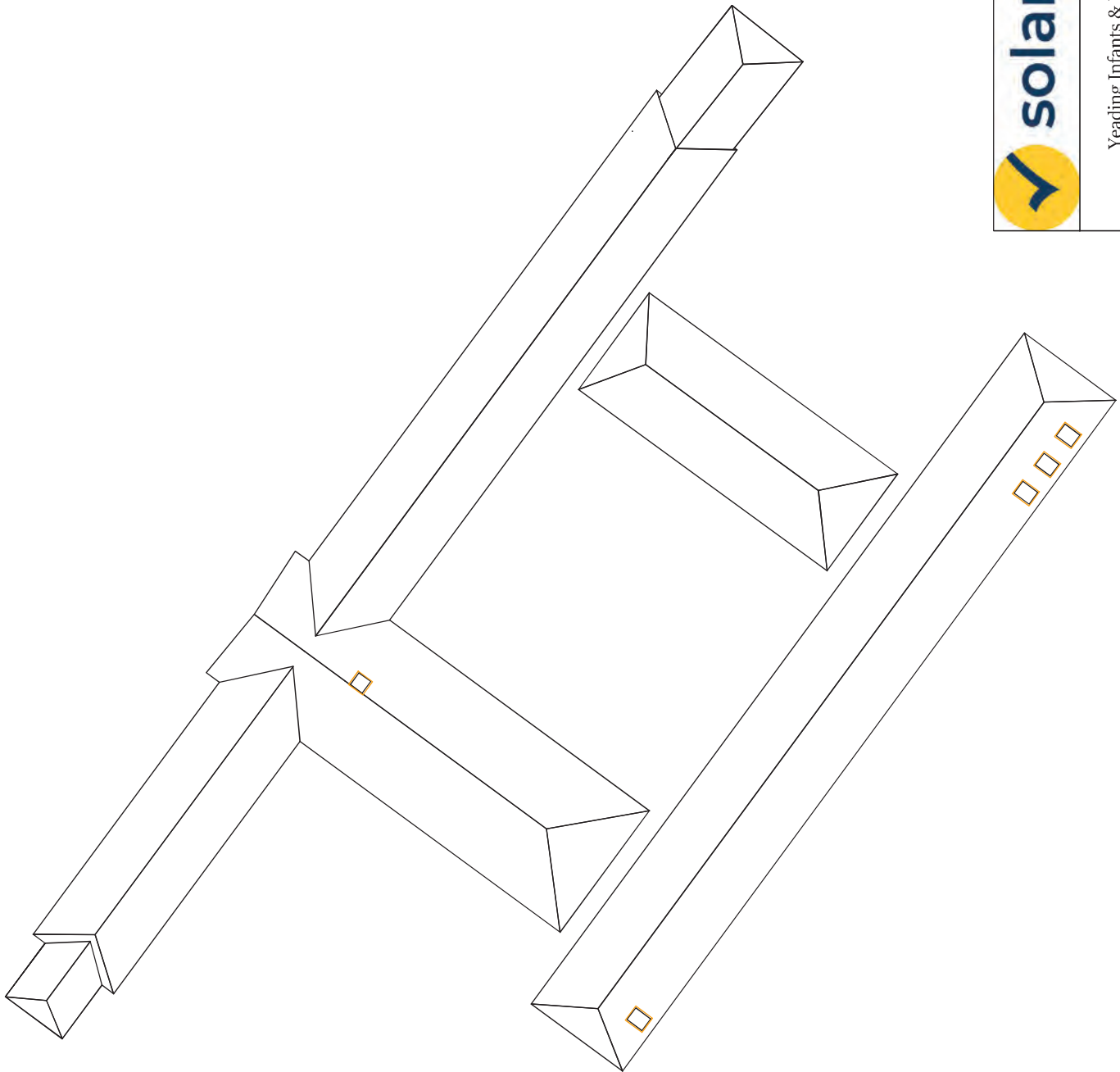
DWG: 2

Project Name
Yealings Infant and Nursery School - Western Elevation Plan

Scale

1:150

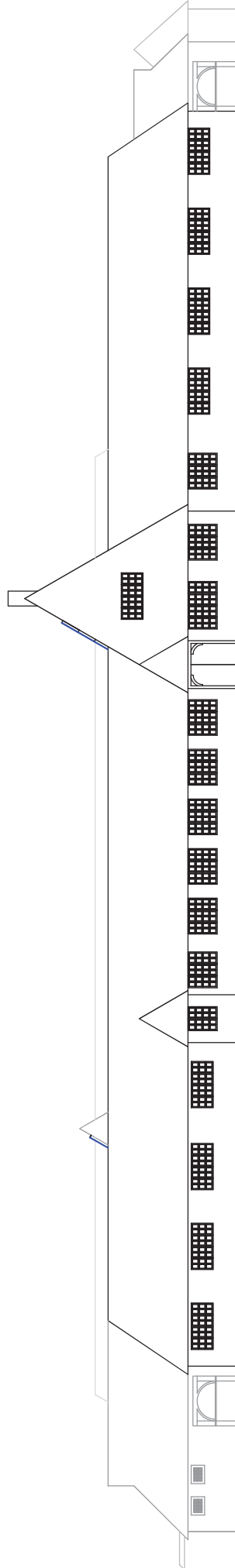




Yeading Infants & Nursery School - Existing Roof Plan

Scale 1:300 All dimensions in mm

DWG: 3



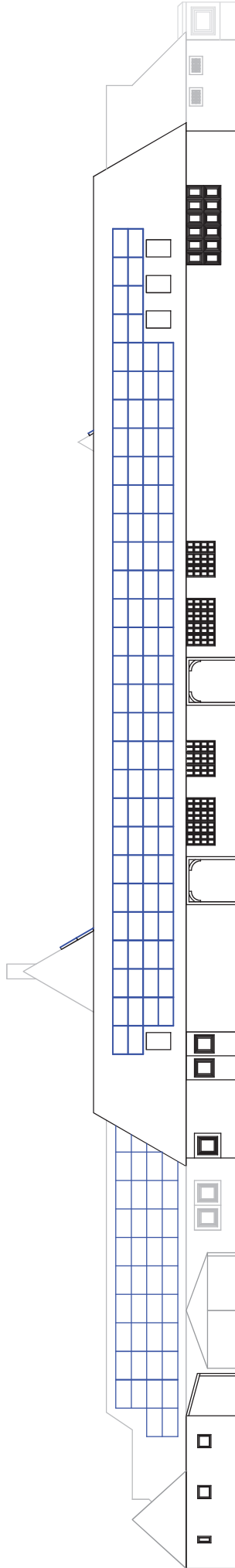
DWG: 1

solar for schools

Project Name
Yealings Infant and Nursery School - Northern Elevation Plan

Scale

1:250



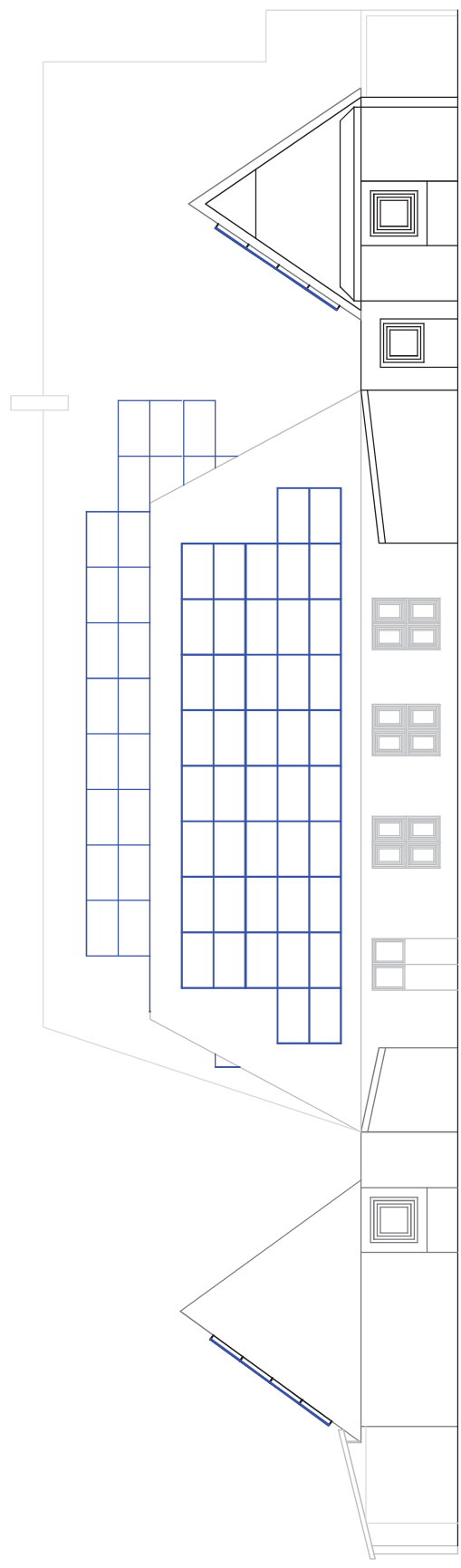
solar for schools


DWG: 1

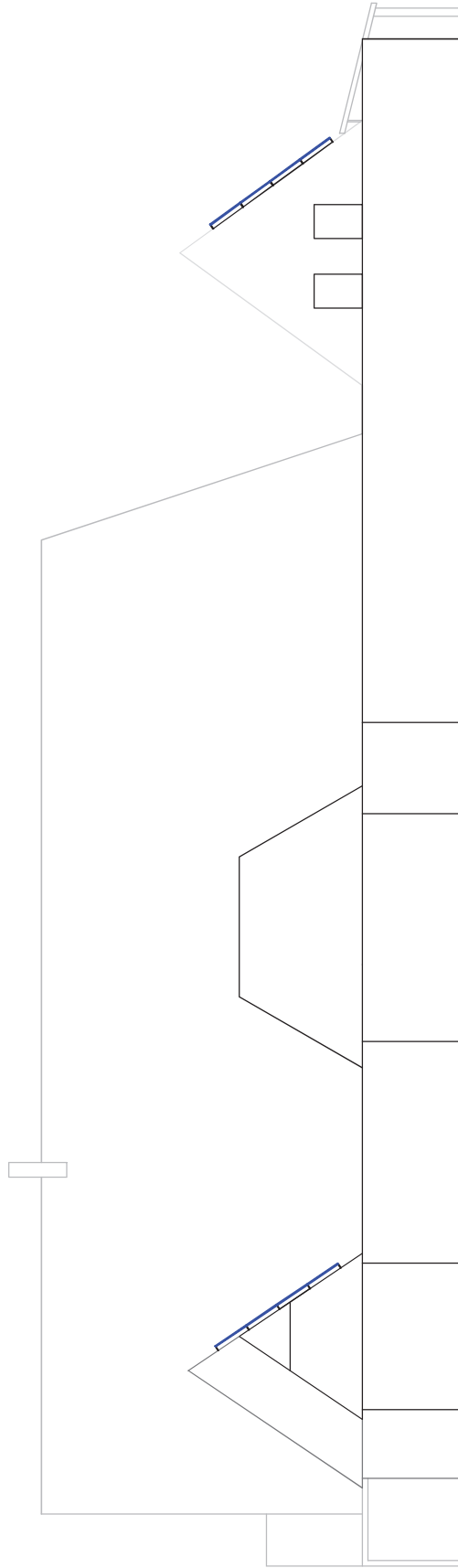


Project Name
Yeading Infant and Nursery School - Southern Elevation Plan

Scale
1:250



		DWG: 1	
Project Name		Yeendings Infant and Nursery School - Eastern Elevation Plan	
Scale		1:150	



solar for schools

DWG: 1

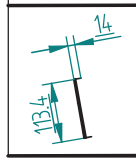
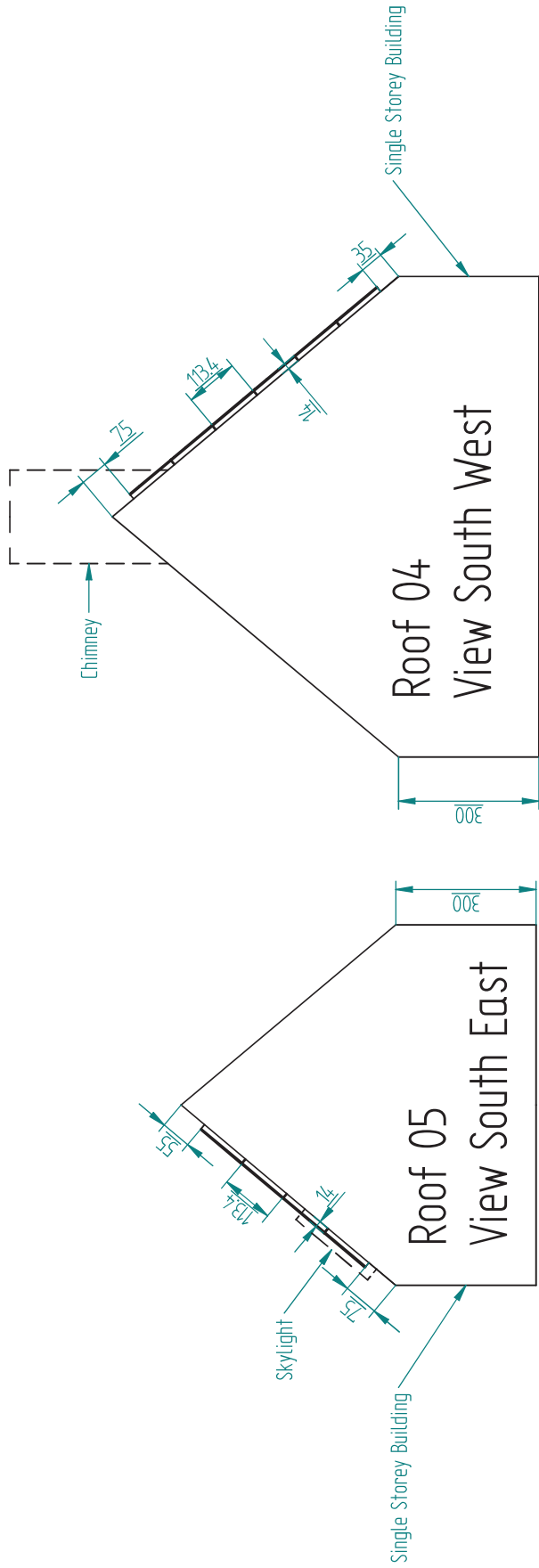
Project Name
Yeading Infant and Nursery School - Western Elevation Plan

Scale

1:150



SCALE: 1:100




Pitched Roof:
PV panel and mounting kit
side view
13.4 x 172.2 x 14 cm

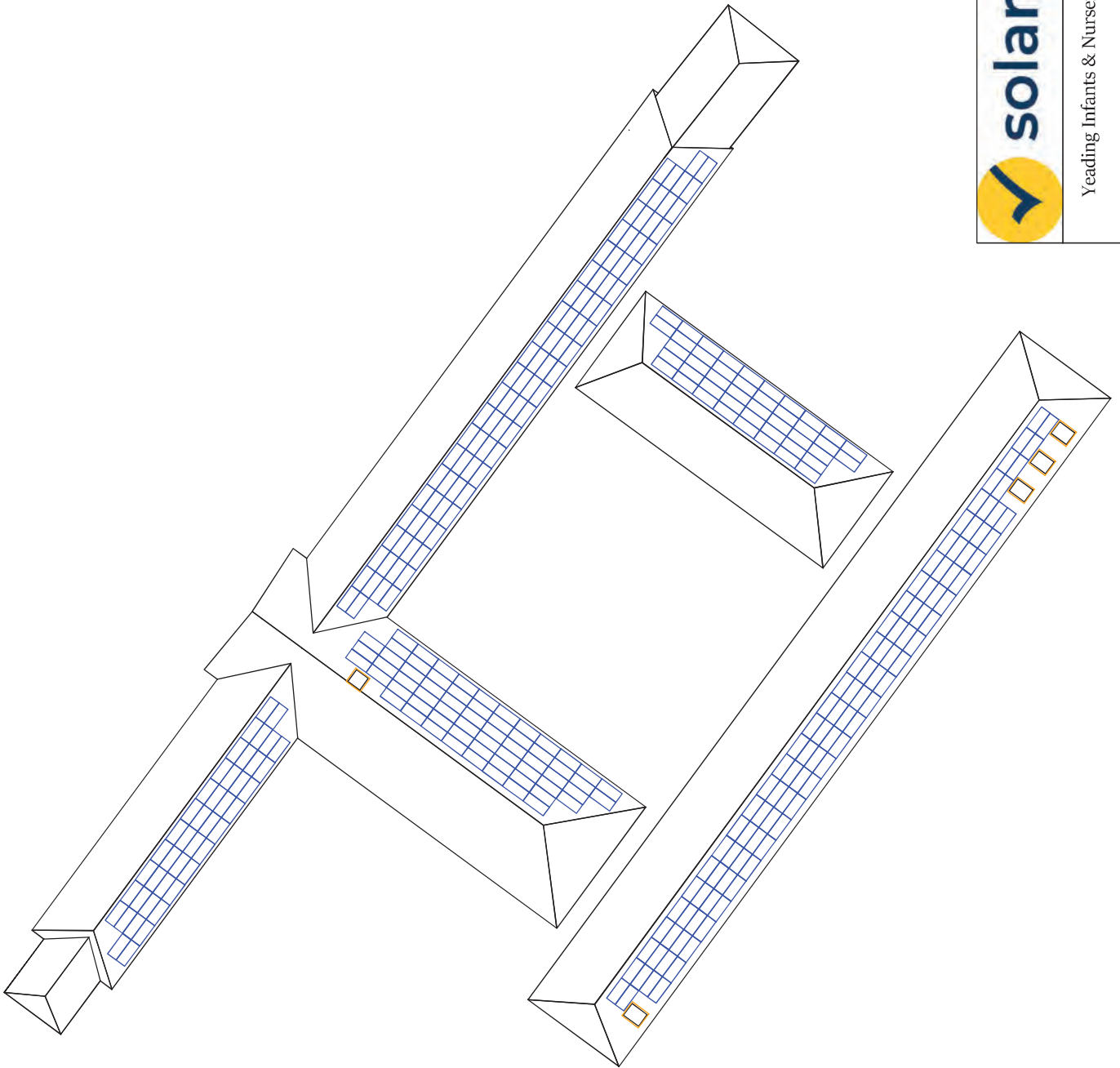
NAME	DATE
SolarForS	
DRAWN	
CHECKED	
ENG. APPR	
MGR. APPR	

<p>Solar For Schools</p> <p>TITLE Yeading Infants & Nursery School – Cross Section</p>	
<p>UNLESS OTHERWISE SPECIFIED DIMENSIONS ARE IN CM</p>	<p>SIZE: A3</p>
<p>SCALE: 1:100</p>	

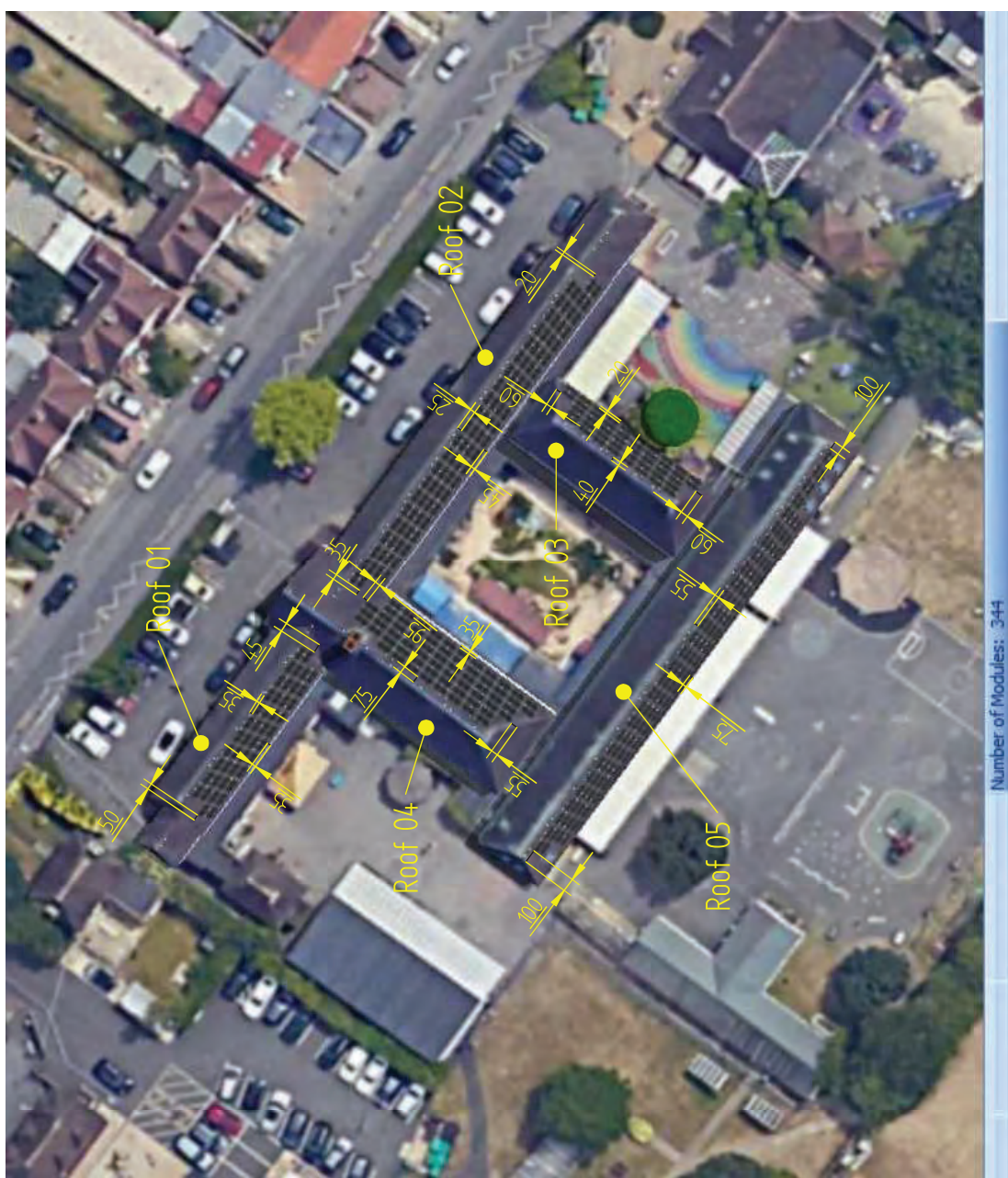
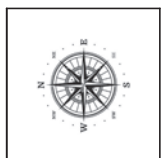
SOLID EDGE ACADEMIC COPY



 solar for schools
Yeading Infants & Nursery School - Roof Plan with Proposed Installation
Scale
1:300



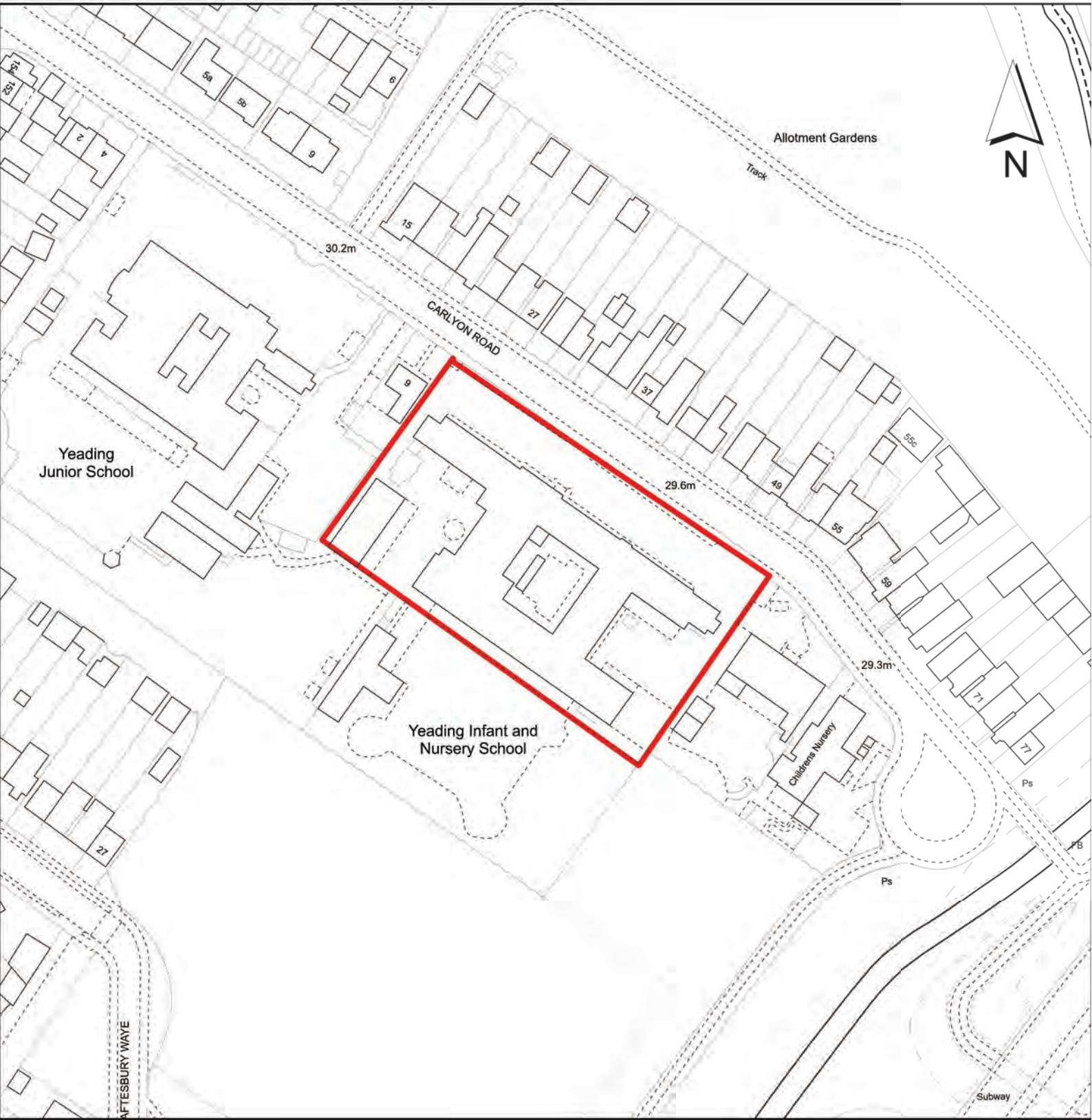
DWG: 4





DRAWN	NAME	DATE
CHECKED	SolarFS	
ENG. APPR		
MGR. APPR		

	Pitched Roof: PV panel and mounting kit side view 13.4 x 172.2 x 14 cm
--	---

<h2>Solar For Schools</h2> <p>Yeading Infants & Nursery – Scaled Map</p>	
UNLESS OTHERWISE SPECIFIED DIMENSIONS ARE IN CM	
SIZE: A3	SCALE: 1:500



KEY :  Site Boundary	ADDRESS : Yeadling Infant School		LONDON BOROUGH OF HILLINGDON RESIDENTS SERVICES PLANNING SECTION CIVIC CENTRE, UXBRIDGE, UB8 1UW
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	PLANNING COMMITTEE :	DATE : October 2024	

Report of the Head of Development Management and Building Control

Address: PINKWELL PRIMARY

Development: Replacement of the existing timber fence with 3.0m high V mesh security fencing.

LBH Ref Nos: 11242/APP/2024/1302

KEY:

- Fence Area Boundary
- Site Ownership Boundary



SITE LOCATION PLAN

SCALE : 1:1250

Rev	Description	Date
P02	Site key revised.	22/05/2024
P01	First issue.	15/05/2024

Client:
TEFAT

Project Title:
TEFAT - EDEN PINKWELL

RLB ID:
008434

Drawing Number:
01

Drawing Title:
SITE LOCATION PLAN

Stage: **PLANNING** Status: **S0** Revision: **P02**

Date: **22/05/2024** Size: **A3** Scale: **1:1250**



Level 11, The Shard, 32 London Bridge Street, SE1 9SG
+44 (0) 20 7398 8300 RLB_UK RLB.com

Contact: Jack.Hutchinson@rlb.com
Author: Harvey.Lee@uk.rlbc.com
QA: Ryan.Srinivas@uk.rlbc.com

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All work to comply with Construction Change Management (CCM) 2015.
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EXISTING AND PROPOSED FENCING PLAN

SCALE : 1:500

KEY:

- Site Ownership Boundary
- Existing Fence Perimeter
- Proposed Fence Perimeter

Rev	Description	Date
-----	-------------	------

PO3	Site key revised.	18/09/2024
PO2	Site key revised.	22/05/2024
PO1	Final Issue.	15/05/2024

Client:
TEFAT

Project Title:
TEFAT - EDEN PINKWELL

RLB ID:
008434

Drawing Number:
03

Drawing Title:
EXISTING AND PROPOSED FENCING PLAN

Stage:
PLANNING

Status:
S0

Revision:
P03

Date:
19/09/2024

Size:
A3

Scale:
1:500



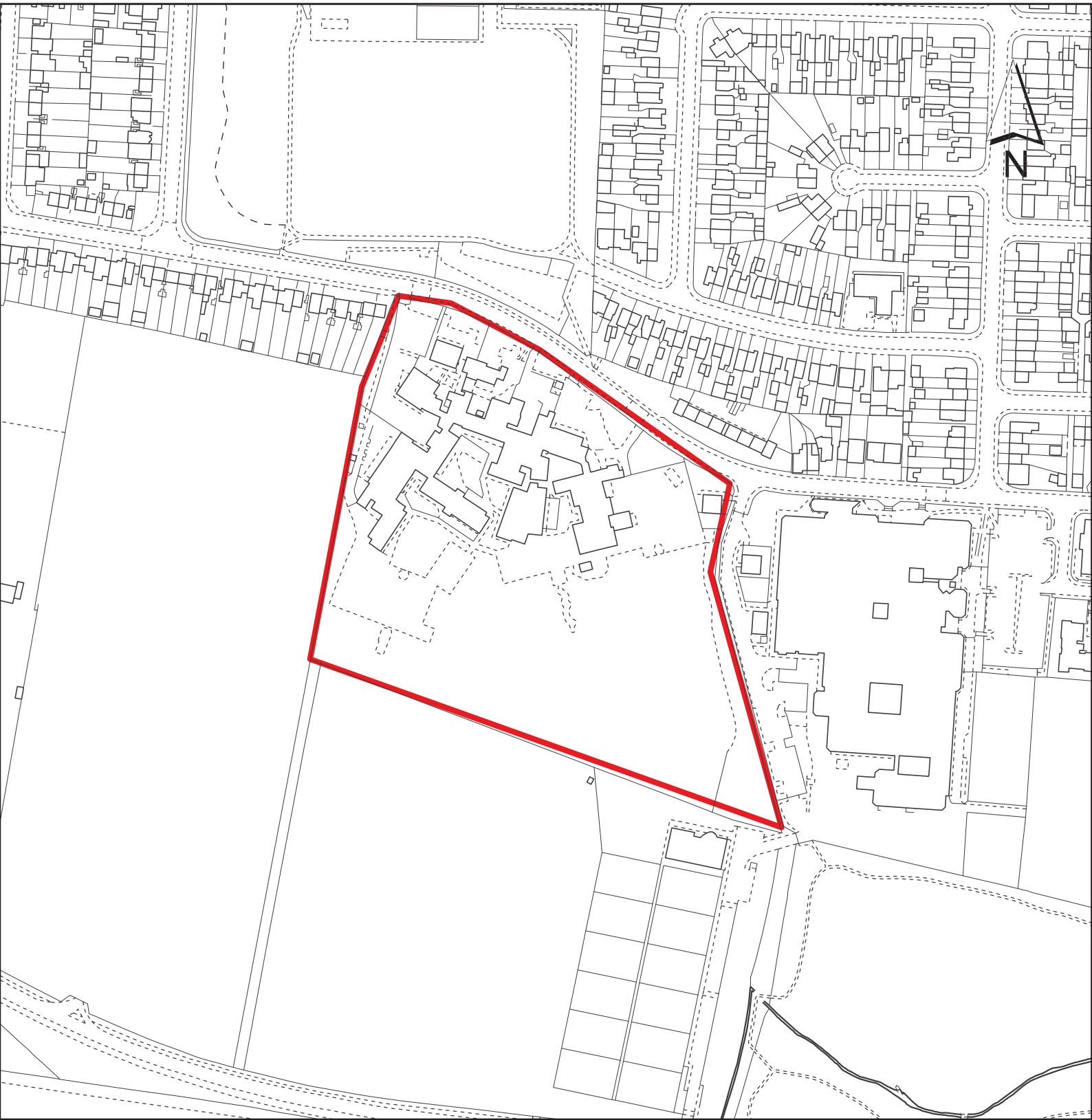
Level 11, The Strand, 20 Leaden Ridge Street, SE1 1RS
+44 (0) 20 7388 6330



Contact: Jacob.Hutchinson@rlb.com

Author: Harvey.Lee@rlb.com

QA: Ryan.Shera@rlb.com

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All work to comply with Construction Design Management (CDM) 2015.
Drawing to be printed in A3, to the scale indicated in the title block.
Prepared: Date: 19/09/2024
Checked: Date: 19/09/2024
Approved: Date: 19/09/2024

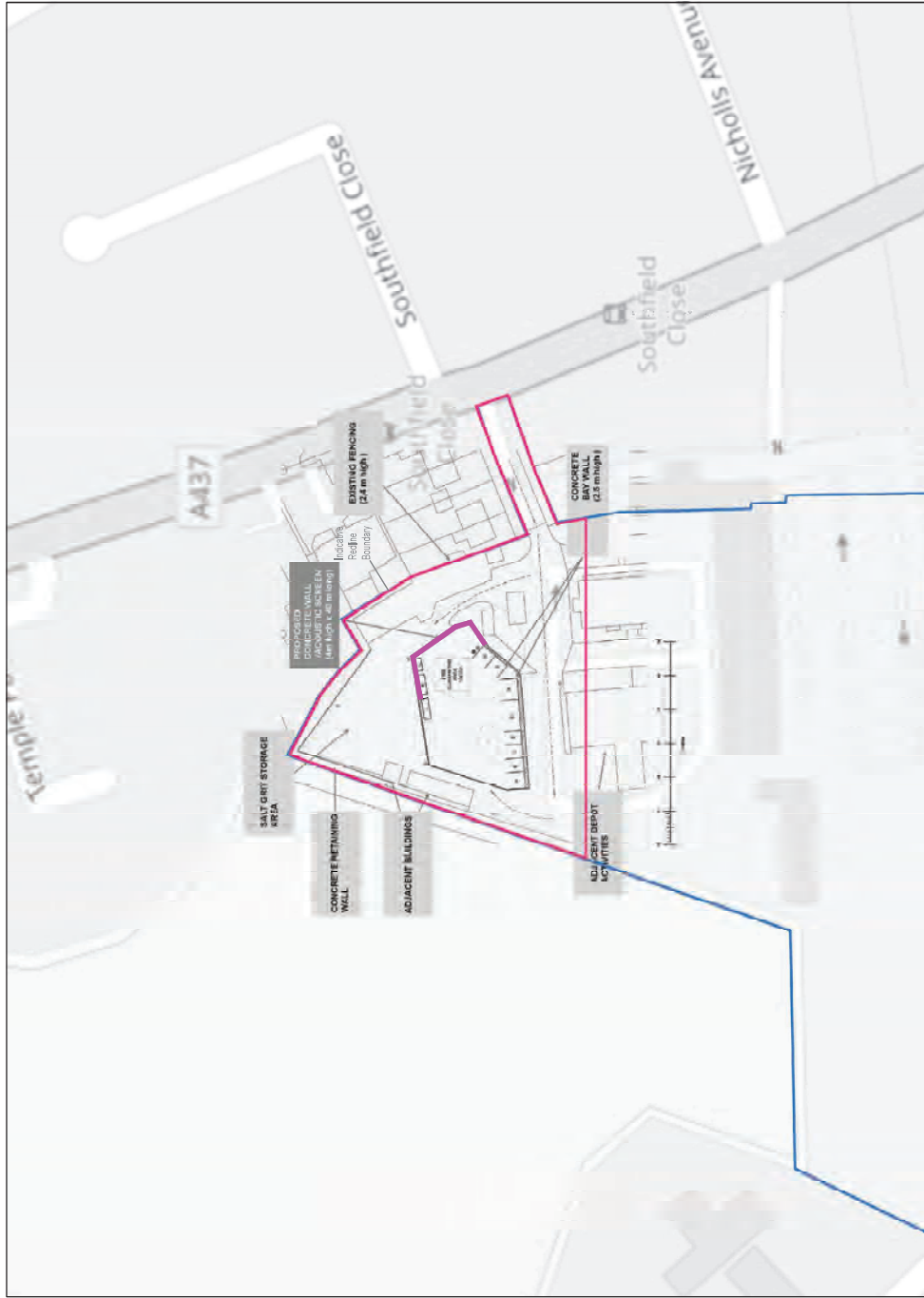


KEY :  Site Boundary	ADDRESS : Pinkwell Primary		LONDON BOROUGH OF HILLINGDON RESIDENTS SERVICES PLANNING SECTION
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	PLANNING COMMITTEE :	DATE : October 2024	

Report of the Head of Development Management and Building Control

Address: LBH CENTRAL DEPOT 128 HARLINGTON ROAD HILLINGDON
Development: Installation of an acoustic wall around service yard.
LBH Ref Nos: 4501/APP/2024/1618

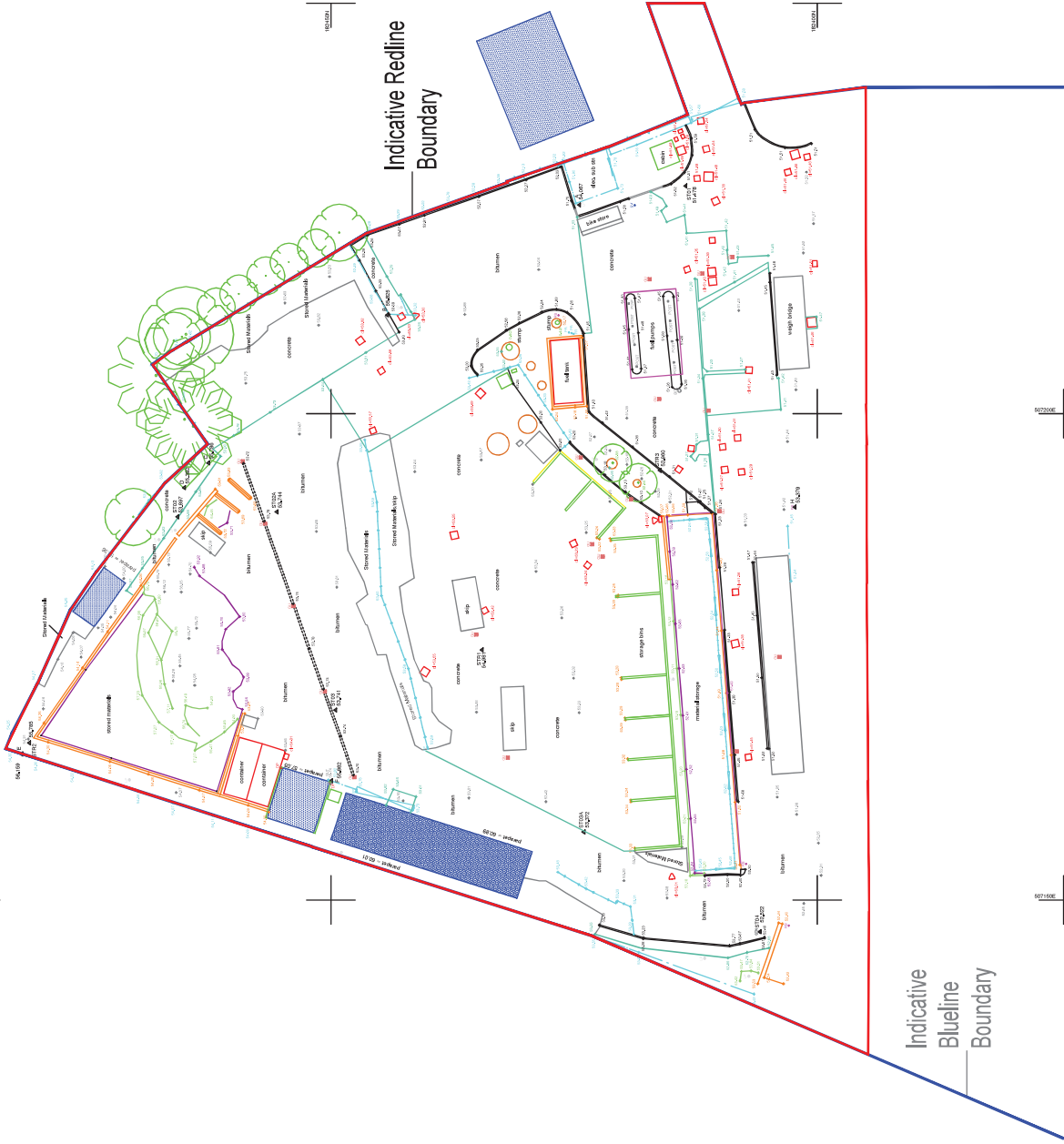
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Rev	Comment	Date
-	-	-

..Hillingdon Construction Consultancy Logo Highlands House, Highlands Road Shirley, Solihull, B90 4ND Tel: +44 (0)121 765 2900 Email: info@gallen.uk.com	
Client	London Borough of Hillingdon
Project	London Borough of Hillingdon - Depot Office Refurbishment Works UB8-3EU
Drawing Name	Site Location Plan
Project No.	6718
Date	20/05/2024
Drawn by	C.N.
Checked by	P.Q.
Drawing No.	6718-ACC-00-ZZ-DR-A-1001
Scale @A3	1:1250 REV P1

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Rev	Comment	Date
-	-	-

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 Shirley, Solihull,
 B90 4ND
 Tel: +44 (0)121 765 2900
 Email: info@gallen-uk.com

Client
 London Borough of
 Hillingdon

Project
 London Borough of
 Hillingdon - Depot
 Office Refurbishment
 Works

Drawing Name
 Existing Site
 Topographical Survey

Project No. 6718

Date 20/05/2024

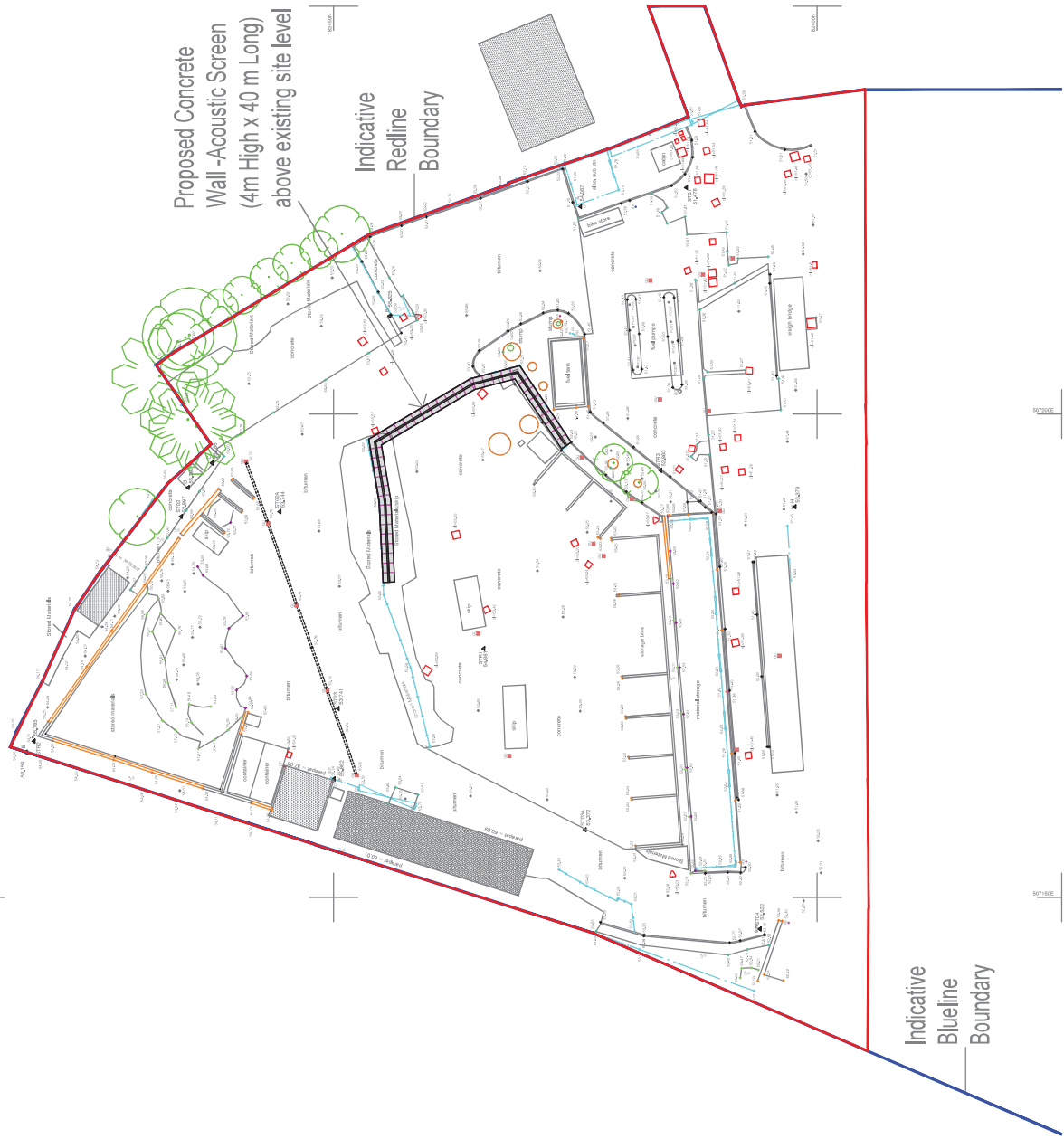
Drawn by C.N.

Checked by P.Q.

Drawing No. 6718-ACC-00-ZZ-DR-A-1002

Scale @A3 1:500 REV P1

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SCALE 1:500

Rev	Comment	Date
-	-	-

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 Email: info@gallen-uk.com

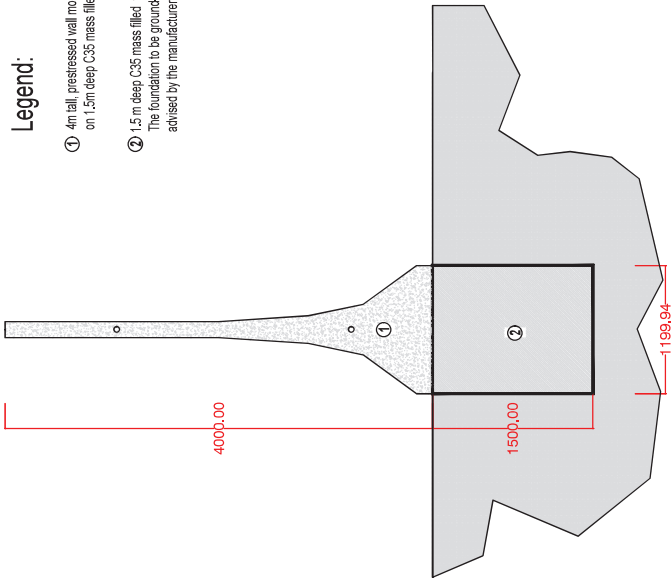
Client
 London Borough of
 Hillingdon

Project
 London Borough of
 Hillingdon - Depot
 Office Refurbishment
 Works

Drawing Name
 Proposed Site Block
 Plan

Project No.	6718
Date	20/05/2024
Drawn by	C.N.
Checked by	P.Q.
Drawing No.	6718-ACC-00-ZZ-DR-A-1003
Scale @A3	1:500 REV P1

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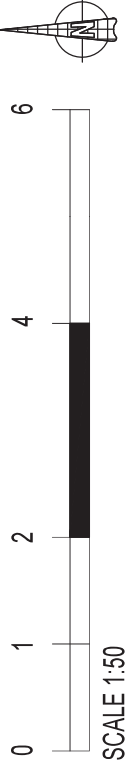
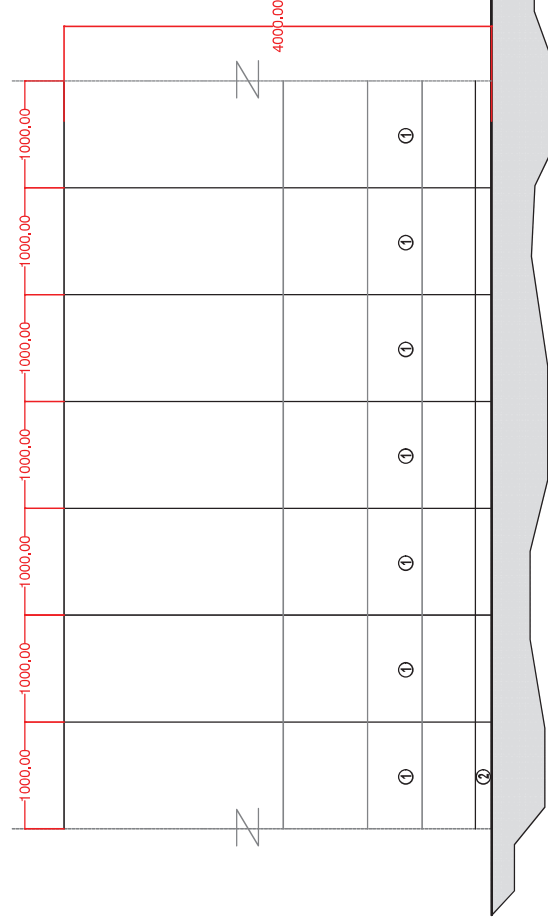


Legend:

- ① 4m tall, prestressed wall modules, 1m wide on 1.5m deep C35 mass filled foundation, and 40 m overall length.
- ② 1.5 m deep, C35, mass filled, foundation. The foundation to be ground-fitted as advised by the manufacturer.



Indicative Visual of the Wall Modules



Rev	Comment	Date
-	-	-

Highlands House, Highlands Road
 Shirley, Solihull,
 B90 4ND
 Tel: +44 (0)121 765 2900
 Email: info@allen-uk.com

Client
 London Borough of
 Hillingdon

Project
 London Borough of
 Hillingdon - Depot
 Office Refurbishment
 Works

Drawing Name
 Section, Elevation and
 Indicative Visual of
 the Proposed Wall


Project No. 6718
 Date 20/05/2024

Drawn by C.N.

Checked by P.Q.

Drawing No. 6718-ACC-00-ZZ-DR-A-1004
 Scale @A3 1:50 REV P1



KEY :  Site Boundary	ADDRESS : LBH Central Depot		LONDON BOROUGH OF HILLINGDON RESIDENTS SERVICES PLANNING SECTION
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	PLANNING COMMITTEE :	DATE : October 2024	